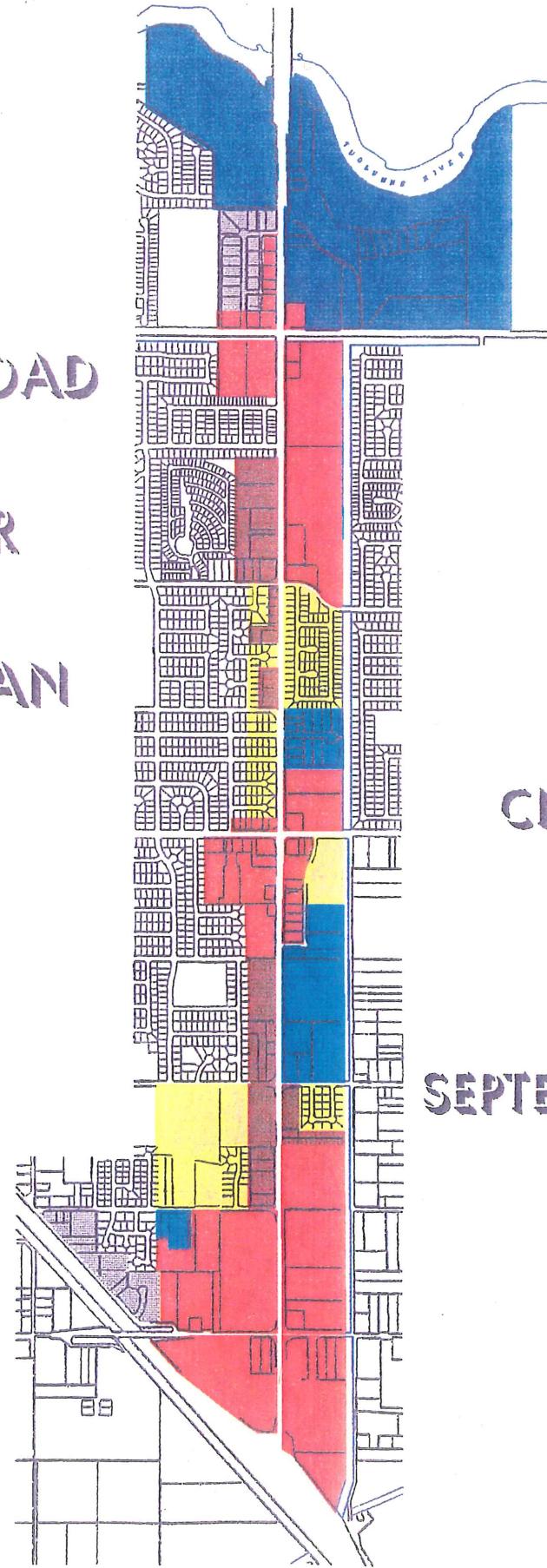


**Mitchell Road
Corridor
Specific Plan**



**CITY OF CERES
ADOPTED
SEPTEMBER 25, 1995**

PART ONE

MITCHELL ROAD CORRIDOR SPECIFIC PLAN

MITCHELL ROAD CORRIDOR SPECIFIC PLAN

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Adopted: Resolution 89-177, August 28, 1989
Ordinance 89-739, September 11, 1989
Ordinance 89-744, January 9, 1990

Amended: Ordinance 94-825, March 14, 1995
Resolution 95-120, September 11, 1995
Ordinance 95-850, September 25, 1995

Revised and edited by the
Ceres Planning and Community Development Department
September 1995

MITCHELL ROAD CORRIDOR SPECIFIC PLAN
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SPECIFIC PLAN - PREFACE

The Mitchell Road Corridor Specific Plan (MRCSP) establishes comprehensive guidance and regulations for the development of approximately 450 acres located along a 2 and 1/2 mile stretch of Mitchell Road between Highway 99 and the Tuolumne River within the City of Ceres, California. The (MRCSP) establishes development regulations, policies and programs for the implementation of the approved land use plan. The Specific Plan, which is implemented in accordance with the Government Code, is a regulatory document adopted by ordinance to be consistent with the City of Ceres General Plan.

The MRCSP combines the concepts, procedures and regulations of numerous documents into one. The Specific Plan provides for the consideration of a wide variety of commercial and business park uses. The Specific Plan also contains the development controls to provide the City with assurances that the project will be built out as described in this document. To further ensure that design criteria are adhered to, a set of design guidelines have been prepared as a separate document and in conjunction with this Specific Plan. The design guidelines cover project design criteria, including site planning, architecture, circulation, open space and recreation, landscaping and lighting.

The authority for preparation of specific plans is found in the California government Code, Section 65450 et. seq. The law allows preparation of specific plans based on the General Plan, as may be required for the systematic execution of the General Plan and further allows for their review and adoption.

I. INTRODUCTION

A. PURPOSE AND INTENT

Responsible planning for future development of the Mitchell Road Corridor can be ensured through the adoption of a specific plan, an enforceable development control mechanism that will provide thorough and comprehensive land use planning. The Specific Plan will be adopted by ordinance and will therefore serve as a regulatory land use plan, functioning in the place of traditional zoning. A specific plan is a device used to implement the City's General Plan and, in accordance with state regulation (Sections 65450 and 65454), will be consistent with the General Plan.

The Specific Plan is used to provide more detail for a focused planning area such as the Mitchell Road Corridor. It will impose regulations on properties within the planning area to coordinate the complex mix of industrial, commercial and residential uses in order to create a vital, aesthetic and safe urban environment.

The adoption of the MRCSP establishes the type, location, intensity and character of land use and the required infrastructure needed for its integrated development. The (MRCSP) shapes development to respond to the physical and environmental constraints of the area, coordinates the

mix of industrial, commercial and residential uses and provides adequate circulation, public facilities and internal community space amenities.

B. PROJECT LOCATION AND DESCRIPTION

1. Project Location

The Mitchell Road Corridor study area is located in the City of Ceres which is located adjacent to and south of the City of Modesto in Stanislaus County. Ceres enjoys good regional access, being located approximately 75 miles south of Sacramento on State Highway 99, and 100 miles east of San Francisco via the 5, 205, and 580 Freeways (see Figure 1). In addition, Mitchell Road provides direct access to the Modesto City-County Airport located north of the Tuolumne River.

The corridor defines the eastern edge of Ceres and extends from Highway 99 to the Tuolumne River. The entire study area is 2.5 miles long and varies from 1/4 mile wide in the mid-sections, to 1/2 mile wide at either end (see Figure 2). The corridor supports a range of land uses including older strip commercial areas and new or planned commercial centers. These uses are located primarily between Fowler and Roeding Roads and at the intersections of Hatch/Mitchell and Don Pedro/Mitchell. Underutilization of parcels can be seen in the form of space intensive land uses such as used car lots and remnant agricultural lands. Some lots are merely vacant while others, primarily between Fowler and Whitmore, have undergone residential development.

2. Project Description

The Mitchell Road Specific Plan will be used to guide future development in a manner that will produce an urban corridor that integrates industrial, commercial and residential uses into a vibrant community. The major land uses of the Specific Plan are divided into seven (7) Specific Plan land use zones (see Table 1, page 10). While each of these zones has a unique purpose and focus, together they form a pattern that: emphasizes large-scale, region-serving commercial in the area generally south of Roeding Road around the intersection of Mitchell Road and State Route 99; accommodates a mixture of uses that serve primarily the local market in the area generally between Hatch Road and Roeding Road; and seeks to take advantage of its proximity to the Tuolumne River and Modesto Airport by supporting recreation, business parks, office, residential and mixed use in the area north of Hatch Road. Development will be coordinated through performance standards designed to promote innovative plans for large lot development, special activity nodes and circulation. In addition, the MRCSP establishes design districts for the three distinct areas described above (Figure 3). These design guidelines establish building and site design principles, standards and requirements that serve to unify and otherwise harmonized the architecture, landscaping and site plans of a diverse range of land uses. The design guidelines of each design district are also intended to reflect and reinforce the importance of their locations: a north gateway to Ceres from Modesto, including the commute traffic from the Beard Industrial District, a major employment center in Stanislaus County; a south gateway to Ceres and a strategically important future interchange with State Route 99; and an emerging area along the thoroughfare that has the potential to accommodate a significant portion of the community's

REGIONAL LOCATION MAP

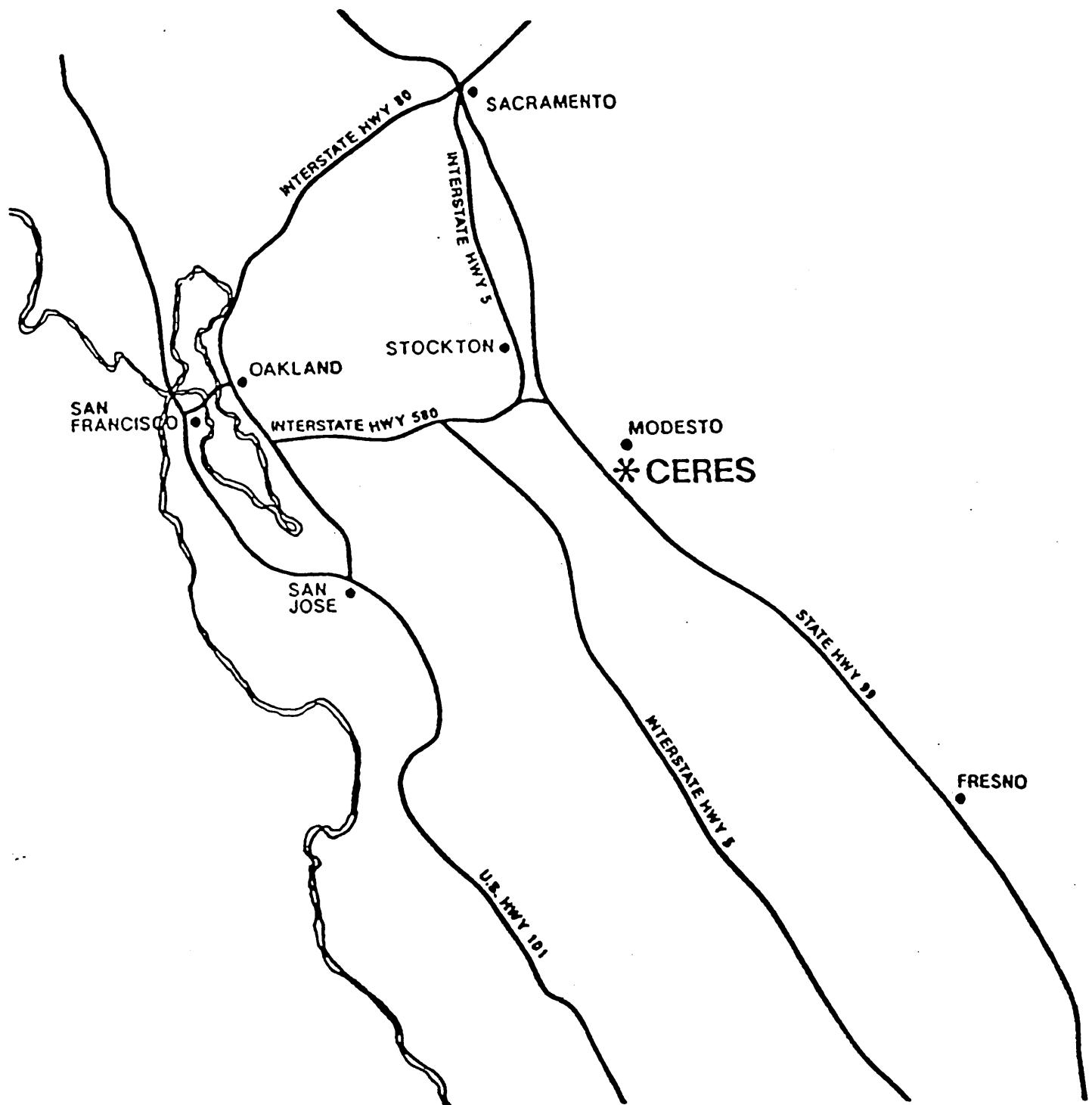


FIGURE 1

VICINITY MAP

FIGURE 2

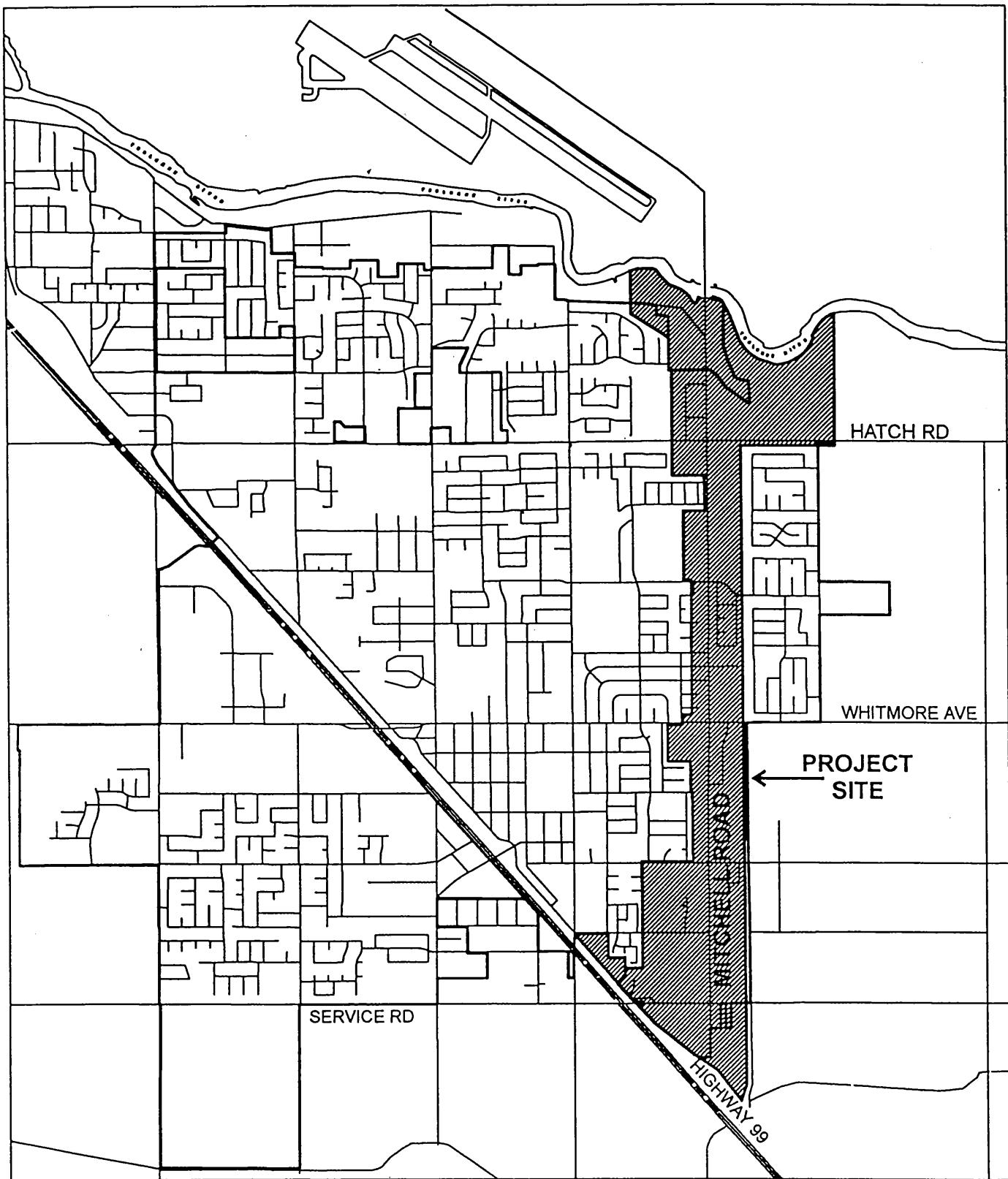


FIGURE 2

**MITCHELL ROAD CORRIDOR
SPECIFIC PLAN
DESIGN DISTRICT BOUNDARY**

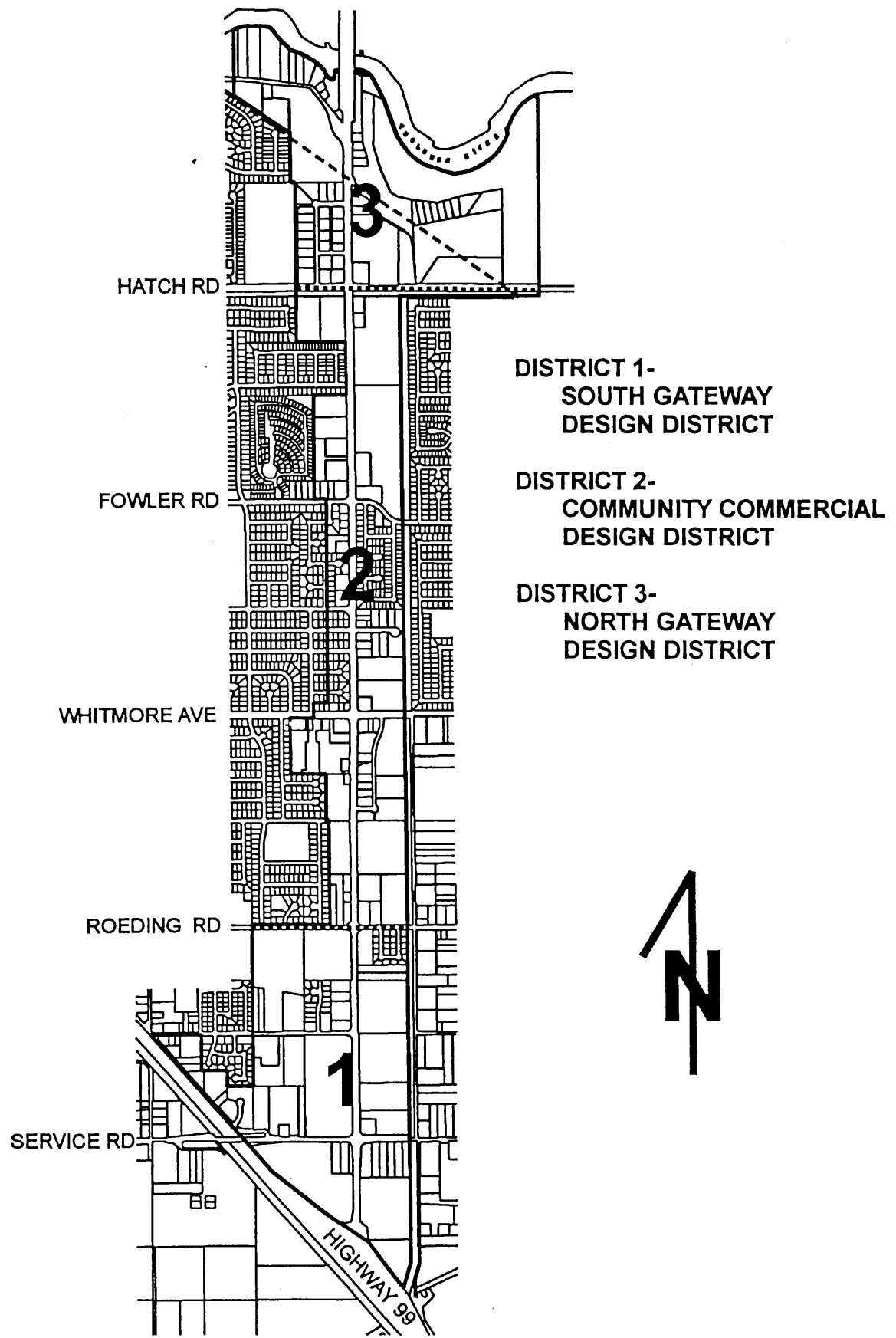


FIGURE 3

retail and service needs, and to help reduce the outflow of potential sales to other communities in the county.

3. Project Background

The Mitchell Road Corridor has evolved into a mix of strip commercial uses scattered with residential and light industrial uses. The emerging trend is predominantly commercial. However, the quality and condition of commercial uses vary from exemplary to deteriorating "eye sores". The corridor is inhabited by numerous "transient" types of commercial uses. These are commercial users that lease space and have a high visibility of closing business when profits drop. Examples of transient uses would be used car lots, or non-franchised take-out food restaurants. These type of uses generally do not upgrade the aesthetic appeal of the store front, but instead rely on heavy signage to attract attention.

Further, the City is interested in upgrading its image as a viable commercial center. These factors have contributed to the need for the MRCSP.

C. CALIFORNIA ENVIRONMENTAL QUALITY ACT COMPLIANCE

This Specific Plan was prepared in accordance with the California Environmental Quality Act (CEQA). An environmental impact report (EIR) was prepared in conjunction with the Specific Plan and has been certified by the City Council on August 28, 1989, Resolution Number 89-176. All development projects in the study area which are consistent with the Specific Plan will be served by this EIR. The environmental analysis will be based on Sections 15162 and 15182 of the CEQA guidelines.

D. AUTHORITY AND SCOPE

The Mitchell Road Corridor Specific Plan has been prepared pursuant to the provisions of the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 64450 through 65457. The Government Code authorizes jurisdictions to adopt specific plans by resolution as policy or by ordinance as regulation. Public hearings are required by both the Ceres Planning Commission and City Council, after which the City Council must adopt the plan before it becomes effective.

The MRCSP is a regulatory plan which will serve as the zoning ordinance for the subject property. Proposed development plans or agreements, tentative maps, and any other development approvals must be consistent with the plan. Projects which are found consistent with the Plan will be deemed consistent with the City's General Plan. The Specific Plan is intended to reflect the interest and concerns of the City and community through its standards and regulations.

This plan will serve to implement the development of the approved plan within the bounds of the regulations provided herein which are adopted by ordinance. This document together with the Final EIR and other associated documents also provide a sufficient data base to support the project concept.

E. SEVERABILITY CLAUSE

In the event that any regulation, condition, program, or portion of this Specific Plan is held invalid or unconstitutional by California or Federal Court of competent jurisdiction, such portions shall be deemed separate, distinct, and independent provisions, and in the validity of such provisions shall not affect the validity of the remaining provisions thereof.

II. DEVELOPMENT PLAN

A. PURPOSE AND INTENT

The development plan is a culmination of several project alternatives and the input provided from the project consultant team, the city staff and the interested public. As a result, the plan resolves, as much as possible, development-related issues in the form of proposed physical improvements, guidelines for future development, technical data, implementation procedures and regulations.

The primary purpose of the MRCSP is to implement the General Plan through the coordinated physical and economic development of the Mitchell Road Corridor. Included in this concept are a number of design objectives to guide development of the Specific Plan area and create a planned commercial and business park area harmonious with the existing uses within the corridor and the adjacent and nearby land uses (i.e., airport, residential) while meeting the standards of site development, circulation, intensity of use and community character.

The Specific Plan identifies the development issues, opportunities and constraints posed by corridor development, and from them, an overall project concept is derived that will organize future development into a planned urban pattern that will maximize the corridor's economic potential and quality of life. The project concept provides the rationale for the development regulations that will control development according to the Specific Plan.

B. ISSUES, CONSTRAINTS AND OPPORTUNITIES

In the preliminary stages of plan formulation, the key development issues, opportunities and constraints were identified through meetings between the consultant, city staff, responsible agencies and interested parties, property owners and community members. The identification of these concerns provides the initial direction for formulating the overall project concept and subsequent development regulations. In this way the Specific Plan has the ability to spatially define an area, analyze the development constraints and opportunities, and play them off each other in the form of a development plan.

1. Issues

The issues which have been identified are:

- The perception of the Mitchell Road Corridor, both from the north and south entryways and from within the corridor itself.
- The relationship between different modes, i.e., pedestrian, auto, transit and their relation to different and uses areas.
- The spatial relationships between different commercial, industrial/business park and residential uses.
- Loss of agricultural acreage.
- Compatibility of proposed uses and building heights on property adjacent to the Modesto City-County Airport and Stanislaus County Airport Land Use Commission.

2. Constraints

Constraints that were identified and considered in the development of the land use concept include:

- Existing land use and development patterns.
- Existing circulation and parking configurations.
- Floor Area Ratio (FAR) and building height restrictions.
- Airport height and land use restrictions.
- Deep lots served by single frontage road.
- Lack of water and sewer in the southern end of the corridor.
- Lack of an accepted design, funding and construction schedule for the Mitchell Road State Route 99 interchange.

3. Opportunities

Potential opportunities were identified and enhanced as feasible to maximize the positive aspects of the project area and carry them into the development of the land use concept. Opportunities include the following:

- Increased employment base of City.
- Increased municipal revenue.
- Good regional access.

- Expansion of economic growth for Ceres.
- Creation of gateways to and an identity for Ceres; an integrated urban environment with a strong sense of place.
- Expansion of Mitchell Road to a thoroughfare.
- Creation of a coordinated circulation system that provides internal circulation for large parcel development.
- Creation of uniform development standards that will attract development interests.
- Comprehensive Planning to reduce incremental development costs and processing time.
- Protect existing residential development from impacts of urban development.

C. GOALS, OBJECTIVES AND POLICIES

Goal - Create a pattern of commercial uses that capitalizes on and maximizes the economic potential of the Mitchell Road Corridor so that area can be viewed as an economic asset.

Objectives

1. Recognize a variety of commercial opportunities with three primary areas of focus:
 - a regional commercial center in the south gateway between State Highway 99 and Roeding Road;
 - community commercial uses to serve the immediate area located along Mitchell Road from Roeding Road to Hatch Road; and,
 - mixed residential, recreation commercial, business park, and uses in the north gateway between Hatch Road and the Tuolumne River.
2. Develop an image for the City that is distinctive and inviting.
3. Create a entry statement to the City of Ceres.
4. Incorporate a circulation concept that maximizes land use potential while minimizing traffic conflicts.
5. Reflect a sensitivity to and protection for existing adjacent neighborhoods.
6. Provide a defensibility in terms of economic and political feasibility.

7. Insure consistency with the goals and policies of the Ceres General Plan.
8. Address the immediate short-term needs of the corridor as well as considers the comprehensive and long-range concerns for the area.

Policies

- P1. Provide measures which will enhance the visual quality of the Mitchell Road Corridor by concealing unsightly uses, equipment (i.e., screening of roof top equipment and undergrounding of utilities).
- P2. Preserve existing significant trees.
- P3. Permit a range of building heights which are demonstrated to be suitable for the corridor area.
- P4. Provide buffers and transitions between commercial uses and adjacent non-commercial uses.
- P5. Incorporate innovative circulation concepts which encourage public transit and internal circulation alternatives to Mitchell Road.
- P6. Develop an implementation program that will provide for the reimbursement of all capital costs and planning costs.
- P7. Provide incentives and measures to avoid breaking up of existing large parcels into small, separate, unrelated development projects.
- P8. Evaluate the community-wide impact of full build-out of the Specific Plan area on the need for public facilities and services.
- P9. Determine whether the City's infrastructure system will adequately serve full build-out of the Specific Plan development.
- P10. Establish residential densities that are consistent with surrounding land uses.
- P11. Prepare a land use plan and associated development regulations which reflect the goals of the other applicable plans, the character of the existing environment and the goal of the Specific Plan.
- P12. Assure compatibility between park and open space areas and any new development within the Specific Plan area.

- P13. Incorporate open space areas into the design of development projects proposed for the corridor when feasible.
- P14. Ensure the publics' health, welfare and safety through design and building development.
- P15. Develop an implementation program that establishes specific procedures for site plan review to ensure project conformance with the Specific Plan.
- P16. Address growth limiting factors that may hinder development within the corridor.
- P17. Designate the area from State Highway 99 to Roeding Road as South Gateway Design District.
- P18. Maximize southern portion of the corridor for uses that have a regional market attraction such as theaters, department store, membership/wholesale stores, etc.
- P19. Require architectural, landscape and hardscape treatments that distinguish this as a major entryway to the City of Ceres within the South Gateway Design District.
- P20. Designate the area from Roeding Road to Hatch Road as Community Commercial Design District.
- P21. Permit uses which primarily serve Ceres and adjacent communities within the corridor between Roeding and Hatch Roads.
- P22. Require internal circulation patterns to reduce traffic impacts on Mitchell Road.
- P23. Permit residential uses when proposed in conjunction with commercial projects. Densities are noted on the Specific Plan Land Use Map.
- P24. Designate area from Hatch Road to Tuolumne River as North Gateway Design District.
- P25. Permit uses which capitalize on the proximity to the Mitchell Road commute traffic, airport, Tuolumne River, and recreation facilities in that portion of the corridor north of Hatch Road.
- P26. Require architectural, landscaping and hardscaping treatments which reinforce this area's importance as an entryway to the City of Ceres and are sensitive to the River Road Parkway and Tuolumne River environment within the North Gateway Design District.
- P27. Adequate water and sewer services shall be required for the development of properties within the Mitchell Road Corridor. Where adequate water and sewer services are not available, the use and development of properties in the Mitchell Road Corridor shall be limited to those uses and developments that are short-term and temporary.

P.28 The use and development of the properties south of Service Road shall be limited to those uses and developments that are short-term and temporary until such time as the design for the Mitchell-Service Road/State Route 99 Interchange has been accepted by CalTrans.

D. LAND USE PLAN

The Mitchell Road Corridor Land Use Plan is depicted on Figure 4, and the Land Use Designations (Specific Plan zones) allocations are summarized in Table 1. The project site encompasses approximately 450 acres of land which are currently in various stages of development.

TABLE 1

MITCHELL ROAD CORRIDOR SPECIFIC PLAN LAND USE DESIGNATIONS

Community Commercial (CC)

Highway Commercial (HC)

Industrial Park (IP)

Mixed Use 1 (Commercial Residential Business Park, MX-1)

Mixed Use 2 (Residential, Business Park, Recreational, MX-2)

Regional Commercial (RC)

Residential (R)

The Mitchell Road Corridor Specific Plan offers a wide range of commercial uses to support community and regional needs.

E. CIRCULATION PLAN

Figure 5 shows the arterials and expressways located within the MRCSP's planning area as classified by the 1991 Circulation Element.

III. DEVELOPMENT REGULATIONS

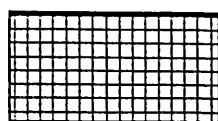
STATEMENT OF PURPOSE

The purpose of these regulations is to protect the public health, safety and welfare by implementing the planning provisions of the Mitchell Road Corridor Specific Plan and Ceres General Plan.

A. STATEMENT OF INTENT

These regulations constitute the primary zoning provisions for the properties included in the MRCSP. Figure 4 illustrates the area which is subject to these regulations.

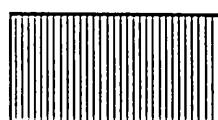
COMMUNITY COMMERCIAL (CC)



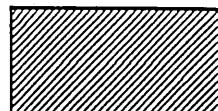
REGIONAL COMMERCIAL (RC)



HIGHWAY COMMERCIAL (HC)

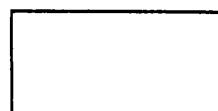


INDUSTRIAL PARK (IP)



RESIDENTIAL

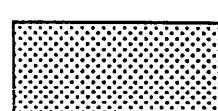
RL-7 LOW DENSITY RESIDENTIAL (7 DU/AC. MAX)
RM-15 MEDIUM DENSITY RESIDENTIAL (15 DU/AC. MAX)
RH-25 HIGH DENSITY RESIDENTIAL (25 DU/AC. MAX)



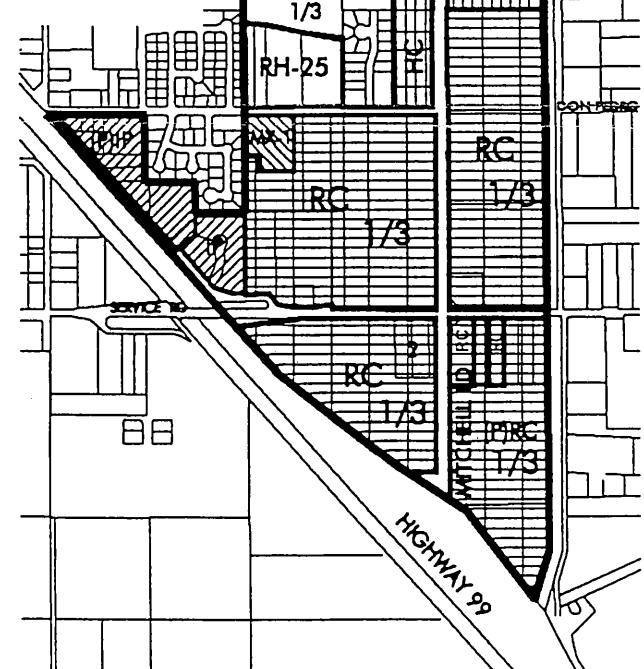
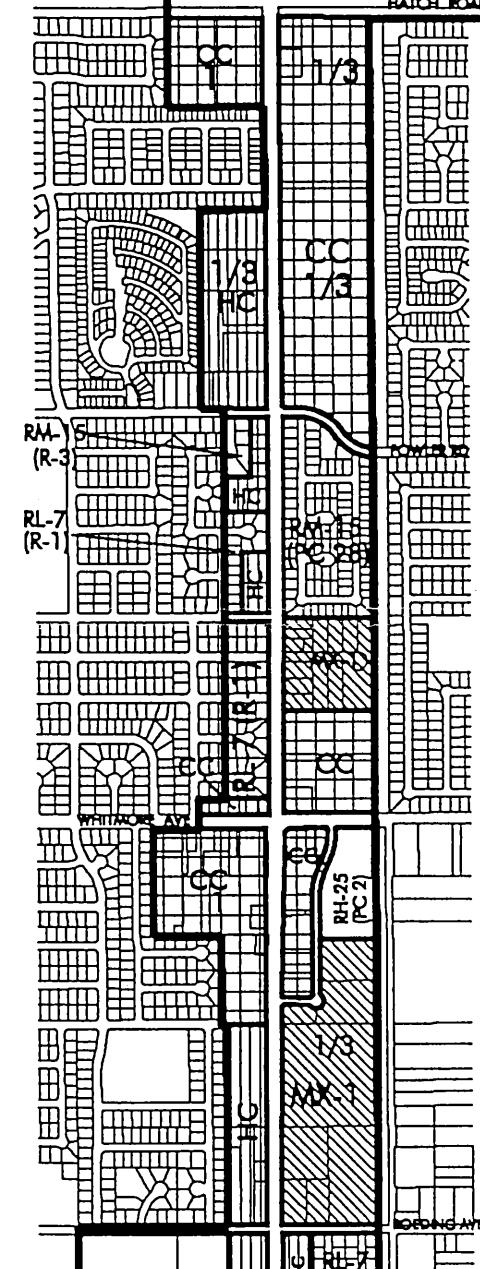
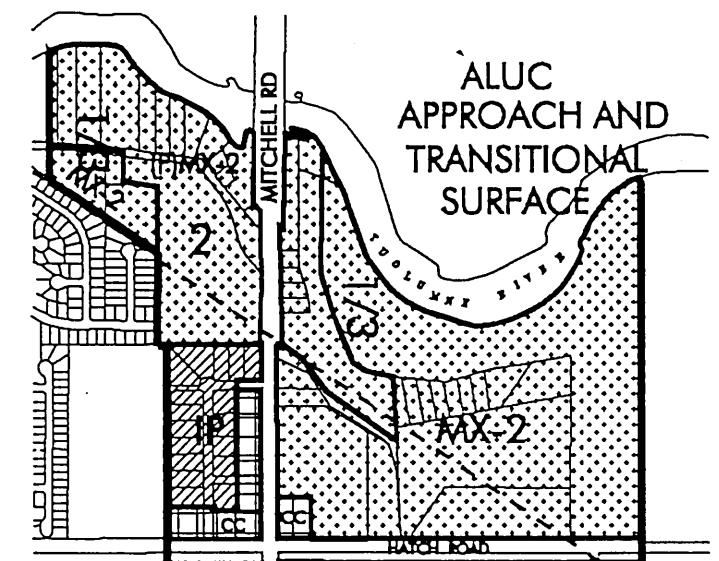
MIXED USE 1 (MX-1)



MIXED USE 2 (MX-2)



STANISLAUS AIRPORT LAND USE COMMISSION'S
TRANSITIONAL - APPROACH SURFACES BOUNDARY (APPROXIMATELY)



MITCHELL ROAD CORRIDOR SPECIFIC PLAN

CITY OF CERES

ADOPTED: RESOLUTION 89-177 AUGUST 28, 1989
ORDINANCE 89-739 SEPTEMBER 11, 1989
ORDINANCE 89-744 JANUARY 9, 1990
AMENDED: ORDINANCE 94-825 MARCH 3, 1994
RESOLUTION 95-120 SEPTEMBER 11, 1995
ORDINANCE 95-850 SEPTEMBER 25, 1995

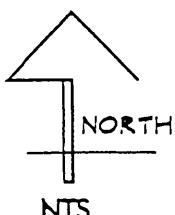


FIGURE 4

MITCHELL ROAD CORRIDOR SPECIFIC PLAN CIRCULATION PLAN

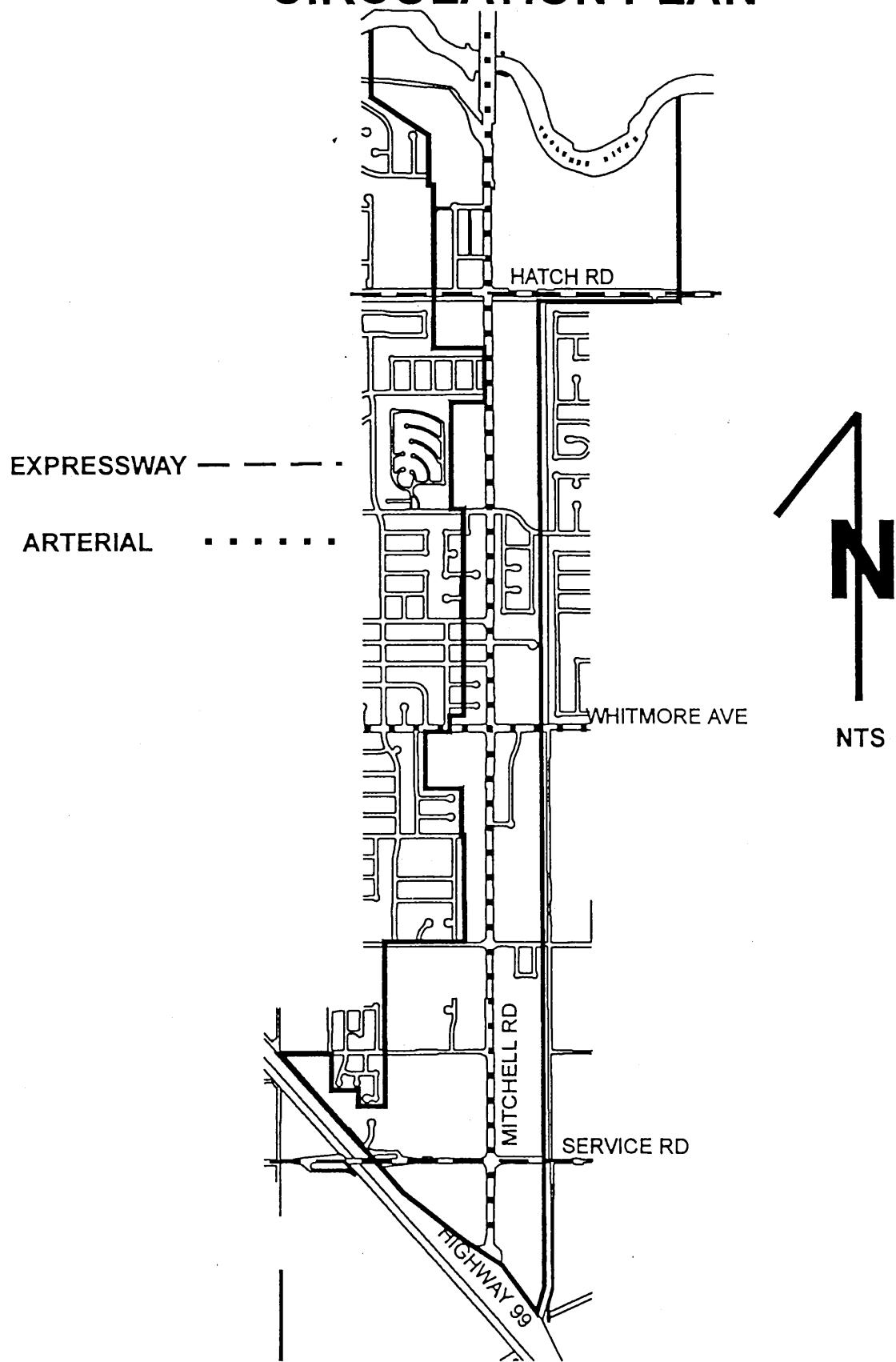


FIGURE 5

These regulations are in addition to the provisions of the Municipal code. Where there is a conflict, the regulations herein shall prevail.

The overall intent of these regulations is to guide development within the Mitchell Road Corridor so as to ensure that each individual development project contributes to meeting the goals and objectives for the entire Corridor. More specifically, the regulations are intended to:

1. Encourage an attractive variety of commercial uses within the Corridor to contribute economically, socially and visually to the community.
2. Provide opportunities and incentives for economically productive business enterprises and efficient use of land parcels, either through consolidation of existing parcels or coordinated planning of adjacent parcels.
3. Establish uses, local street design treatments, site development standards and design guidelines which contribute to the preservation and enhancement of adjacent residential environments.
4. Provide for the scale and nature of uses which can make the most efficient use of the existing development pattern as well as parcel size and shape.
5. Contribute to the improvements of both visual and functional attributes of the Corridor to help stimulate business activity.
6. Expand the traffic handling capacity of arterial highways, local streets and develop alternate circulatory routes to accommodate new development and redevelopment or rehabilitation of existing areas.
7. Establish a sense of place, as well as continuity and consistency of development standards within each district in the Corridor.

B. LAND USE LIMITATIONS - AIRPORT APPROACH AND TRANSITIONAL SURFACES

The use of buildings, structures and/or land listed in the table of compatible land uses, contained in Appendix D, shall be subject to the additional land use limitations described below. Such limitations shall apply only to those properties located under the Approach and/or Transitional surfaces as defined in the Stanislaus County Airport Land Use Commission's (ALUC) Land Use Plan for the Modesto City-County Airport (see Figure 6). These land use limitations are in addition to any zoning or other regulation of the use of buildings, structures and/or land established by the Mitchell Road Corridor Specific Plan.

USES LISTED IN APPENDIX D AS PROHIBITED. Such uses, including any residential use with a density of more than one unit per ten (10) acres, are prohibited under the Approach and/or Transitional Surfaces of the ALUC's Land Use Plan. However, this provision shall not prevent

AIRPORT APPROACH
AND TRANSITIONAL SURFACES

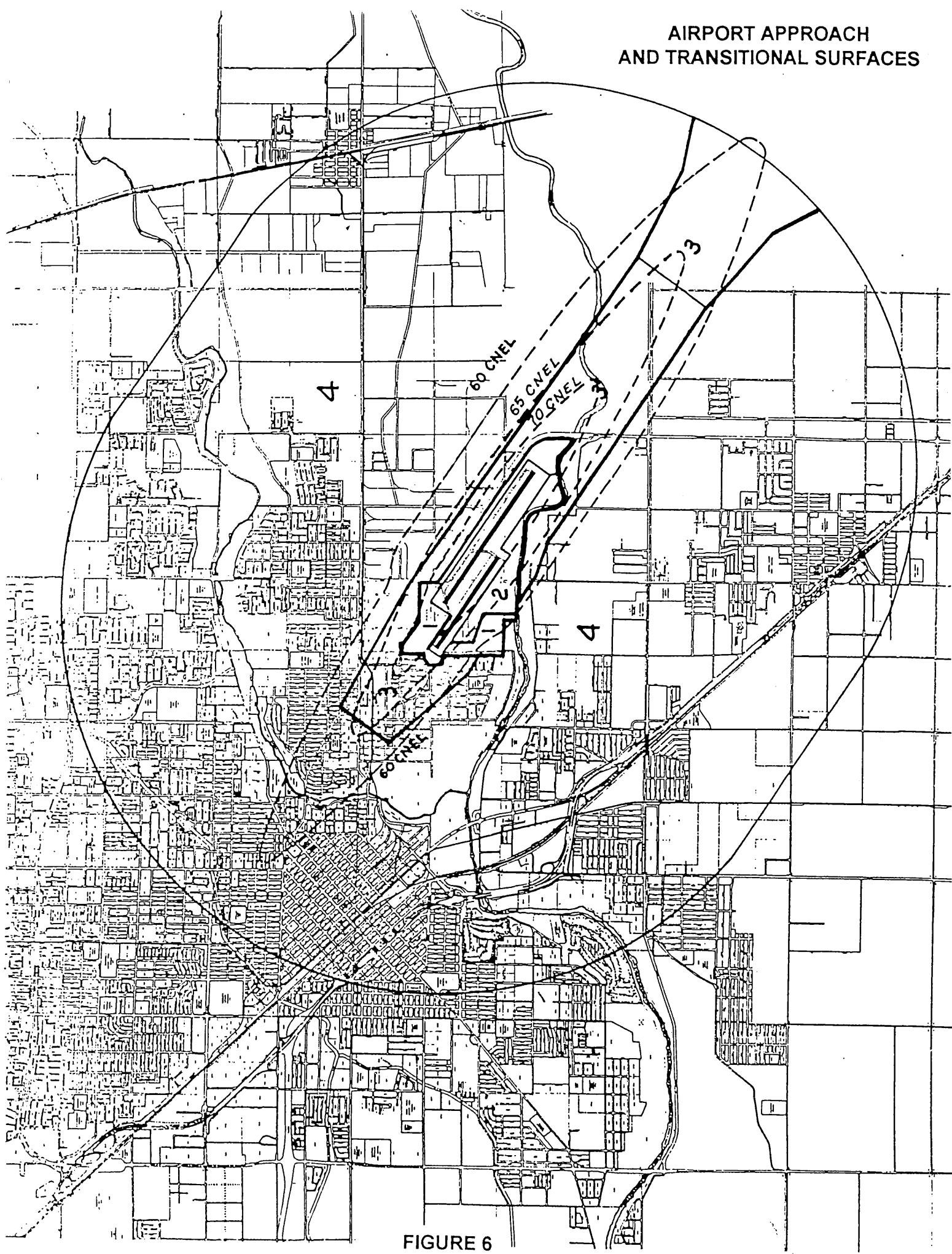


FIGURE 6

the owner of any existing legal parcel from constructing a residence on said parcel even if said parcel is less than ten (10) acres, providing such construction is approved by the ALUC.

USES LISTED IN APPENDIX D AS COMPATIBLE. Such uses may be established on land that is located under the Approach and/or Transitional Surfaces of the ALUC's Land Use Plan subject to the applicable review and approval processes contained in the Mitchell Road Corridor Specific Plan.

USES LISTED IN APPENDIX D AS CONDITIONALLY APPROVABLE. Such uses may be established on land that is located under the Approach and/or Transitional Surfaces of the ALUC's Land Use Plan provided that any application for a permit to establish such a land use shall first be submitted to the ALUC for its review, comment, and the imposition of such condition(s) of approval as it may deem appropriate to achieve compatibility with the ALUC's Land Use Plan.

Any condition(s) of approval imposed by the ALUC shall become conditions of approval by the City of Ceres in the event that the City approves the establishment of the proposed use.

However, the City Council may, pursuant to Section 21676(b) of the Public Utilities Code, overrule or modify any condition(s) of approval imposed by the ALUC by a two-thirds (2/3) affirmative vote of the City Council following a public hearing, if it makes specific findings supported by substantial evidence in the record of the public hearing that the deletion or modification of the condition(s) is consistent with the purposes of Division 9, Part 1, Chapter 4, Article 3.5 of the Public Utilities Code as stated in Section 21670.

USES NOT LISTED IN APPENDIX D. Should a land use which is not specifically listed in Appendix D be proposed for any land located under the Approach and/or Transitional Surfaces of the ALUC's Land Use Plan, the City shall, before approving any application for a permit establish such use, refer the matter to the ALUC for its determination regarding the consistency of such proposed land use with the ALUC's Land Use Plan. Should the ALUC determine that the proposed land use is either inconsistent with its Plan or conditionally approvable subject to certain conditions, it shall so notify the City.

The City Council may, after a public hearing, overrule the ALUC's finding of inconsistency, or delete or modify any condition(s) of approval imposed by the ALUC, by a two-thirds (2/3) vote of the City Council if the Council makes specific findings supported by substantial evidence in the record of the public hearing that the proposed land use, or the deletion or modification of the condition(s) of approval, is consistent with the purposes of Division 9, Part 1, Chapter 4, Article 3.5 of the Public Utilities Code as stated in Section 21670.

All applications for building permits or other entitlements within the Mitchell Road Corridor Specific Plan north of Hatch Road shall be accompanied by information that shows the location of the proposed use relative to the Approach and Transitional Surfaces established for the Modesto City-County Airport. The maps and diagrams contained in the Final Environmental Impact Report prepared for the 1990 Master Plan for the Modesto City-County Airport shall be used for this

purpose. At the discretion of the Ceres Director of Planning and Community Development, an applicant for a permit or other entitlement shall submit survey information sufficient to clarify a property's location (or the location of a proposed land use on a site) relative to the Approach and Transitional Surfaces where needed to insure that the integrity of above policy is maintained.

C. LAND USE DESIGNATIONS AND ZONING

The Mitchell Road Corridor has been divided into seven (7) Specific Plan land use designations or zones. Each represent a distinct function within the overall scope of the Specific Plan and incorporate regulations regarding the use and development of land. Each land use designation or zone contributes to the establishment and maintenance of the MRCSP's goals, objectives, and policies through the regulation of permitted uses, setbacks, landscape, signage, building height, parking, etc. The land use designations or zoning districts are as follows:¹

Specific Plan Zones

CC Community Commercial

The Community Commercial Zone is intended to accommodate those uses that serve the needs of the community for retail goods and services used on a daily basis. The stores in this zone are intended to fit into the residential pattern of surrounding neighborhoods without creating either architectural or traffic conflicts. To the extent they are not already prevented from doing so, the development in this zone is encouraged to aggregate into larger scale shopping centers.

HC Highway Commercial

The HC, Highway Commercial Zone, is intended to provide for and promote concentrations of those administrative professional, retail commercial, and service commercial uses which serve the needs of the traveling public.

IP Industrial Park

The IP, Industrial Park Zone, is intended to provide for and promote concentration of light and specialized industrial uses and administrative and research establishments. Commercial-related uses and services may be permitted. All uses shall be of a non-nuisance type, as determined by the Planning Commission.

¹ Because the MRCSP's land use designations and zones are the same they can be referred to as either land use designations or zones. However, since they take on greater regulatory significance, from here on they will be referred to as Specific Plan zones or zoning.

MX-1 Mixed Use 1 (Commercial, Residential, Business Park)

The MX-1, Mixed Use 1 Zone, is intended to provide for and promote comprehensive planning and design at a large scale for mixed uses of residential (Low, Medium, or High Density Residential with a maximum of 25 du/ac), retail and service commercial as well as business park.

MX-2 Mixed Use 2 (Residential, Business Park, Recreational)

The MX-2, Mixed Use 2 Zone, is intended to provide for and promote high quality planning and design for the mixed uses of business park, recreational and residential (Low, Medium, or High Density Residential with a maximum of 25 du/ac except within the Approach and Transitional Surfaces as established by the Stanislaus County Airport Land Use Commission; see Section B, III DEVELOPMENT REGULATIONS) in a golf course and river view setting.

RC Regional Commercial

The RC, Regional Commercial Zone, is intended to provide for and promote concentration of large-scale commercial uses such as department store, outlet stores, or big "box" retail, furniture stores and other large retail-service uses which serve the needs of the city, surrounding area, and its neighboring communities. Because of its proximity to State Route 99, the RC Zone includes automobile-oriented uses that are traditionally associated with commercial zoning adjacent to freeway interchanges. Because of its proximity to State Route 99, the RC Zone includes automobile-oriented uses that are traditionally associated with commercial zoning adjacent to major freeway interchanges.

R Residential

The R, Residential Specific Plan designation, is intended to provide for the development of housing pursuant to the City's Zoning Ordinance. Residential densities included in this designation include Low Density Residential - 5 to 7 units per net acre; Medium Density - 7 to 15 units per net acre; and High Density Residential - 15 to 25 units per net acre. Those properties designated for Low Density Residential development shall be shown as RL-7, those designated for Medium Density Residential development shall be shown as RM-15, and those designated for High Density Residential shall be shown as RH-25 on the Specific Plan Map with the appropriate zoning as established in Chapter 18 of the Ceres Municipal Code shown in parentheses following the land use designation.

D. LAND USES BY SPECIFIC PLAN ZONE

Buildings, structures and land shall be used, and building and structures shall hereafter be erected, structurally altered or enlarged in the MRCSP area (as defined herein) only for the uses indicated for each district or zone by the following table/matrix entitled "LAND USE MATRIX". Other such uses may be permitted if the Planning Director or Planning Commission find such uses to be similar to other permitted uses, consistent with the purpose of the Specific Plan land use designation and/or zone, and not more obnoxious or detrimental to the public health, safety,

welfare, and general well-being. All uses shall be subject to the property development standards in Section III F. The symbols shown in the following Land Use Matrix table are based on the definitions below:

<u>Symbol</u>	<u>Meaning</u>
p	Permitted Use - use is permitted unconditionally in the zone.
a	Accessory Use - use permitted only if accessory to an existing or proposed principle use of the same site. Depending on the scope and scale of an accessory use for a conditional use, it may be necessary to include the accessory use within the scope of the conditional use permit or to require one if no use permit has been obtained.
c	Conditional Use - use eligible for consideration under the conditional use procedure and permitted only if a conditional use permit is approved, subject to the specific conditions of such permit. Use may be conditional consequent upon area-wide site and circulation plan development (see area-wide site and circulation plan, Section III H.). A conditional use proposed to be located in an existing building or structure and that is determined by the Director of Planning and Community Development to be routine in nature, meet city development standards, and not thought to be controversial, is considered a "minor" conditional use. Application fees reflect the reduced time and effort associated with processing a "minor" conditional use permit.
adm	Administrative Use - those uses intended to be operated on a temporary or seasonal basis, including seasonal or theme oriented businesses, transient general merchandise, special event or independently operated businesses in conjunction with permanent commercial businesses and for which an administrative permit is required.
x	Prohibited Use - use expressly prohibited in the district.

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
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OFFICE USES

Bail bond offices	p	p	p	x	p	p*
Banks, financial institutions	p	p	p	x	p	p*
Business, administrative, professional	p	p	p	x	p	p*
Editorial offices	p	p	p	x	p	p*
Employment agencies	p	p	p	x	p	p*
Insurance brokers, adjusters, agents	p	p	p	x	p	p*
Law offices	p	p	p	x	p	p*
Medical offices (including dentists, opticians, chiropractic's)	p	p	p	x	p	p*
Notary publics	p	p	p	x	p	p*
Real Estate brokers	p	p	p	x	p	p*
Tax consulting services	p	p	p	x	p	p*

COMMERCIAL USES

RETAIL COMMERCIAL						
Antique stores	p	p	p	x	p	x
Apparel-clothing, millinery, etc.	p	p	p	x	p	x
Art supplies	p	p	p	x	p	x
Automotive retail/sales						
Auto parts or accessories not including tires or batteries (no second-hand sales)	p	p	p	p	p	x
Auto, RV, trailers, trucks, motorcycles sales & services, new and used (not service stations)	c	c	x	x	x	x
Tires: Retail and installation (no wholesale, retreading, or recapping ¹)	p	p	p	p	p	x
Bakeries (retail)	p	p	p	x	p	x

¹Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by residential units and/or is zoned for residential.

*Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
Bars (only) ^{1,2}	p	p	p	x	p	x
Bar-restaurants ^{1,2}	p	p	p	x	p	p*
Bicycle sales or rentals	p	p	p	x	p	x
Boat retail/sales and service	c	c	x	x	x	x
Book stores	p	p	p	x	p	x
Building materials sales yard	p	x	x	x	p	x
Carpet sales (retail)	p	p	p	x	p	x
Computer stores	p	p	p	x	p	x
Department stores and variety stores (large scale)	p	x	p	x	p	x
Discount stores	p	p	p	x	p	x
Drug stores	p	p	p	x	p	x
Electrical fixture supply stores	p	p	p	x	p	x
Electronic equipment stores (radios, televisions, stereos, cameras, etc.)	p	p	p	x	p	x
Emergency medical & sickroom sales & service facilities	p	p	p	x	p	x
Equipment/sales-agricultural, industrial & construction	c	x	c	p	c	x
Florists	p	p	p	x	p	x
Furniture stores-New/Used (retail)	p	p	p	x	p	x
Garden supply/patio furniture, masonry ¹	p	c	p	p	p	x
Gifts	p	p	p	x	p	x
Grocery stores (supermarkets)	p	p	p	x	p	x
Gun shops and repair	p	p	p	x	p	x
Hardware stores	p	p	p	x	p	x
Hobby shops	p	p	p	x	p	x

¹Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by residential units and/or is zoned for residential.

²Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by a public or private elementary, junior high or high school.

* Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
Household appliance stores	p	p	p	x	p	x
Ice cream/candy stores	p	p	p	x	p	x
Jewelry, camera & supply, luggage stores	p	p	p	x	p	x
Liquor stores ²	p	p	p	x	p	x
Meat/fish markets	p	p	p	x	p	x
Music, musical instrument & record stores	p	p	p	x	p	x
Office supply stores	p	p	p	x	p	x
Paint supply stores	p	p	p	x	p	x
Pet stores with grooming	p	p	p	x	p	x
Plumbing supplies	p	p	p	x	p	x
Pool & Spa equipment (retail)	p	p	p	x	p	x
Restaurants-fast food ¹	p	p	p	x	p	p*
Restaurants-sit down, quality	p	p	p	x	p	p*
Shoe stores	p	p	p	x	p	x
Souvenirs	p	p	p	x	p	x
Sporting goods, toys	p	p	p	x	p	x
Stationery stores	p	p	p	x	p	x
Video rentals/sales stores	p	p	p	x	p	x
Wallpaper and floor covering stores	p	p	p	x	p	x
Wholesale outlets (big box uses, home supply, furniture, carpet, etc.)	p	x	p	p	p	x
SERVICE COMMERCIAL						
Automotive service/repair						
Auto club offices with repair	p	p	p	p	p	p*
Body shops, machinery repair & painting totally within enclosed buildings	c	c	c	p	c	x
Car washes ¹	p	p	p	p	p	c*

¹Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by residential units and/or is zoned for residential.

²Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by a public or private elementary, junior high or high school.

* Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
Rental agencies ¹	p	p	p	p	p	p*
Repair garages ¹ (including oil and lube, muffler, brake shops)	p	p	p	p	p	x
Service Stations ³	c	c	c	c	c	x
Towing services	c	c	c	p	c	x
Truck/bus repair and service shops	x	x	x	p	x	x
Education						
Colleges, modeling schools						
13 or more students	c	c	c	c	c	c*
12 or less students	p	p	p	p	p	p*
Libraries, reading rooms	p	p	p	x	p	p*
Miscellaneous schools						
Gym, self defense, dance						
(13 or more students)	c	c	c	c	c	c*
(12 or less students)	p	p	p	p	p	p*
Vocational colleges-barber/beauty						
(13 or more students)	c	c	c	c	c	c*
(12 or less students)	p	p	p	p	p	p*
Health Services						
Acute care offices	p	p	p	x	p	p*
Ambulance offices ¹ (ambulance parking and overnight sleeping)	p	p	p	x	p	p*
Convalescent hospitals/nursing homes	p	p	p	x	p	x
Hospitals	c	c	c	x	c	c*
Pharmacy shops	p	p	p	x	p	p*

¹Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by residential units and/or is zoned for residential.

³Automobile service stations subject to the following specific conditions: a. A maximum of two automobile service stations shall be allowed at a cross intersection; one at "T: intersection; b. All automobile service stations shall be limited to intersections of major thoroughfare or greater as designated by the General Plan.

* Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
Personal Services						
Barbershops, beauty salons	p	p	p	x	p	p*
Dry cleaning (large scale & delivery services)	x	x	x	p	x	c*
Dry cleaning - retail	p	p	p	x	p	p*
Laundromats	p	p	p	x	p	p*
Shoe repair, tailor, dressmaker shops	p	p	p	x	p	p*
Suntanning booths-within exercise facilities and beauty salons	p	p	p	x	p	p*
Recreation						
Adult entertainment (see Section 9.42)	c	x	x	x	x	x
Archery ranges, batting cages, slot car, etc	p	p	p	p	p	p*
Billiards/pool parlors ¹	p	p	p	c	p	p*
Bowling alleys ¹	p	p	p	c	p	p*
Entertainment ¹	p	p	p	c	p	p*
Health clubs (indoor only) ¹	p	p	p	c	p	p*
Health clubs (w/outdoor activities) ¹	p	p	p	c	p	p*
Operation of amusement devices						
6 or less coin operated	a	a	a	a	a	a
6 or more coin operated	c	c	c	c	c	c*
Night clubs (with dancing and music)	c	c	c	c	c	c*
Parks & recreational facilities	p	p	p	p	p	p*
Social Halls (non public-banquets, bingo, bridge clubs, etc.)	c	c	c	c	c	c*
Skating rinks ¹	p	p	p	c	p	p*
Stadiums/arenas	c	c	c	c	c	c*
Theaters	c	c	c	c	c	c*
Social Services						
Churches	c	c	c	c	c	c*

¹Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by residential units and/or is zoned for residential.

* Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
Conference Centers	c	x	x	x	x	c*
Day nursery and nursery schools						
13 or more children	c	c	c	x	c	c*
12 or less children	p	p	p	x	p	p*
Other services						
Alarms-sales/services	p	p	p	p	p	p*
Auctions	c	c	c	c	c	c*
Bicycle repair shops	p	p	p	p	p	p*
Contractors-building, painting, landscaping, etc. (with outdoor storage)	x	x	x	p	x	x
Electricians (no outdoor storage)	p	p	p	p	p	p*
Equipment rental-agriculture, industrial, and construction	c	x	c	p	c	x
Hotels, motels, motor hotels	p	p	x	x	x	p*
Lock smiths	p	p	p	p	p	p*
Mini-storage facilities	c	c	c	p	c	c*
Mortuaries	c	c	c	c	c	c*
Pest control services	p	p	p	p	p	p*
Plumbers (no outdoor storage)	p	p	p	p	p	p*
Printing, graphic arts services: copying, addressographing, mimeographing, photostatting, instant printing	p	p	p	x	p	p*
Sign Shops (sales and fabrication)	x	x	x	p	x	x
Studio-art studios, art galleries, interior decoration, costume design, arts & crafts, Photography	p	p	p	x	p	p*
Studios-radio/television ¹	p	p	p	x	p	p*
Studios-music, recording ¹	p	p	p	x	p	p*
Upholstery shops (indoor only)	p	p	p	p	p	p*
Upholstery shops (with outdoor activity)	x	x	x	p	x	x
Warehouse wholesale/businesses	x	x	x	p	x	x

¹Where otherwise not prohibited, a conditional use permit is required if the property is located within 300 feet of a property occupied by residential units and/or is zoned for residential.

* Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
<u>INDUSTRIAL USES</u>						
All industrial uses	x	x	x	p	x	x
Assembly plants	x	x	x	p	p	p*
Awning manufacture	x	x	x	p	x	x
Cabinet shops, woodworking business including furniture manufacture/repair	x	x	x	p	x	x
Computer manufacture/maintenance/service	x	x	x	p	p	p*
Glass manufacture & glass projection, fabrication & distribution	x	x	x	p	x	x
Laundry & dry cleaning plants	x	x	x	p	x	x
Machine shops & tool & die making	x	x	x	p	x	x
Mail order & mail-out	x	x	x	p	p	p*
Metal welding & plating business	x	x	x	p	x	x
Outside storage associated with industrial & commercial uses	c	c	c	c	c	c*
Paint manufacture/packing/distribution	x	x	x	p	x	x
Parcel delivery services	x	x	x	p	p	p*
Printing & publishing	x	x	x	p	p	p*
Processing/distribution plants (including fabricating, packaging, repair & storage of: audio/video/ electronic equipment, communication equipment, food & beverages, plastic products using previously manufactured or formulated plastics, pharmaceutical products, photographic products, garments & textile products, soap & detergent)	x	x	x	p	c	c*
Recycling plants	x	x	x	c	x	x
Research & development labs/businesses	x	x	x	p	p	p*
Roofing business	x	x	x	p	x	x
Technical & industrial training schools	c	c	c	c	c	c*
Trucking companies	x	x	x	p	x	x

*Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
<u>RESIDENTIAL USES</u> ⁴						
Low-density residential (RL-7)	x	x	x	x	c	c*
Medium-density residential (RM-15)	x	x	x	x	c	c*
High-density residential (RH-25)	x	x	x	x	c	c*
Mobile homes for residential uses	x	x	x	x	c	c*
Condo-group dwellings	x	x	x	x	c	c*
<u>ACCESSORY USES</u>						
Dwelling units for caretakers	a	a	a	a	a	a*
Off-site advertising ⁵	x	x	x	c**	x	x
Off-street loading facilities	a	a	a	a	a	a*
Off-street parking facilities (public/private)	a	a	a	a	a	a*
Outdoor sales- permanent (e.g. fruit & vegetable stands, flower stands, craft stands, plants, nursery products, sporting goods, etc.)	c	c	c	c	c	c*
Propane tanks	adm	adm	adm	adm	adm	adm
Publicly owned facilities	p	p	p	p	p	p
Recreation Accessory Uses						
Commercial uses incidental to public recreational facilities (pro shop, concession stand, etc.)	p	p	p	p	p	p*
Signs which pertain only to a permitted use on the premises	a	a	a	a	a	a*
Special events and uses operated in conjunction with established commercial or industrial uses (promotional sales, carnivals, pumpkin lots, accessory food sales etc., not to exceed more than 90 days in a calendar year)	adm	adm	adm	adm	adm	adm*
Storage structures for goods sold at retail (minor enclosed structure)	adm	adm	adm	adm	adm	adm

⁴NOTE: The residential matrix addresses new residential development. The existing residential uses and are designated RL-7: R-1 and P-C; RM-15: R-2, R-3; and P-C; RH-25: R-4 and P-C on the map. These existing residential uses shall abide by current zoning requirements as specific by the Zoning Ordinance.

⁵Except temporary signs allowed by Chapter 13.42.160.C2. located at least 300 feet from other temporary signs.

*Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

**Only when located adjacent to State Route 99.

LAND USE MATRIX

KEY: p=Permitted Use; a=Accessory Use; adm=Administrative Permit c=Conditional Use; x=Prohibited Use

	RC	HC	CC	IP	MX-1	MX-2
Utilities and Communications						
Water well, pumping station, water reservoir, electrical substation	p	p	p	p	p	p
Other public utility facilities/structures	p	p	p	p	p	p
TEMPORARY USES						
Temporary uses on undeveloped lots (promotional sales, carnivals, pumpkin lots, accessory food sales etc., not to exceed more than 90 days in a calendar year)	adm	adm	adm	adm	adm	adm*
Garage sales, not to exceed 2 per year per location	x	x	x	x	x	x
Itinerant vendors, road side sales, open air sales, etc.	x	x	x	x	x	x
Fireworks stands	Fireworks stand permits issued by the Fire Marshal					

*Uses located within the Approach and Transitional surface may not be allowed and/or approval may be required by the Stanislaus County Airport Land Use Commission (see Section III, B., Land Use Limitations-Airport Approach and Transitional Surfaces).

E. GENERAL PROVISIONS

This portion of the MRCSP's development regulations require that all development projects must observe basic general regulations at a minimum in order to protect the public health, safety and welfare. These regulations apply to properties in all zones unless otherwise specified in the particular zone, under Special Provisions.

1. Height of Structure

No main building erected shall have a height greater than thirty-five feet except as provided herein. No accessory building erected shall have a height greater than fifteen (15) feet. Permitted projections above the height include: vertical accents at the building entries such as towers and penthouses or roof structures for the housing of elevators, stairways, tanks, ventilating fans, or similar mechanical equipment, fire or parapet walls, skylights, flagpoles, and chimneys, approved by the Planning Commission or the Planning Director, depending on the approval process, provided that they may be safely erected and maintained at such height in view of the surrounding conditions and circumstances and screened from view in accordance with the Design Guidelines. No penthouse or roof structures, or any space above the height limit shall be allowed for the purpose of providing additional floor space. Any area that falls within the area designated as an airport overlay zone shall comply with the height limitations prescribed by that zone, if they are more restrictive than those herein prescribed.

Structures with the Airport Approach zone shall be subject to the height limit restrictions established within the A-O Airport Overlay Zone (Sec. 18.36 et. seq. of the Ceres Municipal Code).

2. Floor Area Ratio (F.A.R.) requirements

Floor Area Ratio (F.A.R.) is the relationship between the overall area of a building and the area of the site. Generally, a limit to the FAR is established to ensure that adequate site area remains available to accommodate the required parking, setbacks, landscape requirements, and open space.

The following FAR standards are provided to guide the intensity of commercial/industrial development within the Mitchell Road Corridor Specific Plan area:

Single-story	maximum FAR = 0.55:1.00
Two-story	maximum FAR = 0.80:1.00
Three-story	maximum FAR = 0.95:1.00

The FARs above are a general target maximum to be followed; however, variances from these maximums can occur if it can be established, through site plan review, that adequate parking, setback and landscape requirements have been incorporated. FAR standards of the appropriate residential zone shall apply to residential projects.

3. Lot Area

For all new lots, the standards for lot size included in each specific plan zone shall apply: Lot sizes smaller than the minimum lot size may be approved by the Planning Commission in conjunction with a subdivision map concurrently reviewed and a use permit, or specific plan site plan approval and the Planning Commission finds that the reduction in lot size is consistent with the purpose and intent of the MRCSP.

- a. For all permitted uses, refer to SPECIFIC PLAN ZONES AND SITE DEVELOPMENT STANDARDS.

4. Lot Dimensions

For all new lots the standards for lot dimensions included in each specific plan zone. Lot dimensions smaller than the minimum lot dimension may be approved by the Planning Commission in conjunction with a subdivision map concurrently reviewed and a use permit, or specific plan site plan approval and the Planning Commission finds that the reduction in lot size is consistent with the purpose and intent of the MRCSP.

5. Minimum Space Between Buildings

When pedestrian access is provided, the minimum space between buildings must be ten (10) feet.

6. Vehicular Height Clearance

Vehicular height clearance shall be consistent with the Uniform Building Code (presently seven (7) feet).

7. Specific Plan Site Plan Approval

Before any primary building is erected on any lot, or significant exterior alteration made to an existing building, a site plan, floor plans and elevations of all buildings and a landscape plan shall be submitted to and approved by the Planning Commission. Accessory buildings that conform to the standards of the MRCSP may be approved administratively by the Planning Director. No specific plan site plan shall be required in conjunction with a proposed application for which a use permit is required, provided that the application submitted for the use permit also incorporates the requirements for a specific plan site plan.

8. Landscaping requirements

The *Ceres Water Efficient Landscape Guidelines and Standards* shall apply to the properties within the MRCSP except as specifically modified by the MRCSP document.

- a. All parking lots shall have a minimum of one (1) tree per eight (8) parking spaces.

9. Fences, hedges and walls

All nonresidential uses that abut a residential zone, an eight (8) foot high solid masonry wall and a ten (10) foot wide landscape buffer shall be required.

10. Off-street parking requirements

For all uses, the following off-street parking requirements shall apply:

- a. Automobile repair and service shops: Four (4) parking spaces for every service bay.
- b. Automobile washing and cleaning establishments including self-service: Four (4) spaces and reserve parking equal to five (5) times the capacity of the car wash; for self-service automobile washes: Two (2) spaces for each stall.
- c. Banks, savings and loans and other financial institutions: One (1) space for every 250 square feet of gross floor area.
- d. Bowling alleys: Three (3) spaces for each alley plus two (2) for each billiard table, plus one (1) space for each five (5) seats in any gallery.
- e. Clubs, discos, ballrooms, cocktail lounges, lodge, dance halls and incidental dancing areas and similar facilities where dancing is the principal use of the premises: One (1) for each seventy-five (75) square feet of gross floor area.
- f. General office uses: One (1) space for every 300 square feet of gross floor area.
- g. Hotels and motels: One (1) space for each guest unit and one (1) short-term/visitor space for every fifteen (15) rooms and one (1) space for each two (2) employees on the maximum shift, plus the number of additional spaces as may be prescribed by the Planning Director.
- h. Laundry or dry cleaning establishments, solely coin operated: One (1) for each three (3) machines.
- i. Medical/dental offices: One (1) space for each 200 square feet of gross floor area.
- j. Public utility facilities including but not limited to, electric, gas, water, telephone and telegraph facilities not having business office on the premises: One (1) space for every two (2) employees plus one (1) space for each vehicle kept in connection with the use.
- k. Restaurants, cafes, nightclubs, taverns, lounges or other establishments for the sale and consumption on the premises of food and beverages: One (1) space for each three (3) seats, plus any additional spaces as prescribed by the Planning Director.

1. Retail stores:
 - i. General retail, including discount department stores: Except as otherwise specified herein, one (1) space for every 250 square feet of gross floor area.
 - ii. Furniture and appliances: One for each 500 square feet gross floor area.
- m. Skating rink, ice or roller: One (1) for each 100 square feet of gross floor area, plus the spaces required for additional uses on the site.
- n. Uncovered sales areas, including areas for new or used automobiles or trailer sales, lumber or building material yards, plant nursery or similar uses: A minimum of five customer parking spaces for the first 5,000 square feet of uncovered sales area and one (1) customer parking space for each additional 1,000 square feet of uncovered sales area to a required maximum of 20 customer parking spaces. In addition, there shall be at least one (1) parking space for each employee.
- o. Video arcades: One (1) parking space for every three machines or one (1) parking space for every 250 square feet of gross floor area, whichever is greater.
- p. Parking requirements for unlisted uses shall be determined by the Planning Director.
- q. Parking requirements for uses which are not listed in the Specific Plan but are listed in the Ceres Municipal Code, shall be those required by the Ceres Municipal Code.

11. Off-street loading requirements

All uses that require the receipt, delivery, or distribution of goods by truck with the potential frequency of once a day or greater: one loading berth, plus such additional berths as may be prescribed by the Planning Director.

12. Access

- a. All main vehicular access to and from a property or project site shall be through streets, with a classification of primary collector or greater, as designated by the General Plan.
- b. There shall be vehicular access from a dedicated street to on-site parking facilities where on-site parking is required.
- c. All ingress to and egress from public or private property shall be in a forward motion, except for detached single-family residential uses located on appropriately zoned property.

13. Signs

All new signs shall be in accordance with the provisions of Chapter 18.42 of the Ceres Municipal Code, with the additions and/or modifications summarized below:

- a. Off-site freestanding directional signs for business centers meeting certain criteria (e.g. individually or in aggregate more than one acre in size and no major street frontage).
- b. Special provisions regarding off-site signs advertising subdivisions and/or residential developments.
- c. Standards for signs associated with the marketing of approved development projects on properties located in the Mitchell Road Corridor.

These, and the other provisions of Chapter 18.42 of the Ceres Municipal Code, are described and illustrated in the section of the Mitchell Road Corridor Design Guidelines regarding signs. Any changes to Chapter 18.42, not specifically in conflict with the MRCSP, shall automatically apply to properties within the Mitchell Road Corridor.

14. Non-Conforming Land Uses or Structures

All non-conforming land uses or structures shall be subject to Section 18.48 of the Ceres Municipal Code.

15. Miscellaneous Uses, Activities and Structures

Uses, activities, structures, etc., not controlled or identified within the Specific Plan shall be subject to the provisions of the Ceres Municipal Code.

F. SPECIFIC PLAN ZONES AND SITE DEVELOPMENT STANDARDS

The seven (7) Specific Plan zones each provide information on permitted land uses, development standards, design districts and special provisions.

The following Specific Plan zones are hereby established, with their general purpose as indicated, and classified as shown for purposes of reference in this chapter:

1. REGIONAL COMMERCIAL ZONE (RC)

a. Purpose and Intent

The RC, Regional Commercial Zone, is intended to provide for and promote concentration of large-scale commercial uses such as department store, outlet stores, or big "box" retail, furniture stores and other large retail-service uses which serve the needs of the city, surrounding area, and its neighboring communities. Because of its proximity to

State Route 99, the RC Zone includes automobile-oriented uses that are traditionally associated with commercial zoning adjacent to freeway interchanges.

b. Permitted/Conditional Uses

Buildings, structures, and land shall be used and buildings and structures shall hereafter be erected, structurally altered or enlarged in the RC zone only for the uses indicated in the LAND USE MATRIX, plus such other uses as the Planning Director or Planning Commission may deem to be similar and not more obnoxious or detrimental to the public health, safety, and welfare.

c. Development Standards

All uses shall be subject to the following development regulations: (Also see GENERAL PROVISIONS)

1. Lot Area/Dimensions

- Minimum Lot Area 27,000 square feet
- Minimum Lot Width 100 feet
- Minimum Lot Depth 270 feet

2. Buffers (Yards and Setbacks)

- Minimum Front Setback 15 feet

- Minimum Side Setback (Exterior Side) 15 feet

- Minimum Side Yard (Interior Side) 15 feet

Adjacent to nonresidential none

Adjacent to residential and building less than 35 feet in height 10 feet

Adjacent to residential and building more than 35 feet in height 20 feet

- Minimum Rear Yard

Adjacent to nonresidential none

Adjacent to residential and building less than 35 feet in height 10 feet

Adjacent to residential and building more than 35 feet in height	20 feet
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d. Design Guidelines

See Specific Plan Guidelines for the appropriate set of design guidelines based on location of property.

e. Special Provisions

- i. An area-wide site and circulation plan may be required, depending upon the location of the site and proposed development. (see Section H., Area-Wide Site and Circulation Plan) A specific plan site plan shall also be required for the actual development of the site. However, the specific plan site plan may be reviewed and approved concurrently with any required area-wide site and circulation plan.
- ii. Bonuses or incentives may be available for provision of an Activity Center (see Section I. ASSISTANCE AND INCENTIVE PROGRAM - ACTIVITY CENTERS).

2. HIGHWAY COMMERCIAL ZONE (HC)

a. Purpose and Intent

The HC, Highway Commercial Zone, is intended to provide for and promote concentrations of those administrative professional, retail commercial, and service commercial uses which serve the needs of the traveling public.

b. Permitted/Conditional Uses

Buildings, structures, and land shall be used and buildings and structures shall hereafter be erected, structurally altered or enlarged in the HC zone only for the uses indicated in the LAND USE MATRIX, plus such other uses as the Planning Director or Planning Commission may deem to be similar and not more obnoxious or detrimental to the public health, safety, and welfare.

c. Development Standards

All uses shall be subject to the following development regulations: (Also see GENERAL PROVISIONS.

1. Lot Area/Dimensions

- Minimum Lot Area 10,000 square feet
- Minimum Lot Width 100 feet
- Minimum Lot Depth 100 feet

2.	Buffers (Yards and Setbacks)	
	• Minimum Front Setback	5 feet
	• Minimum Side Setback (Exterior Side)	5 feet
	• Minimum Side Yard (Interior Side)	
	Adjacent to nonresidential	none
	Adjacent to residential	10 feet
	• Minimum Rear Yard	
	Adjacent to nonresidential	none
	Adjacent to residential	10 feet

d. Design Guidelines

See Specific Plan Guidelines for the appropriate set of design guidelines based on location of property.

e. Special Provisions

- i. An area-wide site and circulation plan may be required, depending upon the location of the site and proposed development. (see Section H., Area-Wide Site and Circulation Plan) A specific plan site plan shall also be required for the actual development of the site. However, the specific plan site plan may be reviewed and approved concurrently with any required area-wide site and circulation plan.

3. COMMUNITY COMMERCIAL ZONE (CC)

a. Purpose and Intent

The Community Commercial Zone is intended to accommodate those uses that to serve the needs of the community for retail goods and services used on a daily basis. The stores in this zone are intended to fit into the residential pattern of surrounding neighborhoods without creating either architectural or traffic conflicts. To the extent they are not already prevented from doing so, the development in this zone is encouraged to aggregate into larger scale shopping centers.

b. Permitted/Conditional Uses

2 Buildings, structures, and land shall be used and buildings and structures shall hereafter be erected, structurally altered or enlarged in the CC zone for the uses indicated in the LAND USE MATRIX plus such other uses as the Planning Director or Planning Commission may deem to be similar and not more obnoxious or detrimental to the public health, safety and welfare.

c. Development Standards

All uses shall be subject to the following development regulations: (Also see GENERAL PROVISIONS.)

1. Lot Area/Dimensions

- Minimum Lot Area 10,000 square feet
- Minimum Lot Width 100 feet
- Minimum Lot Depth 100 feet

2. Buffers (Yards and Setbacks)

- Minimum Front Setback 15 feet
- Minimum Side Setback (Exterior Side) 10 feet

- Minimum Side Yard (Interior Side)

Adjacent to nonresidential none

Adjacent to residential 10 feet

- Minimum Rear Yard

Adjacent to nonresidential none

Adjacent to residential 10 feet

d. Design Guidelines

See Specific Plan Guidelines for the appropriate set of design guidelines based on location of property.

e. Special Provisions

- i. An area-wide site and circulation plan may be required, depending upon the location of the site and proposed development. (see Section H., Area-Wide Site and Circulation Plan) A specific plan site plan shall also be required for the actual development of the site. However, the specific plan site plan may be reviewed and approved concurrently with any required area-wide site and circulation plan.

4. INDUSTRIAL PARK ZONE (IP)

a. Purpose and Intent:

The IP, Industrial Park Zone, is intended to provide for and promote concentration of light and specialized industrial uses and administrative and research establishments. Commercial-related uses and services may be permitted. All uses shall be of a non-nuisance type, as determined by the Planning Commission.

b. Permitted/Conditional Uses

Buildings, structures, and land shall be used and buildings and structures shall hereafter be erected, structurally altered or enlarged in the IP Zone only for the uses indicated in the LAND USE MATRIX, plus such other uses as the Planning Director or Planning Commission may deem to be similar and not more obnoxious or detrimental to the public health, safety, and welfare.

c. Development Standards

All uses shall be subject to the following development regulations: (Also see GENERAL PROVISIONS)

1. Lot Area/Dimensions

- Minimum Lot Area 10,000 square feet
- Minimum Lot Width 100 feet
- Minimum Lot Depth 100 feet

2. Buffers (Yards and Setbacks)

- Minimum Front Setback 15 feet
- Minimum Side Setback (Exterior Side) 10 feet
- Minimum Side Yard (Interior Side)

Adjacent to nonresidential none

Adjacent to residential	10 feet
• Minimum Rear Yard	
Adjacent to nonresidential	none
Adjacent to residential	10 feet

d. Design Guidelines

See Specific Plan Guidelines for the appropriate set of design guidelines based on location of property.

e. Special Provisions

- i. An area-wide site and circulation plan may be required, depending upon the location of the site and proposed development. (see Section H., Area-Wide Site and Circulation Plan) A specific plan site plan shall also be required for the actual development of the site. However, the specific plan site plan may be reviewed and approved concurrently with any required area-wide site and circulation plan.
- ii. Bonuses or incentives may be available for provision of an Activity Center (see Section III I., ASSISTANCE AND INCENTIVE PROGRAM - ACTIVITY CENTERS).

5. RESIDENTIAL ZONES (R)

a. Purpose and Intent:

The R, Residential Specific Plan designation, is intended to provide for the development of housing pursuant to the City's Zoning Ordinance. Residential densities included in this designation include Low Density Residential - 5 to 7 units per net acre; Medium Density - 7 to 15 units per net acre; and High Density Residential - 15 to 25 units per net acre. Those properties designated for Low Density Residential development shall be shown as RL-7, those designated for Medium Density Residential development shall be shown as RM-15, and those designated for High Density Residential shall be shown as RH-25 on the Specific Plan Map with the appropriate zoning as established in Chapter 18 of the Ceres Municipal Code shown in parentheses following the land use designation.

b. Permitted/Conditional Uses

Buildings, structures, and land shall be used and buildings and structures shall hereafter be erected, structurally altered or enlarged in the R Zone only for the uses indicated in the

City's Zoning Ordinance (RL-7: R-1 and P-C; RM-15: R-2, R-3; and P-C, RH-25: R-4 and P-C)

c. Development Standards

All uses shall be subject to the City's Zoning Ordinance as indicated on the Specific Plan Map.

d. Design Guidelines

See Specific Plan Guidelines for the appropriate set of design guidelines based on location of property.

6. MIXED USE 1 ZONE (MX-1)

a. Purpose and Intent

The MX-1, Mixed Use 1 Zone, is intended to provide for and promote comprehensive planning and design at a large scale for mixed uses of residential (Low, Medium or High Density Residential with a maximum of 25 du/ac), retail and service commercial as well as business park.

b. Permitted/Conditional Uses

Buildings, structures, and land shall be used and buildings and structures shall hereafter be erected, structurally altered or enlarged in the MX-1 Zone only for the uses indicated in the LAND USE MATRIX, plus such other uses as the Planning Director or Planning Commission may deem to be similar and not more obnoxious or detrimental to the public health, safety, and welfare. Residential development in the MX-1 zone shall require the approval of a conditional use permit and shall meet the density and site development standards of the appropriate residential zone contained in Chapter 18 of the Ceres Municipal Code (see 5. Residential Zone).

c. Development Standards

All uses shall be subject to the following development regulations: (Also see GENERAL PROVISIONS.

1. Lot Area/Dimensions

• Minimum Lot Area	10,000 square feet
• Minimum Lot Width	100 feet
• Minimum Lot Depth	100 feet

2. Buffers (Yards and Setbacks)

• Minimum Front Setback	15 feet
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• Minimum Side Setback (Exterior Side)	10 feet
• Minimum Side Yard (Interior Side)	
Adjacent to nonresidential	none
Adjacent to residential	10 feet
• Minimum Rear Yard	
Adjacent to nonresidential	none
Adjacent to residential	10 feet

d. Design Guidelines

See Specific Plan Guidelines for the appropriate set of design guidelines based on location of property.

e. Special Provisions

- i. An area-wide site and circulation plan may be required, depending upon the location of the site and proposed development. (see Section H., Area-Wide Site and Circulation Plan) A specific plan site plan shall also be required for the actual development of the site. However, the specific plan site plan may be reviewed and approved concurrently with any required area-wide site and circulation plan.
- ii. The Planning Commission may approve, disapprove or modify a mixed use project (a project that includes both residential and nonresidential activities) after receiving a staff report and holding a public hearing.

7. MIXED USE 2 ZONE (MX-2)

a. Purpose and Intent

The MX-2, Mixed Use 2 Zone, is intended to provide for and promote high quality planning and design for the mixed uses of business park, recreational and residential (Low, Medium, or High Density Residential with a maximum of 25 du/ac except within the Approach and Transitional Surfaces as established by the Stanislaus County Airport Land Use Commission; see Section B, III DEVELOPMENT REGULATIONS) in a golf course and river view setting.

b. Permitted/Conditional Uses

Buildings, structures, and land shall be used and buildings and structures shall hereafter be erected, structurally altered or enlarged in the MX-2 zone only for the uses indicated in the LAND USE MATRIX, plus such other uses as the Planning Director or Planning Commission may deem to be similar and not more obnoxious or detrimental to the public health, safety, and welfare. Residential development in the MX-2 zone shall require the approval of a conditional use permit and shall meet the density and site development standards of the appropriate residential zone contained in Chapter 18 of the Ceres Municipal Code (see 5. Residential Zone).

c. Development Standards

All uses shall be subject to the following development regulations: (Also see GENERAL PROVISIONS.

1. Lot Area/Dimensions

- Minimum Lot Area 7,500 square feet
- Minimum Lot Width 75 feet
- Minimum Lot Depth N/A

2. Buffers (Yards and Setbacks)

- Minimum Front Setback 15 feet
- Minimum Side Setback (Exterior Side) 10 feet
- Minimum Side Yard (Interior Side)

Adjacent to nonresidential none

Adjacent to residential 10 feet

- Minimum Rear Yard

Adjacent to nonresidential none

Adjacent to residential 10 feet

d. Design Guidelines

See Specific Plan Guidelines for the appropriate set of design guidelines based on location of property.

e. Special Provisions

- i. An area-wide site and circulation plan may be required, depending upon the location of the site and proposed development. (see Section H., Area-Wide Site and Circulation Plan) A specific plan site plan shall also be required for the actual development of the site. However, the specific plan site plan may be reviewed and approved concurrently with any required area-wide site and circulation plan.
- ii. Landscaping requirements
All parking lots shall have a minimum of one (1) tree per 6 parking spaces.
- iii. The Planning Commission may approve, disapprove or modify a mixed use project (a project that includes both residential and nonresidential activities) after receiving a staff report and holding a public hearing.
- iv. Height limits are subject to the A-O, Airport Overlay District, Section 18.36 in the Ceres Municipal Code.

G. AREA-WIDE SITE AND CIRCULATION PLAN OR AREA PLAN

The intent of the area-wide site and circulation plan process is to look beyond the limits of a particular property to improve circulation, parking and neighborhood compatibility. The purposes of the area plan are to address the special or unique needs or characteristics of certain areas designated by this Specific Plan, to assure a harmonious relationship between existing and proposed uses, and to coordinate and promote improvement efforts of both private and public interests.

1. Applicability

- a. Area-wide site and circulation plan may be required in conjunction with proposed development on properties designated as needing Large-Lot Performance Standards, indicated by the numeral "1" on the Land Use Plan. Area plans may also be required in conjunction with proposed development on properties designated as needing Internal Circulation Performance Standards, indicated by the numeral "3" on the Land Use Plan.
- b. All development within the area encompassed by an approved area-wide site and circulation plan shall comply with the area-wide site and circulation plan.

2. Administration

- a. An area-wide site and circulation plan shall be required in conjunction with a proposed development when the Planning Director or Planning Commission determines that it is needed to comply with either the Large-Lot Performance Standard, Internal Circulation Performance Standard, or both. Where a proposed development does not adversely affect

any of the conditions or circumstances addressed by either performance standard an area plan shall not be required. The Planning Director's decision on whether an area plan is required may be appealed to the Planning Commission.

- b. Where required, an area-wide site and circulation plan shall be submitted to the city Planning Department for its review and recommendation to the Planning Commission. The Planning Commission may approve, conditionally approve or disapprove an area plan. The Planning Commission's decision on an area plan may be appealed to the City Council. Area-wide site and circulation plans shall be adopted by resolution. Applicants shall submit preliminary area plans for review and comment by the City through the pre-development review process prior to final preparation of an area plan. Comments from other City departments shall be sought by the staff prior to preparing a recommendation on the finalized area-wide site and circulation plan. Area-wide site and circulation plan shall indicate, through graphics and text, conceptual building locations, and orientation thereof to the site, overall circulation (vehicular and pedestrian), points of ingress and egress to both public and private streets, parking lot locations and configurations, landscaping, plazas, sign locations, and open space areas. Areas with potential for common use, such as shared access or reciprocal parking, shall also be identified.

Prior to any development, a specific plan site plan or Use Permit must be approved by the Planning Commission unless exempt from these requirements. Specific plan site plans shall be consistent with any adopted area-wide site and circulation plans.

3. Performance Standards

It is not the intention of the area planning process to limit the flexibility to develop alternative design solutions to problems consistent with the intent of the area plan and this Specific Plan. It is the intent of the area plan process to address those issues that are unique to properties within the Specific Plan area. Within certain areas identified on the Specific Plan area plans must address the following performance standards.

a. Large Lot Performance Standards

To maintain the integrity of the large lot areas along the Mitchell Road Corridor, the indicated large lots are to be treated as one, cohesive planning area. The integration of all uses within the lot must be planned for and the design guidelines and development regulations herein shall provide the basis for future development.

The large lot shall remain an integrated planning area regardless of the number of future owners and/or lessees. The use of cooperative agreements or easements shall be a tool by which multiple ownership can be accommodated as a single entity.

The achievement of goals, objectives, and policies, as outlined in this Specific Plan, shall be the same goals, objectives, and policies, if they are applicable, of the area-wide site and circulation plan. This allows the site to develop as a unit, rather than as an incongruous and disjointed body of developers. Multiple ownership and development may or may not

achieve the aforementioned Specific Plan objectives that the scale of the large lot makes possible.

The large lots should be planned with the design themes that are consistent with the particular Specific Plan's Design Guidelines, either North Gateway, Highway Commercial or South Gateway Design District. The area plans should also be responsive to overall Corridor strategies, especially circulatory needs and maximization of commercial potential.

b. Internal Circulation Performance Standards

To alleviate congestion along Mitchell Road areas designated area-wide site and circulation plan areas which must meet Internal Circulation Performance Standards. The flexibility of the plan allows for a variety of approaches and numerous site configurations. However, the restrictions imposed by the physical constraints of Mitchell Road necessitate a pattern of development which has the fewest adverse impacts upon traffic along the road.

Access to properties located along Mitchell Road will need to be carefully considered, if it is to successfully accommodate high volumes of through traffic and provide access to fronting commercial properties. To meet this challenge it will be necessary to: 1) limit the number of properties with direct access to Mitchell Road in favor of fewer access drives that serve multiple properties or developments; 2) require that properties share parking and access through common access/parking plans and reciprocal agreements; 3) take access from alternative locations such as side streets or shared driveways; and 4) develop a system of parallel access ways or public streets that permit north-south movement without having to use Mitchell Road.

These approaches access and internal circulation need to be reflected in the area plans for those properties where the MRCSP calls for Internal Circulation Performance Standards. Depending on the complexity of the circulation and traffic issues involved, it may be necessary to have a traffic engineer analyze and design the circulation system, particularly when these issues involve traffic signals, stop signs, and/or complimentary alignments on the opposite side of Mitchell Road or intersecting side street.

Within the central portion of the Mitchell Road Corridor, between Hatch Road and Roeding Road, the Specific Plan anticipates that properties will be developed with the main commercial buildings located in the front with parking in the rear. Other secondary commercial buildings and/or residential development where appropriate, would be located in the rear and served by a common private access way and driveways that were carefully located and limited in number. Figure 6 provides a graphic example of the relationship between land use and circulation that reflects these concepts outlined above. Figure 7 provides a standard for the private access ways that will provide integrated circulation.

These approaches to internal circulation, limited and shared access are not only Specific Plan concepts but required Mitigation Measures from the MRCSP Final EIR. These

requirements are necessary for Mitchell Road to carry high volumes of through traffic and provide access to commercial development on fronting properties without unacceptable traffic congestion. Residential development in these areas should be utilized as an incentive or a bonus upon the development and implementation of a area plan which meets the objectives of the Specific Plan. Multi-family residential development could be developed in the rear reaches of these large lots, thus utilizing the internal parking and circulatory facilities servicing the commercial structures along Mitchell Road. Residential may be developed only after the Commercial/Business areas are under construction.

4. Authority

Upon determination that the area-wide site and circulation plan complies with the provisions of the Specific Plan Design Guidelines and site development regulations, the area plan shall be submitted to the Planning Commission for its consideration. The Planning Commission shall then hold a public hearing on such project and shall approve or conditionally approve or deny the plan. Area-wide site and circulation plans shall be adopted by a resolution of the Planning Commission. The Planning Commission's decision to approve, conditionally approve or deny the plan may be appealed to the City Council. Public hearing procedures shall be governed by the City's Zoning Ordinance, Chapter 18.46.

5. Findings

In addition to the findings required for the approval of the related development application, the Planning Commission shall specifically find that the area-wide site and circulation plan proposed for the area is consistent with the purpose and intent of this Specific Plan. Moreover, the Planning Commission shall find that the proposed area-wide site and circulation plan adequately addresses the planning issues as identified in III.G.3.B, above.

H. ASSISTANCE AND INCENTIVE PROGRAM - ACTIVITY CENTERS

1. Criteria

Two activity center foci are envisioned by this plan. The first, opportunities for recreation oriented uses in the MX-1 Zone which would respond to the needs of business travelers utilizing the Modesto City-County Airport north and/or proximity to the Tuolumne River and nearby golf course. The second is a shopping center in the South Gateway Design District which responds to region wide market demands for retail goods and services.

The two activity centers would provide an anchor to development which is consistent with this Plan. Siting for the two centers has been suggested, but this represents approximate sites which may or may not respond to unique development strategies. The flexibility of this Plan allows for varied and even multiple sites within the particular district, as long as the Activity Center conforms to the goals, objectives, policies, and standards of this Specific Plan. Specifically, the project must respond to the following criteria:

- o Development Regulations;

- o Market Appropriateness;
- o Design Objectives;
- o Building location and relationship to on-site and off-site structures;
- o Parking - adequacy of spaces and quality of design;
- o Combined overall implementation of the district's visual intent.

Degree of responsiveness is and will always be a matter of judgment. However, failure to address all the required criteria is not. They must all be addressed. In order to qualify for incentives, the plan must positively document how it responds to all of the above criteria. And, if in the opinion of the Planning Commission, an activity center augments, and does not detract, from the intent and purpose of this Specific Plan, the center is eligible for incentives and assistance.

2. Program Actions

Provided that all criteria above are satisfied and, to the degree that they are satisfied, any or all of the following actions may be taken by the Planning Commission as either assistance or incentive programs to achieve activity centers. The extent of each individual action or combination of actions taken may vary on a case-by-case basis and will be the result of negotiations between the project applicant and the city.

- a) Parking reductions - relaxation of required parking standards so long as adequacy of proposed parking can be documented;
- b) City loan for infrastructure improvements - partial or total funding of required infrastructure improvements, subject to recovery on a mutually agreeable pay-back schedule;
- c) Non-residential intensity increase - added square footage up to a maximum of 20% above the prescribed density;
- d) Residential development where such development is allowed under the goals, policies and programs of the MRCSP.

IV. SPECIFIC PLAN IMPLEMENTATION

A. PURPOSE AND INTENT

Development will be implemented in conformance with the regulations and guidance contained within the MRCSP. This section contains the procedures for administration of the provisions contained herein. This section also contains the programs and comprehensive funding for the projected sequence of development. Implementation of the plan will further be carried out by a method of site plan review as outlined in Section IV, C. Other information covered in this chapter pertain to general administration and amendment procedures, and the linkage between these elements. In addition to specific plan site plan review, the MRCSP shall be implemented through the subdivision process. Tentative maps or parcel maps shall be required where properties are to

be separately financed, sold, leased or otherwise conveyed. Where vesting maps are proposed, all discretionary permits and/or approvals must first be approved by the City. All discretionary permits and/or approvals shall be submitted and processed concurrently unless waived by the Planning Director.

An economic analysis was prepared in conjunction with the preparation of the MRCSP to determine the demand for retail development by analyzing the population and income patterns within a specific trading area. (See Technical Appendices) The population profile and income characteristics of this area have been ascertained utilizing available or generated data.

The demand for office development within the area has been analyzed on the basis of projected growth and on the availability of suitable office space and the historical development patterns, transportation networks, population and employment concentrations.

Data collected and analyzed profiling the competitive areas included: lease rates; land values; current absorption activity; proposed development; and competitive positioning.

A quantitative assessment of the historical performance of the retailers operating in the area was performed in order to evaluate their sales relative to local and state norms. The performance evidenced by these businesses was analyzed in terms of the available support within the trade area and the competitive supply.

B. MONITORING PROCEDURES

Legislation adopted in 1988 (AB3180, Cortese) amending the California Environmental Quality Act, requires public agencies to establish Monitoring Programs to ensure effective implementation of mitigation measures presented in the CIR. Monitoring procedures for projects within the Specific Plan area will be established on a project basis and will be administered by the City in accordance with AB 3180 and/or related guidelines.

C. SITE PLAN REVIEW PROCEDURES

Requirements for specific plan site plan

A specific plan site plan shall consist of plans, drawings, illustrations, and designs, and any other detailed information as required to determine compliance with the provisions of the Specific Plan and responsiveness to design guidelines. The following list of plans and information is required:

1. Assessor's parcel(s) numbers.
2. Area and dimensions of the property.
3. Vicinity map indicating project location.
4. North arrow/scale.

5. All applicable tentative subdivision maps or tentative parcel maps.
6. A physical description of the site, including boundaries, easements, existing topography, natural features, trees, existing buildings, structures and utilities.
7. Location, grade widths and types of improvements proposed for all streets.
8. A site plan showing location of all structures, landscape and hardscape areas, parking areas, walks, internal circulation, access, adjacent streets, sign type and placement and fence/wall type and placement.
9. Building elevations.
10. Description of the extent to which design guidelines have been used in the plan and a statement documenting Specific Plan consistency.
11. A tabulation of square footage, area devoted to parking, parking spaces, landscape coverage, building coverage and heights.
12. Such applications and environmental assessment forms as are provided by the City staff.

Mandatory Findings for Approval of a Specific Plan Site Plan

The Planning Commission shall make the following written findings before approving or conditionally approving any specific plan site plan.

1. The proposed project is compatible with other projects within the Specific Plan area.
2. The proposed plan will not have an adverse impact on the public health, safety, interest, convenience or general welfare. In the event there is a significant adverse impact, the site plan may be approved if a Statement of Overriding Considerations is made pursuant to Section 15093 of the CEQA Guidelines.
3. The specific plan site plan is compatible with the regulations, and design guidelines of the Mitchell Road Corridor Specific Plan, consistent with any adopted area-wide site and circulation plan, and the site plan conforms to the General Plan and implementing ordinances.

D. GENERAL ADMINISTRATION

The Mitchell Road Corridor Specific Plan shall be administered and enforced by the City of Ceres Planning and Community Development Department in accordance with the provisions of the Ceres Municipal Code.

Certain changes to explicit provisions in the Specific Plan may be made administratively by the Planning Director, subject to appeal to the Planning Commission and, subsequently, the City Council.

1. The addition of new information to the Specific Plan maps or text that does not change the effect of any regulations or guidelines.
2. Changes to the community infrastructure, such as drainage, water, and sewer systems which do not have the effect of increasing or decreasing development capacity in the Specific Plan area, nor change the concepts of the Plan.
3. The determination that a use be allowed which is not specifically listed as permitted but which may be determined to be similar in nature to those uses explicitly listed as permitted.

All discretionary permits and/or approvals shall be submitted and processed concurrently unless waived by the Planning Director. When more than one discretionary permit and/or approval is required and authority rests with both the Planning Commission and City Council the final action on all discretionary permits or approvals shall be taken by the City Council. In such cases, the Planning Commission's actions shall consist of recommendations to the City Council.

E. AMENDMENT PROCEDURES

In accordance with the California Government Code Sections 65453-65454, specific plans shall be prepared, adopted and amended in the same manner as general plans, except that specific plans may be adopted by resolution or ordinance. This plan may be amended as necessary in the same manner it was adopted, by ordinance. Said amendment or amendments shall not require a concurrent general plan amendment unless, it is determined by City Staff that the proposed amendment would substantively affect the General Plan Goals, objectives, policies or programs or otherwise result in an inconsistency between the two documents. An environmental assessment form shall accompany the proposed amendment, but it is presumed that the master EIR approved for the project area includes all future development for the Specific Plan. If further environmental documentation were required, in special cases, it would be a focused analysis and action as documented in Section 15162 and 15182 of the CEQA guidelines.

Minor modifications in the boundaries and acreage of planning areas or adjustments because of final road alignments specified by the City will occur during technical refinements in the tentative tract map process and shall not require an amendment to the Specific Plan.

F. SPECIFIC PLAN FEES

The City of Ceres in the development review and adoption of the Specific Plan incurred costs and has established a Specific Plan fee based upon the following formula:

$$\text{Fee} = A [M (1+bn)]$$

where:

A= Number of gross acres in the project measured to the centerline of each adjoining street.

M= The city's average cost per gross acre of preparing the Specific Plan (including EIR costs).

b= The project annual inflation rate. If for example, the annual inflation rate is projected to be 6%, then $b = 0.06$.

n= Number of years since the Specific Plan adoption.

The above fee is prorated based on the estimated relative benefit that the project sponsor derives from the Specific Plan.

The Specific Plan fee shall be collected at the issuance of a building permit.

The Specific Plan fee shall be calculated during the specific plan site plan review. The final fee amount shall be calculated with the issuance of the building permit.

G. DEVELOPMENT FEES AND FINANCING OF PUBLIC FACILITIES

The City of Ceres has established and may from time to time establish development fees which are needed to continue service to the residents and businesses within the community.

Public facilities include by are not limited to: bridges, traffic signals, storm drainage facilities, major street improvements, public safety facilities, libraries, city administration building, corporation yards and facilities, wastewater treatment facilities, water source, equipment and furnishings, street trees, public art and sculptures, etc.

Public services include but are not limited to: transportation and circulation systems, delivery of services, sewer, water, recreation, aesthetically pleasing environment including trees, parks, art and sculpture, storm drainage and flood control, lighting for safe street, pedestrian facilities, etc.

1. All developments shall pay their pro-rata share of continued improvements of all major streets and the signalization of major intersections as a condition of approval.
2. All development shall be responsible for all on-site improvements as well as other fees as may be imposed in accordance with the previous policy.
3. All development to pay its pro-rata share to cause the expansion and construction of storm drainage facilities. Such payment may include cash, construction of storm drainage facilities and/or retention of storm water on-site.
4. Require developers to construct water lines of adequate size to provide water for water suppression.
5. Require developers and/or builders to pay their fair share of the cost for providing adequate water supply.

6. The wastewater treatment plant shall be expanded to retain the capability to treat wastewater at a ratio of 125 gallons/day/person. This includes the flows for commercial and industrial users. Expansion or new construction to maintain this level of service shall be paid on a pro-rata share through fees and/or assessment districts or other methods of financing as may be adopted by the City Council.
7. Each commercial, industrial and residential lot created shall make payment of sufficient fees to cause the installation of an urban street tree as to the type and size as identified within the Street Tree Master Plan.
8. All development shall pay its fair share of the cost to construct needed public facilities and to maintain an acceptable level of services as established by the City.
9. All development shall construct all wastewater lines to permit transport of wastewater from point of generation to the wastewater treatment plant or appropriate disposal point.
10. Where adequately sized lines exist, the development shall pay its pro-rata share for previous installation by the City or other developers and development.
11. Sizing of mains to accommodate future growth and development shall be in accordance with the sewer system master plan.
12. All developments shall pay their pro-rata share of costs for improving and/or construction of public facilities.

Development fees shall be collected via assessment districts, benefit districts, fees based upon connection to sewer or water, fees for street trees, fees for urban sculptures and landscaping, and other financing methods as may be established to provide for continued maintenance of level of service required for the public good.

H. SPECIFIC PLAN ENFORCEMENT

Violation of the requirements of the Specific Plan adopted by the City of Ceres, City Council constitutes a violation of the zoning ordinances. Enforcement shall be that as prescribed in the Ceres Municipal Code.

V. APPENDICES

APPENDIX A. GENERAL PLAN CONSISTENCY

The Ceres General Plan is the applicable plan for the Mitchell Road Corridor area. Section 65454 of the California Administrative Code requires that the Specific Plan be consistent with and implement the provisions of the General Plan. The following discusses the relationship between the policies and provisions of the Specific Plan and the Ceres General Plan (CGP). The discussion is keyed around each relevant policy of the CGP which is presented first, followed by relevant policies of the Mitchell Road Corridor Specific Plan if available, and finally, a discussion of the Specific Plan provisions and characteristics to illustrate how those policies will be implemented. It should be noted that certain policies of the CGP are not applicable to the Mitchell Road Corridor Specific Plan (MRCSP) either because the subject is not related to the proposed development or because the geographic location of the Specific Plan property does not correspond to the General Plan policy. Non-applicable policies are listed at the end of this section.

Land Use and Circulation Element

URBAN SERVICE BOUNDARY

CGP POLICY: Provide for balanced, orderly, efficient and reasonable urban growth where urban services are available and can feasibly be provided by the city (CGP/Urban Service Boundary Policy 1).

MRCSP POLICY: Evaluate the community-wide impact of full build-out of the Specific Plan area on the need for public facilities and services (Policy P8).

PROVISIONS: The Environmental Impact Report prepared for the Specific Plan identifies the community-wide impacts of the plan. The EIR provides mitigation measures for all impacts identified.

CGP POLICY: Compliment the goals and policies of the Stanislaus County General Plan that encourage the county to provide exclusively rural services and the city to provide exclusively urban services (CGP/Urban Service Boundary Policy 4).

MRCSP POLICY: Determine whether the City's infrastructure system will adequately serve full build-out of the Specific Plan development (Policy P9).

PROVISIONS: The environmental Impact Report (EIR) will assist the city in its evaluation of the plan's impacts on the provision of infrastructure to the Specific Plan area.

RESIDENTIAL LAND USE:

CGP POLICY: Provide for residential development at efficient urban densities (CGP/Residential Land Use Policy 4).

MRCSP POLICY: Establish residential densities that are consistent with surrounding land uses (Policy P10).

PROVISIONS: The development standards regulate the type of residential development allowed within each zone of the corridor.

CGP POLICY: Land uses within the "Residential Agriculture" area should be developed to comply with the General Plan Noise Element. The Modesto City-county Airport Master Plan and the Stanislaus County Airport Land Use Commission Plan (Policy 9).

MRCSP POLICY: Prepare a land use plan and associated development regulations which reflect the goals of other applicable plans, the character of the existing environment and the goal of the Specific Plan (Policy P11).

PROVISIONS: The Specific Plan was prepared with consideration of the constraints associated with the Modesto City-County Airport. The development regulations restrict noncompatible land uses in this area.

COMMERCIAL LAND USE:

CGP POLICY: Encourage a wide variety of retail and service commercial uses at convenient and proper locations throughout the General Plan area (CGP/Commercial Land Use Policy 1).

MRCSP POLICY: Designate the area from State Highway 99 to Roeding Road for highway-oriented commercial uses (Policy P17). Maximize highway commercial are for uses that have a regional market attraction such as theaters, department stores, membership stores, etc. (Policy P18). Designate the area from Roeding Road to Hatch Road for community-oriented commercial uses (Policy P20). Permit uses [within the Community Commercial designation] that primarily serve Ceres and the immediate vicinity (Policy P21). Designate the area from Hatch Road to Tuolumne River for uses that capitalize on the proximity to the airport and the Tuolumne River such as recreation-related uses, airport-related business, conference centers, offices uses (Policy P25).

PROVISIONS:	The Mitchell Road Corridor will be developed as an intense commercial corridor with a balance of freeway, highway and neighborhood commercial uses with opportunities for industrial, business park and residential uses. The development standards for each land use designation provide sufficient flexibility to anticipate future needs and to achieve compatibility among on-site and adjacent land uses.
CGP POLICY:	Prevent uncontrolled and inappropriate strip commercial uses along major street frontages (CGP/Commercial Land Use Policy 2).
MRCSP POLICY:	Provide buffers and transitions between commercial uses and adjacent non-commercial uses (Policy P4). Provide incentives and measures to avoid breaking up of existing large parcels into small, separate, unrelated development projects (Policy P7). Require architectural, landscaping and hardscaping statements that distinguish the land uses around the SR99-Mitchell Road interchange as a major entryway to the City of Ceres (Policy P19). Permit residential uses when proposed in conjunction with commercial projects (Policy P23). Require architectural, landscaping, and hardscaping treatments which reinforce the Mitchell Road Corridor's importance as an entry-way to the City of Ceres (Policy P26).
PROVISIONS:	The Design Guidelines include recommendations for development and design of the corridor. The Specific Plan divides the corridor into three distinct design districts: South Gateway District, Highway Commercial and North Gateway. The Design Guidelines specify that the transition between these districts will be gradual, generally following a more urbanized character to a more natural character when traveling south to north. The intent of the Guidelines is to provide a sense of cohesiveness as one travels the corridor, yet preempt monotony and enhance each area's unique features. In addition, the requirement for area-wide site and circulation plan for specified areas along the corridor will guard against the breaking up of large lots. Areas designated for area-wide site and circulation plan areas, must also meet Internal Circulation Performance Standards. The intent of such standards is to alleviate congestion along Mitchell Road by creating an internal parking and circulatory system, thereby reducing curb-cuts to Mitchell, within these areas.

CGP POLICY:	Encourage high quality commercial developments that do not adversely affect surrounding land uses (CGP/Commercial Land Use Policy 3).
MRCSP POLICY:	Provide measures which will enhance the visual quality of the Mitchell Road Corridor by concealing unsightly uses, equipment, i.e., screening of roof top equipment and under-grounding of utilities (Policy P1). Provide buffer and transitions between commercial uses and adjacent non-commercial uses (Policy P4). Permit residential uses when proposed in conjunction with commercial projects (Policy P23).
PROVISIONS:	Areas of the Specific Plan where different land use abut must adhere to development standards and specific design guidelines that minimize negative impacts on adjoining properties (i.e., landscape buffers and masonry walls and fences).
CGP POLICY:	The interchange at Mitchell Road and State Highway 99 should be developed as the City's major Highway Oriented Commercial Center (CGP/Commercial Land Use Policy 8).
MRCSP POLICY:	Designate the area from State Highway 99 to Roeding Road for highway-oriented commercial uses (Policy P17). Maximize highway commercial area for uses that have a regional market attraction such as theaters, department stores, membership/wholesale stores, etc. (Policy P18).
PROVISIONS:	The development standards for the southern end of the study area, between Highway 99 and Roeding Road, generally provides for concentrations of retail and service commercial uses such as automobile, travel and entertainment related uses which serve the needs of the traveling public. This area is visualized as a major commercial gateway to the city.
CIRCULATION POLICIES:	
CGP POLICY:	Provide for the safe, efficient and convenient movement of people and goods throughout the planning area (CGP/Circulation Policy 1).
MRCSP POLICY:	Provide a circulation concept that maximizes land use potential while minimizing traffic conflicts (Objective 4). Incorporate innovative circulation concepts which encourage public transit and other circulation alternatives to Mitchell Road (Policy P5). Require internal circulation patterns [within the Community Commercial

designation] to reduce traffic impacts on Mitchell Road (Policy P22).

PROVISIONS:

The Development Regulations in guiding development within the Specific Plan area intend to expand the traffic handling capacity of arterial highways, local streets and develop alternate circulatory routes to accommodate new development and redevelopment of existing area. The Design Guidelines establish specific provisions for vehicular travel, mass-transit, and paths and trails. Specific Plan Guidelines also proposed the enhancement of key gateways to announce arrival into Mitchell Road and each of the projects within.

Noise Element

GENERAL RECOMMENDATIONS

CGP POLICY:

The Noise Element recommends that in order to reduce or eliminate noise/land use conflicts, emphasis should be placed on reducing such conflicts through noise compatible land use planning. However, these efforts prove to be infeasible, the Noise Element recommends that mitigating measures be employed at site development/or redevelopment.

MRCSP POLICY:

Provide buffers and transitions between commercial uses and adjacent non-commercial uses (Policy P4).

PROVISIONS:

The Specific Plan's development standards direct development in a way which avoids or minimizes potential land use conflicts among on-site and adjacent land uses. The Plan's Design Guidelines provide more specific methods to reduce conflicts between different adjacent commercial or industrial park uses, residential uses and walls and fences are required in order to attenuate noises generated by major streets and specified and uses.

Open Space and Agriculture Element

CGP POLICY:

Preserve open space land within the city's 20-year planned urban development area for parks and recreational use and provide open spaces which break the monotony of continuous building, construction or urban development (CGP/Open Space/Agriculture Policy - page 1, paragraph 3).

MRCSP POLICY:

Provide measures which will enhance the visual quality of the Mitchell Road corridor by concealing unsightly uses, equipment, i.e., screening of roof top equipment and under-grounding of

utilities (Policy P1). Preserve existing significant trees (Policy P2). Provide buffers and transitions between commercial uses and adjacent non-commercial uses (Policy P4). Permit uses which capitalize on the proximity to the airport and Tuolumne River such as recreation-related uses, airport related businesses, conference centers, office uses (Policy P25).

PROVISIONS:

Specific Plan Design Guidelines provide recommendations for a range of amenities along the corridor including paths and trails, gardens, courtyards and other outdoor gathering areas. Areas for more intensive recreational activities will be centralized in the northern portion of the study area.

CGP GOAL:

Provide for future urbanization, as needed, while maintaining open space areas within the city (CGP/Open space/Agriculture: Goal A).

MRCSP POLICY:

Assure compatibility between park and open space areas and any future development within the Specific Plan Area (Policy P12).

PROVISIONS:

The Specific Plan's Development Standards for the Mixed Use Zone #2 within the Airport Overlay Zone provides for and promotes high quality planning and design for the mixed uses of business park, recreational and residential in a golf course and water view setting.

CGP POLICY:

Encourage urban development which provides open space areas, including provisions for cluster and planned unit developments (CGP/Open space/Agriculture Program B).

MRCSP POLICY:

Incorporate open space areas into the design of development protects proposed for the corridor when feasible (Policy P13).

PROVISIONS:

Specific Plan Design Guidelines provide for pedestrian walkways along major streets and connector streets, which connect developments through natural open space areas.

CGP GOAL:

Provide for a variety of outdoor recreation activities within the Ceres urban areas (CGP/Open Space/Agriculture Goal C).

PROVISIONS:

The Specific Plan provides the opportunity for passive recreational uses along the three planning districts, with more extensive recreational activities occurring at the northern end of the corridor near the Tuolumne River and the golf course. The Design Guidelines encourage bicycle lanes along secondary roadways and

	canals, in addition to the development of courtyards, atriums, and other outdoor gathering and eating areas.
CGP OBJECTIVE:	Control development in geological hazard area.
MRCSP POLICY:	Ensure the public's health, welfare and safety through design and building development standards (Policy P14).
PROVISIONS:	The environmental Impact Report will assist in the City's investigation of potential geological hazards within the Specific Plan area.
Scenic Highway Element	
CGP POLICY:	Encourage those land uses along the River Road Parkway which will enhance the "Parkway atmosphere" (CGP/Scenic Highways Element Recommendation).
MRCSP POLICY:	Require architectural, landscaping and hardscaping treatments which reinforce this area's importance as an entry way to the City of Ceres and are sensitive to the River Parkway environment (Policy P26).
PROVISIONS:	Specific Plan Design Guidelines establish standards and recommended ambiance for the area along the river.
CGP POLICY:	Provide for more extensive landscaping requirements, great setback, etc. for those uses established along the parkway (CGP/Scenic Highways Element Recommendation).
MRCSP POLICY:	Require architectural, landscaping and hardscaping treatments which reinforce this area's importance as an entry way to the City of Ceres and are sensitive to the River Parkway environment (Policy P26).
PROVISIONS:	Specific Plan Design Guidelines establish standards and recommended ambiance for the area along the river.

APPENDIX B. LEGAL DESCRIPTION

That portion of Sections 1, 2, 11, 12, 13, 14, 23 and 24, Township 4 South, Range 9 East, Mount Diablo Base and Meridian, City of Ceres, County of Stanislaus, State of California described as follows:

Beginning at the Northeast Corner of Parcel 2 of Block 847 of the City of Ceres, as shown in Book 8 of Parcel Maps at Page 23, Stanislaus County Records; thence Southerly along the Easterly line of said Parcel 2, a distance of 1258.30 feet more or less, to the Southeast corner of said Parcel 2; thence southerly a distance of 154.0 feet more or less, in a straight line to the Northeast Corner of the Harvest Glen Subdivision of the City of Ceres, as shown in Book 33 of Maps at Page 35, Stanislaus County Records; thence southerly along the Easterly line of said Harvest Glen Subdivision a distance of 569.49 feet more or less to the Southeast corner of said Harvest Glen Subdivision; thence Easterly along the Northerly line of the Rosewood Tract Subdivision of the City of Ceres, as shown in Book 27 of Maps at Page 71, Stanislaus County Records, to the northeast corner of said Rosewood Tract Subdivision; thence southerly along the prolongation of the Easterly line of said Rosewood Tract Subdivision, to the Southeast corner of said Rosewood Tract Subdivision; thence Westerly along the Southerly line of said Rosewood Tract Subdivision, a distance of 418.50 feet more or less to the Northeast corner of Parcel "B" as shown in Book 13 of Parcel Maps at Page 120, Stanislaus County Records, thence southerly along the Southerly prolongation of the Easterly line of said Parcel "B", a distance of 1354.79 feet more or less to the southerly right of way line of Fowler Road; thence Easterly along the prolongation of the Northerly line of Lots 12 and 13 Block 668, of the City of Ceres, Wood Acres Unit 2 Subdivision, as recorded in Book 23 of Maps at Page 17, Stanislaus County Records, to the Northerly prolongation of the Easterly line of said Lot 12; thence southerly along the Southerly prolongation of the Easterly line of Lots 11 and 12 of Block 668 of said Wood Acres Unit 2 Subdivision, to the Easterly Prolongation of the Southerly line of said Lot 11; thence southerly a distance of 52 feet, more or less, to the Northeast corner of Lot 8 of Block 669 of said Wood Acres Unit 2 Subdivision; thence Southerly along the Easterly line of Lots 2, 3, 4 and 8 of Block 669 to the Easterly corner of said Wood Acres Unit 2 Subdivision; thence Westerly along the Northerly line o Lot 14 of Block 670 of the City of Ceres, Wood Acres Unit 1 Subdivision, as recorded in Book 21 of Maps at page 33, Stanislaus County records, a distance of 1.33 feet to the Northwest Corner of Lot 14 of Block 670 of said Wood Acres Unit 1 Subdivision; thence southerly along the Westerly line of said Lot 14 to the Southwest Corner of said Lot 14; thence southerly in a straight line of the Northwesterly beginning of curb return of Lot 8 of Block 670 of said Wood Acres Unit 1 subdivision, also being the Westerly right of way line of Sushan Drive; thence southerly along the westerly line of lots 1, 2, 3, 4, 5, 6, 7 and 8 of Block 670 of said Wood Acres Unit 1 Subdivision to the Southerly line of said Lot 1 of Block 670; thence southerly, in a straight line to the Northwest corner of Lot 11 of Block 514 in the City of Ceres, Morrow Village No. 5 Subdivision, as recorded in Book 18 of Maps at Page 49, Stanislaus County Records; thence southerly along the Westerly line of Lots 4 and 11 of Block 514 of said Morrow Village No. 5 subdivision, to the Southwest Corner of said Lot 4 of Block 514; thence Southerly in a straight line to the Northwest corner of Lot 11 of Block 515 of said Morrow Village No. 5 Subdivision; thence Southerly along the Westerly line of Lots 4 and 11 Block 515 of said Morrow Village No. 5, to the southwest corner of said Lot 4 of Block 515; thence Southerly in a straight

line to the Northwest corner of Lot 4 of Block 516 of said Morrow Village No. 5; thence Southerly along the Westerly line of said Lot 4 of Block 516 to the Southwest Corner of said Lot 4 of Block 516, also being the Northwest Corner of Lot 11 of Block 516, in the City of Ceres, Morrow Village No. 6 as recorded in Book 19 of Maps at Page 12, Stanislaus County Records; thence southerly along the Westerly line of said Lot 11 of Block 516, to the southwest corner of said Lot 11 of Block 516; thence Southerly in a straight line to the Northwest corner of Lot 4 of Block 517 of said Morrow Village No. 5; thence Southerly along the Westerly line of Lots 4 and 11 of Block 517 of said Morrow Village No. 5 to the Southwest corner of said Lot 11 of Block 517; thence Southerly in a straight line to the Northwest corner of Lot 4 of Block 518 of said Morrow Village No. 5; thence Southerly along the Westerly line of Lot 4 of Block 518 of said Morrow Village No. 5, to the Southwest Corner of said Lot 4 of Block 518; thence Westerly along the Southerly line of Line 5, 6, and 7 of Block 518 of said Morrow Village No. 5 to the Southwest corner of said Lot 7 of Block 518; thence southerly along the prolongation of the Westerly line of Lot 8 of Block 518 of said Morrow Village No. 5, to the southerly right of way line of Whitmore Avenue, thence Westerly along the Northerly line of Lots 1, 2, 7 and 8 of Block 5531, of the city of Ceres, as shown in Book 34 of Parcel Maps at Page 42, Stanislaus County Records, to the Northwest corner of said Lot 1 of Block 5531; thence Southerly along the Westerly line of said Block 5531, to the southwest corner of said Lot 8 of Block 5531; thence Easterly along the Southerly line of said Block 5531, to the Northeasterly corner of San Ramon Estates No. 2, of the City of Ceres, as shown in Book 27 of Maps at Page 28, Stanislaus County records; thence southerly along the prolongation of the Easterly line of said San Ramon Estates No. 2, to the center line of Stanford Avenue; thence Easterly along the center line of Stanford Avenue to the Northerly prolongation of the Westerly line of Roeding Heights subdivision, of the City of Ceres, as recorded in Book 26 of Maps at Page 93, Stanislaus County Records; thence Southerly along the Easterly line of said Roeding Heights Subdivision, to the Southeast corner of said Roeding Heights Subdivision; thence Westerly along the southerly line of said Roeding Heights Subdivision to the Southwest corner of said Roeding Heights Subdivision; thence Westerly along the southerly line of Roeding Heights No. 2 Subdivision, of the City of Ceres, as shown in Book 27 of Maps at Page 83, Stanislaus County Records, to the Southwest corner of said Roeding Heights Subdivision; thence Southerly along the prolongation of the Easterly line of Lot 35 Smyrna Park Tract, of the City of Ceres, to the Northeast corner of the Redwood Estates subdivision, thence Southerly along the prolongation of the Easterly line of Lot 35 Smyrna Park Tract, of the City of Ceres, to the Northeast corner of the Redwood Estates Subdivision, of the City of Ceres, as shown in Book 24 at Page 10, Stanislaus County Records; thence Southerly along the Easterly line of said Redwood Estates Subdivision to the Southeast corner of said Redwood Estates Subdivision; thence southerly in a straight line to the Northeast corner of Lot 26 of Block 4552 of the City of Ceres, Redwood Estates Unit 2, as shown in Book 26 of Maps at Page 92, Stanislaus County Records; thence Southerly along the Easterly line of said Redwood Estates Unit 2 Subdivision to the Southeast corner of Lot 19 of Block 4552 of said Redwood Estates Unit 2 Subdivision; thence Westerly along the Southerly line of Lots 17, 18 and 19 of block 4552, of said Redwood Estates Unite 2 Subdivision, to the Southwest corner of said Lot 17 of Block 4552; thence Northerly along the Westerly line of Lots 16 and 17 of Block 4552 of said Redwood Estates Unit 2 Subdivision, to the Northwest corner of said Lot 16 of Block 4556; thence Westerly along the Southerly line of Lots 6, 7, 10 and 11 of Block 4552 of said Redwood Estates Unit 2 Subdivision, to the Southwest corner of said Lot 6; thence Northerly along the

prolongation of the Westerly line of Lots 1, 2, 3, 4, 5 and 6 of Block 4552 of said Redwood Estates Unit 2 Subdivision to the North right of way line of Don Pedro Road; thence Westerly along the prolongation of the Northerly right of way line of Don Pedro Road, to the Easterly right of way line of the U.S. Highway 99; thence Southerly along the prolongation of the Easterly line of said Highway 99, to the Westerly line of the Ceres Main Canal; thence Northerly along the prolongation of the Westerly line of said Ceres Main Canal to the Southerly right of way line of Hatch Road; thence Easterly along the prolongation of the Northerly right of way line of Hatch Road to the Southeast corner of Parcel B as shown in Book 27 of Parcel Map at Page 83, Stanislaus County Records; thence Northerly along the prolongation of the Easterly line of said Parcel B, to the Thread of the Tuolumne River; thence Westerly along said thread to the northerly prolongation of the Westerly line of Parcel 1, City of Ceres, as recorded in Book 4 of Parcel Map at Page 92, Stanislaus County Records; thence Southerly along the prolongation of the Westerly line of said Parcel A to the Northwest corner of the Riverbend Heights Subdivision, City of Ceres, as recorded in Book 31 of Maps at Page 99, Stanislaus County Records, thence Easterly along the Northerly line of said Riverbend Heights Subdivision to the Northwest corner of Lonnie Estates subdivision, City of Ceres, as recorded in Book 33 of Maps at Page 91, Stanislaus County Records; thence Easterly along the Northerly line of said Lonnie Estates Subdivision to the Northeast corner of said Subdivision; thence Southerly along the Easterly line of said Lonni Estates Subdivision to the Southwest corner of said Subdivision; thence Easterly along the Northerly line of Lot 2 of Block 847, City of Ceres, as recorded in Book 8 of Parcel Maps at Page 23, to the true point of beginning.

An overview of the City Demographics and economic analysis follows:²

APPENDIX C. CITY OF CERES DEMOGRAPHICS

1. Description of City

The City of Ceres, which was incorporated in 1918, is located in the northern San Joaquin Valley approximately one mile south of the County seat, Modesto. Ceres is located 300 miles north of Los Angeles and 100 miles east of San Francisco on Highway 99, a major north-south highway.

The City is in the center of Stanislaus County just 15 minutes east of Interstate 5. Ceres industrial areas are serviced by the Southern Pacific Railroad and the Tidewater Southern Railway. The closest commercial airport is in Modesto, one mile north. Commuter flights daily to the Bay Area are available from the Modesto Airport.

2. Population Characteristics

- a. Racial and Ethnic Characteristics - The population of Ceres in 1980 was 13,281. In 1988 the projected population was 18,650, or an increase of 5,369 persons equaling a 28.79% increase in population. The racial composition in 1980 was 11,883 (83.5%) Whites; 34 (.3%) Blacks; 513 (3.9%) Indian and Asian; 1,770 (13.3%) Spanish.
- b. Age - The prime working age group, 20 to 64 years old, accounted for 67.9% of the Ceres population with 9.7% over the age of 65.
- c. Labor Force Characteristics - The total work force in Ceres in 1980 was 5,218. Of the total 4,486 (86.0%) were White; 553 (10.2%) were Spanish; the balance of other races was 199 (3.8%). The largest percentage of jobs by occupation are:

Precision Production, Craft and Repair Services	15.8%
Administrative Support, Including Clerical	13.3%
Machine Operators, Assemblers and Inspectors	11.2%
Service, Except Protective and Household	10.7%
Sales	9.6%
Executive, Administrative, Managerial	9.0%
Transportation and Material Moving	8.0%
Professional Specialty	7.1%
Laborers	5.7%
Farming, Forestry and Fishing	4.3%

Because Ceres has no major industries within its boundaries, 77.5% of the work force works outside the City. However, 90.4% work within the county. Of the total work force, 83.1% have a commute of 29 minutes or less to work.

² NOTE: The data in this portion of the MRCSP document was not updated so that the original context and economic analysis is retained.

d. Education - The level of education of persons over the age of 18 is:

Non High School Graduates	44.7%
High School Graduates	39.9%
College 1-3 years	15.6%
College 4+ years	5.8%

e. Income - The per capita income for the City of Ceres in 1979 was \$6,255 In 1986 the per capita income was \$8,433 or an average increase of 4.38% per year. Based on per capita income, the aggregate house- hold income in 1980 was \$86,708,643 and in 1986 was \$112,141,442.

Based on an average annual taxation cost of 21.87%, the total annual disposable income in 1980 was \$67,745,463 and in 1986 was \$87,616,110.

f. Unemployment - According to the 1980 census, there was an unemployment rate of 19% within the civilian labor force. Of the unemployed, 82% were White and approximately 18% were Hispanic and less than 1% were Indian and Asian.

3. Retail Sales

The sales data indicates a growth from \$75,974,000 in 1980 to \$117,406,000 in 1986. The real dollar growth for the period was \$2,833,000 with the cumulative growth of \$15,189,000 based on inflation for the same period.

In 1984, there was significant growth in retail sales of 15.71% due to the opening of Zody's. The decline or contraction in retail sales in 1985, has been attributed to the closure of Zody's.

4. Business Licenses

The business license process is based on City ordinance originated in 1969 and last revised in 1978. There is a set registration tax per year and a quarterly license tax, based on the type of business, on gross receipts. Business license fees have risen from \$88,555 in 1983-84 to \$121,023 in 1986-87, or an average of 9.1% per year.

ECONOMIC ANALYSIS

In light of the size of the City of Ceres, it was determined that the market for the Mitchell Road corridor is the entire City, plus the residential units to the east in the unincorporated area of the County.

The 1986 disposal income for the area was \$87,616,110; however, total retail sales totaled \$117,406,000. Hence only 25% of the income was derived from outside the City. It is also important to note that retail sales have barely kept pace with inflation.

A comparative analysis by category shows that the City of Ceres represents overall 5.39% of the total sales generated in the County but has garnered 12.41% of the drug store business and 9.37% of sales related to service stations. This is in the context that the City's population is 5.60% of the County. In order to put this into perspective, the County, with an overall population of 320,645, has averaged over a seven year period only 1.1% of the total sales in the State.

Five categories in the City/County comparison were analyzed. The City's average percentage of taxable sales in the County is 5.39%. The retail store categories which fall below this average are:

apparel stores (2.82%), packages liquor stores (0.89%), home furnishings and appliances (1.94%), building material and farm implements (1.36%) and auto dealers and auto supplies (1.68%). The one category which may be understated is packaged liquor stores. It appears the actual sales are included in the food stores numbers.

These percentages indicate that the city, at the present time, does not have the businesses or commercial outlets that represent the above categories.

To facilitate discussion of Mitchell Road, it is referenced according to the three Planning Areas.

Area 1: Roeding South to 99 Highway

Area 2: Roeding North to Hatch Road

Area 3: Hatch North to City limit line (Modesto)

Each Area represents a distinct and unique opportunity for economic development. Area 1 should be considered for off-highway and commercial use such as a motel restaurant complex along with large-scale retail/commercial uses. This area should be considered for redevelopment, thereby providing tax increment to the City.

Area 2, with its existing mix of commercial and retail, should be targeted toward picking up those uses identified in the previous section, specifically, apparel stores, home furnishings and appliances, and building materials. In addition, specialized office space (medical and care facilities) would be appropriate in this zone.

Area 3, the area north of Hatch Road, should be considered for general commercial and some airport related uses. The Modesto airport is located within one mile from the City limit line and the area surrounding the airport is primarily manufacturing and produce related facilities with little available land for office or airport related facilities.

The data accumulated was utilized to optimize the economic benefit to the City, both on a one time and recurring basis. The current revenue from retail sales was analyzed and compared to future income based on recommended and current development proposals.

The potential job creation, and the associated benefits, including return on investment was analyzed and is presented in the Fiscal Impact Report (Technical Appendices). Tables 2, 3, and 4 show the available square footage for development and ten year job creation analysis.

Assumption utilized in consideration of these tables are:

- a. Land vacancies in the corridor were gathered via survey, information provided by community development staff, assessors maps and related brokers. Usable agricultural land was included as land for potential development.
- b. The projection of jobs created by new business is based upon 30% of the vacant acreage which allows for infrastructure improvements and is an acceptable standard for projections of this kind. (Source: Los Angeles County Economic Development Corporation and U. S. Department of Housing and Urban Development).
- c. The ratio of jobs to square feet is an acceptable standard for determining jobs utilized by HUD for its UDAG Program.
 - 1. Commercial One job per 500 sq. ft.
 - 2. Industrial One job per 500 sq. ft.
 - 3. Technical One job per 250 sq. ft.
 - 4. Office One job per 250 sq. ft.
- d. There is also an assumption that the current mixture will change significantly over the next 10 years.
- e. All figures presented are maximums and can be achieved under optimum conditions only. The number of jobs is directly related to factors having to do with the economy, local, political decisions, private sector decisions, taxes, incentives in place, etc., and capital availability.
- f. Without land assemblage, in portions of the corridor, expansion and new development could be limited.
- g. Assuming the economy continues to grow, we project that existing business in the Corridor could increase job opportunities, but at a much slower rate. Our projections are based upon only 4.3% increase over a five-year period.

TABLE 2
ECONOMIC DEVELOPMENT POTENTIAL

Mitchell Road Corridor	Gross Area	Vacant or Unoccupied Land (square footage)	
		Usable Area	
Area 1	3,654,248	1,096,274	
Area 2	2,679,811	803,943	
Area 3	2,042,093	612,628	
TOTAL	8,376,152	2,512,845	

TABLE 3
PROJECTED 10-YEAR JOB CREATION BY ECONOMIC SECTOR

Mitchell Road Corridor	Comm	Ind	Tech	Office	TOTAL
Area 1	1,097	278	261	392	2,028
Area 2	1,019	0	365	392	1,776
Area 3	209	392	0	261	862
TOTAL	2,323	670	626	1,045	4,666

TABLE 4
PROGRAM AREA PROJECTED JOB CREATION BY SKILL CATEGORY

Skill Category		Year 1	Year 5	Year 10
15	Managerial/Professional	70	280	700
7	Technician/Specialist	33	130	326
13.7	Sales	63	255	639
18.3	Administrative Support (including clerical)	85	342	854
17.7	Service	83	330	829
14.3	Production/Craft/Repair	67	267	667
14	Operator/Fabricator/Laborer	65	262	651
TOTAL		466	1,866	4,666

AIRPORT LAND USE COMPATIBILITY LISTING

KEY: O = Compatible; C = Conditionally Approvable; X = Prohibited Use

USES	AIRPORT BUILDING AREA	OTHER AIRPORT PROPERTY	APPROACH & TRANSITIONAL SURFACES	OTHER LANDS WITHIN AIRPORT PLANNING BOUNDARY
AGRICULTURAL USES				
Truck and Specialty Crops	O	O	O	O
Field Crops	O	O	O	O
Pasture and Rangeland	O	O	O	O
Orchard and Vineyards	X	X	O	O
Dry Farm and Grain	O	O	O	O
Tree Farms, Landscape Nurseries and Greenhouses				
	O	O	C	O
Fish Farms	X	X	O	O
Feed Lot and Stockyards	X	X	O	O
Poultry Farms	X	X	C	O
Dairy Farms	X	X	C	O
NATURAL USES				
Fish and Game Reserves	X	X	O	O
Land Reserves and Open Spaces	O	O	O	O
Flood and Geological Hazard Areas	O	O	O	O
Waterways: Rivers, Creeks, Canals, Swamps, Bays, Lakes	O	O	O	O
RESIDENTIAL AND INSTITUTIONAL				
Rural Residential-10 acres or more	X	X	C	O
Suburban Residential-20,000 sq. ft. to 10 acre lots	X	X	X	O
Urban Single Family-under 20,000 sq. ft. lots	X	X	X	O
Multi Family	X	X	X	O
Mobile Home Parks	X	X	X	O
Schools, Colleges and Universities	X	X	X	C
Hospitals	C	C	X	O
Churches	X	X	X	O

AIRPORT LAND USE COMPATIBILITY LISTING

KEY: O = Compatible; C = Conditionally Approvable; X = Prohibited Use

USES	AIRPORT BUILDING AREA	OTHER AIRPORT PROPERTY	APPROACH & TRANSITIONAL SURFACES	OTHER LANDS WITHIN AIRPORT PLANNING BOUNDARY
RECREATIONAL				
Golf Course	O	O	O	O
Parks	O	O	O	O
Playgrounds and Picnic Areas	O	O	O	O
Athletic Fields	X	X	X	O
Riding Stables and Trails	X	X	O	O
Marinas	O	O	O	O
Tennis Courts	O	O	O	O
Outdoor Theaters	X	X	X	O
Swimming Pools	O	O	O	O
Fairgrounds and Race Tracks	X	X	X	O
COMMERCIAL USES				
Aircraft Sales and Repairs	O	O	O	O
Flying Schools	C	C	C	O
Hotels and Motels	C	C	X	O
Shopping Centers	C	C	X	O
Banks	C	C	X	O
Gas Stations	C	C	X	O
Auto Storage and Parking	O	O	O	O
Office Buildings	C	C	C	O
Theaters and Auditoriums	X	X	X	O
Public Buildings	C	C	C	O
Taxi, Bus and Terminals	O	O	X	O
Memorial Parks	X	X	X	O
Pet Cemeteries	X	X	X	O
Restaurants and Food Take-Outs	C	C	C	O
Retail Stores	C	C	C	O
Truck Terminals	O	O	O	O
Other Service Uses	C	C	C	O
INDUSTRIAL				
Research Laboratories	C	C	C	O
Warehouses	O	O	O	O
Aircraft Factories	O	O	C	O
Air Freight Terminals	O	O	O	O
Non-air Related Manufacturing	C	C	C	O

AIRPORT LAND USE COMPATIBILITY LISTING

KEY: O = Compatible; C = Conditionally Approvable; X = Prohibited Use

USES	AIRPORT BUILDING AREA	OTHER AIRPORT PROPERTY	APPROACH & TRANSITIONAL SURFACES	OTHER LANDS WITHIN AIRPORT PLANNING BOUNDARY
Rail Sidings	O	O	O	O
Other Transportation Parks	O	O	O	O
Petroleum and Chemical Products Bulk Storage	C	C	C	O
UTILITIES				
Reservoirs	C	C	O	O
Water Treatment	C	C	O	O
Sewage Disposal	C	C	O	O
Petroleum and Chemical Products Bulk Storage	C	C	C	O
Electrical Plants	C	C	C	O
Power Lines	C	C	C	O

PART TWO

MITCHELL ROAD CORRIDOR SPECIFIC PLAN DESIGN GUIDELINES

MITCHELL ROAD CORRIDOR
SPECIFIC PLAN

DESIGN GUIDELINES

Prepared for:

The City of Ceres
2720 Second Street
Ceres, California

Prepared by:
The Planning Center
1300 Dover Street, Suite 100
Newport Beach, CA 92660

Adopted August 1989

Updated September 1995

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I. INTRODUCTION

A. PURPOSE AND INTENT

These Guidelines have been prepared for the Mitchell Road Specific Plan located in the City of Ceres, California. The Guidelines provide the framework for high quality design and consider project-wide issues as well as site specific issues. The Guidelines express the desired character of future development and address site planning, architecture, signage, landscape and lighting concepts.

The Mitchell Road Design Guidelines are designed to be integrated with the Mitchell Road Specific Plan. Common community features such as signs, landscape and hardscape, walls and fences, and architecture are emphasized to provide unity and express a strong collective design statement.

The Design Guidelines establish development criteria at the project level to assure a unified environment design. At the site specific level, individual projects will be required to comply with relevant Design Guidelines applicable to each use. While each project should relate to the community context, this document is not intended to limit innovative design, but rather provide clear direction and design criteria. Each project shall be compatible with common community design elements; however, differences in product type, use, tenant preferences and other factors necessitate the need for separate identity of each individual project.

Summarized, the goals of the Design Guidelines are as follows:

- To provide the City of Ceres with the necessary assurances that future projects attain the desired level of quality;
- To serve as design criteria for use by planners, architects, landscape architects, engineers, builders and future property owners;
- To provide guidance to City Staff, Planning Commission and the City Council when reviewing future development projects within the Mitchell Road Specific Plan; and
- To provide a viable framework and clear direction without limiting the creativity of the designer, to avoid unnecessary delays.

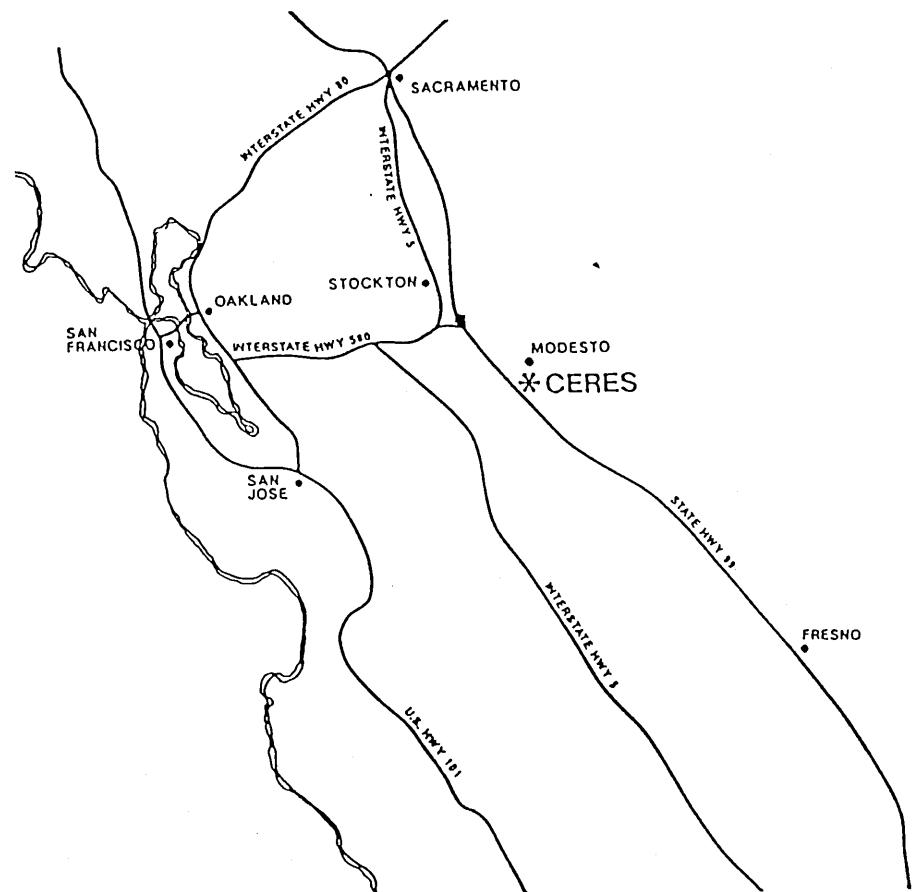
B. PROJECT DESCRIPTION AND LOCATION

The Mitchell Road Corridor study area is located on the eastern edge of the City of Ceres. Ceres is located adjacent to and south of the City of Modesto in Stanislaus County.

The corridor is within the City jurisdiction with the exception of several parcels in the north and south end which are currently within the unincorporated County of Stanislaus. Only a small percentage of the corridor is currently developed with existing commercial uses concentrated predominantly between Fowler and Roeding and at the intersections of Hatch/Mitchell and Don Pedro/Mitchell.

The Mitchell Road Corridor includes a 2-1/2 mile stretch of Mitchell Road extending between Highway 99 on the south

REGIONAL LOCATION MAP



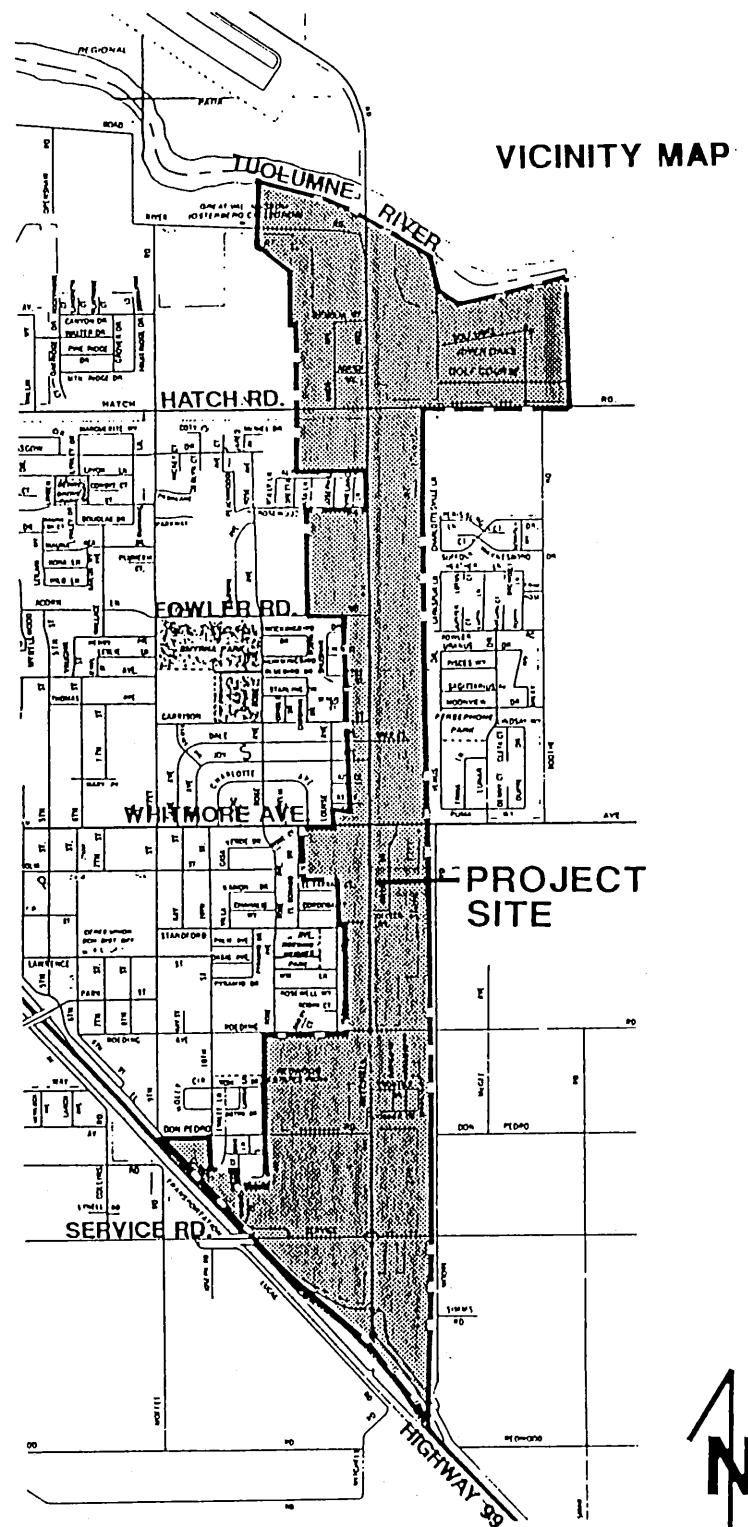
and the Tuolumne River on the north. The corridor is approximately 1/4 mile wide for most of its length but broadens to approximately a half mile wide at the northern and southern ends to include large parcels in those areas.

In general terms, the Specific Plan characterizes the future character of the corridor by dividing it into the (3) distinct design districts. The Design Guidelines may be characterized as:

1. South Gateway Design District: Located at the southern end of the study area, between Highway 99 and Don Pedro. It is visualized as an intense commercial gateway to the city for service city and regional needs.
2. Community Commercial Design District: Located between Don Pedro and Hatch Roads, intended to serve local neighborhood commercial needs, especially the neighborhoods to its immediate east and west.
3. North Gateway Design Districts: Located at the north of the project area it is between the Tuolumne River and Hatch Road. It is intended to take advantage of its connection to Modesto and the Beard Industrial District to the north and its proximity to the Tuolumne River, and the Modesto area in general.

C. A USER'S GUIDE TO THE GUIDELINES

The Design Guidelines contain a large amount of information and guidance. They are designed to facilitate

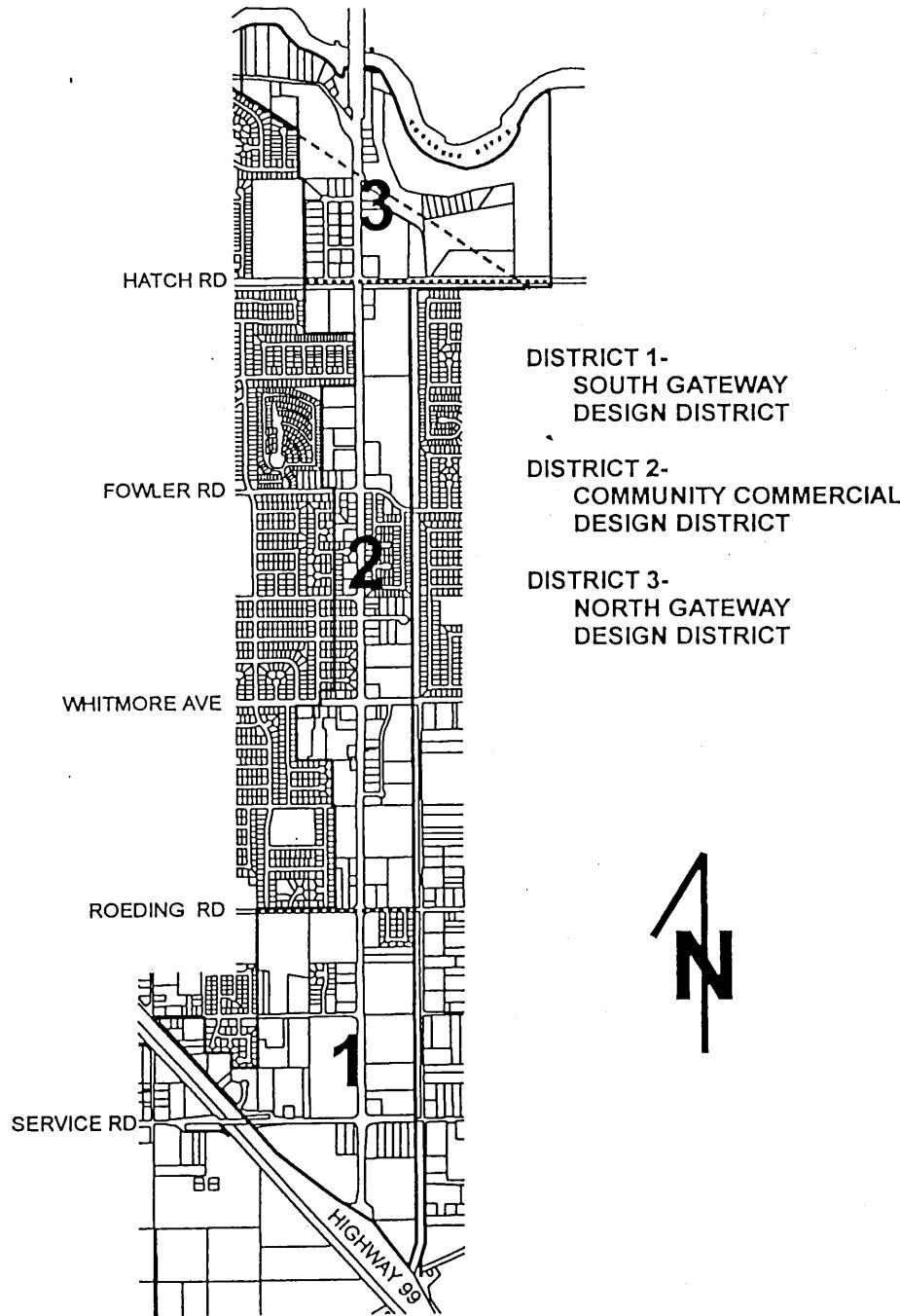


MITCHELL ROAD CORRIDOR
SPECIFIC PLAN
DESIGN DISTRICT BOUNDARY

use by those who wish to build projects in character with the Mitchell Road Specific Plan. It can assist property owners, developers, staff personnel who must review project proposals and decision makers who are called upon to act on these projects.

1. Contents and Sequence

- a. An introduction provides a broad overview of the Guidelines and describes its purpose and intent.
- b. A project wide elements discussion is provided to coordinate the areas of land use, circulation, site planning, and design for the entire corridor. These elements will make a strong statement and will complement other project-wide aspects such as landscape guidelines.
- c. An architecture guidelines section expresses the unique character of the three districts of the corridor: South Gateway Design District, Community Commercial Design District, and North Gateway Design District.
- d. A landscape guidelines section is provided to express the overall landscape concept and explain the elements of that concept.
- e. A sign guidelines chapter describes unique corridor usage and how signs can express the Corridor's identity.



- f. A lighting guidelines section is provided to coordinate the night time expression of the Corridor's image with the other elements mentioned above.

II. PROJECT WIDE ELEMENTS

A. LAND USE

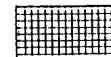
The Mitchell Road Corridor will be developed as an intense commercial corridor with a balance of freeway, highway and neighborhood commercial uses along with opportunities for industrial, business park and residential uses.

The Land Use Concept has been developed by expanding and refining the land use designations from the City's General Plan and the application of the goals, objectives and policies of the Specific Plan.

The Land Use Concept revolves around three distinct Design Districts which are tied together through a unifying architectural and landscaping theme. The intent is to provide a sense of cohesiveness as one travels the corridor, yet preempt monotony and enhance the special features within each area.

The southern end of the corridor (South Gateway Design District from Service Road to State Route 99) is predominantly designated regional commercial (i.e., large-scale large-lot uses) uses are anticipated for this area. This district is intended to serve as a gateway to the City and provide an inviting ambiance.

COMMUNITY COMMERCIAL (CC)



REGIONAL COMMERCIAL (RC)



HIGHWAY COMMERCIAL (HC)



INDUSTRIAL PARK (IP)



RESIDENTIAL

RL-7 LOW DENSITY RESIDENTIAL (7 DU/AC. MAX)
RM-15 MEDIUM DENSITY RESIDENTIAL (15 DU/AC. MAX)
RH-25 HIGH DENSITY RESIDENTIAL (25 DU/AC. MAX)

MIXED USE 1 (MX-1)



MIXED USE 2 (MX-2)

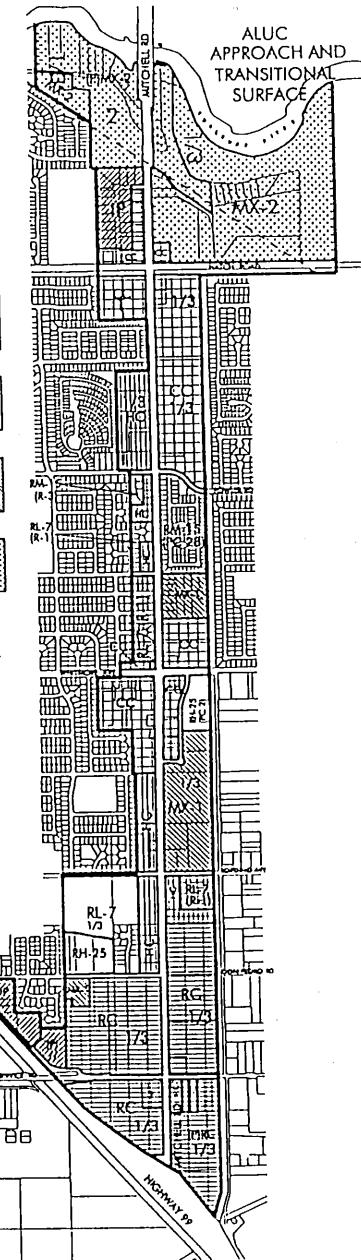


STANISLAUS AIRPORT LAND USE COMMISSION'S
TRANSITIONAL - APPROACH SURFACES BOUNDARY (APPROXIMATELY)

MITCHELL ROAD CORRIDOR SPECIFIC PLAN

CITY OF CERES

ADOPTED: RESOLUTION 89-177 AUGUST 28, 1989
ORDINANCE 89-739 SEPTEMBER 11, 1989
ORDINANCE 89-744 JANUARY 9, 1990
AMENDED: ORDINANCE 94-825 MARCH 3, 1994
RESOLUTION 95-120 SEPTEMBER 11, 1995
ORDINANCE 95-850 SEPTEMBER 25, 1995



MITCHELL ROAD CORRIDOR SPECIFIC PLAN CIRCULATION PLAN

The middle portion of the corridor (Community Commercial Design District) is a mix of highway and commercial and mixed use designations. This area is oriented to City-wide retail and service needs.

The northern end of the corridor (North Gateway Design District) is designated as mixed use and industrial park. This district, also a gateway to the City, will provide a business like ambiance. Architecture and color themes will be more neutral and natural.

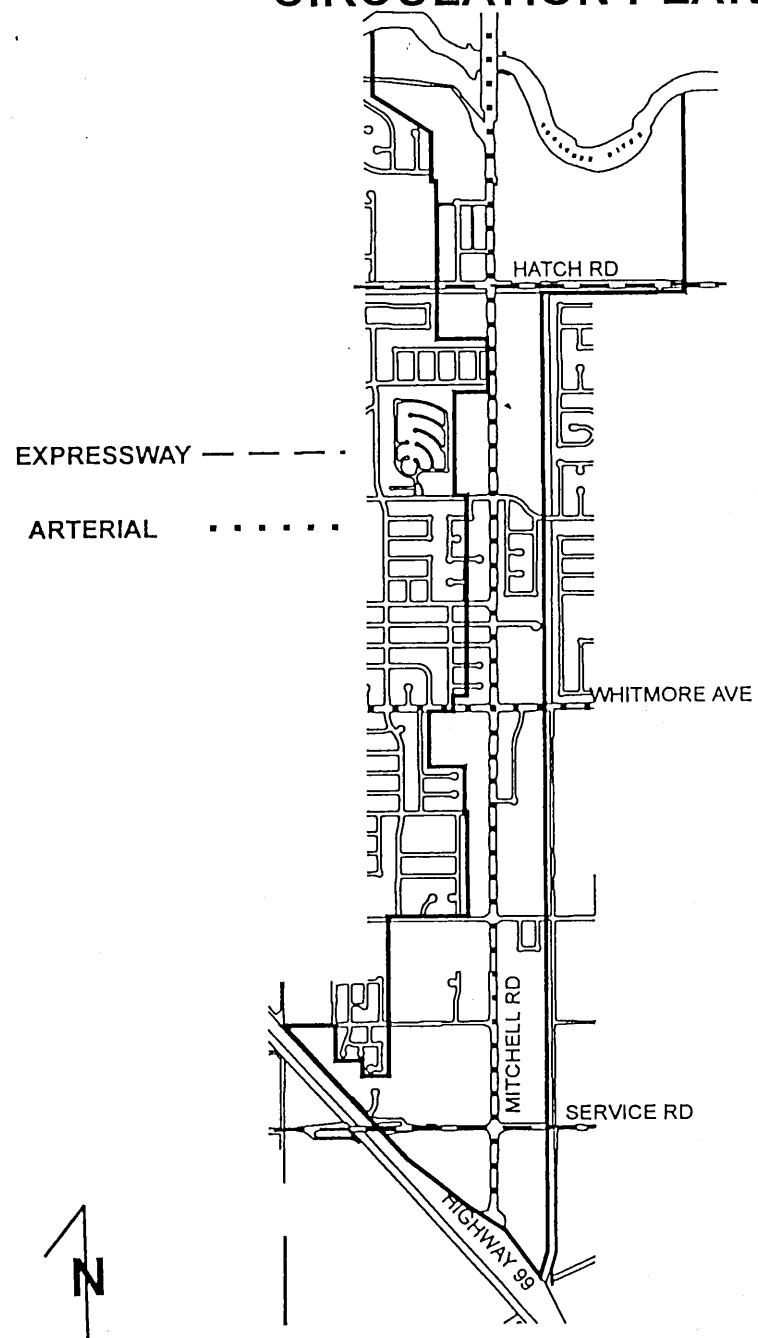
B. CIRCULATION

A strong hierarchy of streets and walkways have been developed to attain a strong circulatory cohesiveness. To provide a clear sense of direction and ease of circulation, key gateways will be enhanced to announce the arrival into Mitchell Road and each of the projects within.

The circulation concept specifies the right-of-way width for each street. At entry points, special setbacks have been recommended to allow for intensive landscaping and hardscape treatment. Major entrances into the projects will also be special with enriched paving treatments (i.e., textured or colored).

1. **Vehicular Guidelines**: Access drives and curb cut-offs of these major streets must be coordinated to provide safe and efficient traffic flow during peak periods.

- Opposing and/or adjacent driveways and intersections should be spaced a minimum of three hundred (300) feet apart, especially

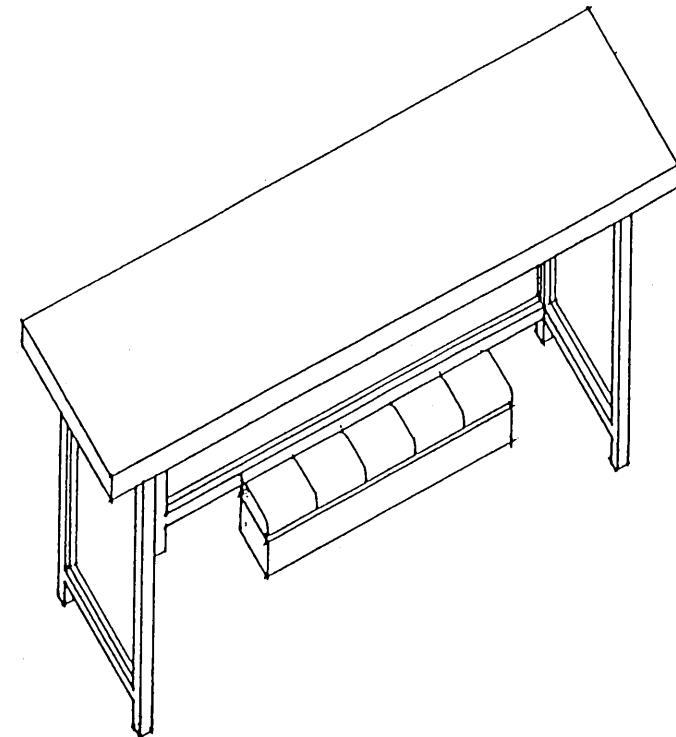


when adjacent to a major intersection to minimize left-turn conflicts.

- Shared access driveways between parcels are encouraged and may be required as a method to reduce the number of curb cuts and potential conflict points along streets.
- Pedestrian walkways are provided on both sides of major streets and collector streets, and connect developments through natural open space areas.
- Internal circulation shall be required when possible and in accordance with the development regulations and performance standards in the Specific Plan.

2. Transit: As mass transit routes are developed which pass through the Mitchell Road Corridor Specific Plan area, accommodations for convenient passenger loading and unloading shall be provided as appropriate.

- a. Dial-a-Ride loading zones (i.e., curbs marked for temporary stopping but not parking) should be provided in major commercial areas to accommodate Dial-a-Ride passengers.
- b. Mass Transit Transition-New development should coordinate with mass transit services to determine if potential passenger loading and unloading points will occur in the



vicinity of that development. If so, appropriate right-of-ways shall be reserved.

- c. Mass Transit Hardscape Guidelines-Mass transit loading and unloading areas shall be clearly marked and provide pedestrian facilities. Design considerations for such facilities will include benches, landscaping, lighting, signage, drinking fountains, special paving, inclement weather shelters and telephone facilities.

3. Paths and Trails: The paths and trails guidelines will include pedestrian and bicycle trails.

a. Pedestrian

- On-site pedestrian circulation systems will be provided to meet the circulation needs of on-site users. Such systems will provide safe, all weather efficient, and aesthetically pleasing means of on-site movement and will be an integrated part of the overall architecture and site design concept.
- Where access is desirable, connections will be made between on-site and perimeter pedestrian circulation systems.
- Pedestrian circulation system dimensions and surface materials shall be governed by minimum city standards. Where no standards exist, minimum five foot sidewalks will be

provided under the existing street right-of-way.

b. Bicycle

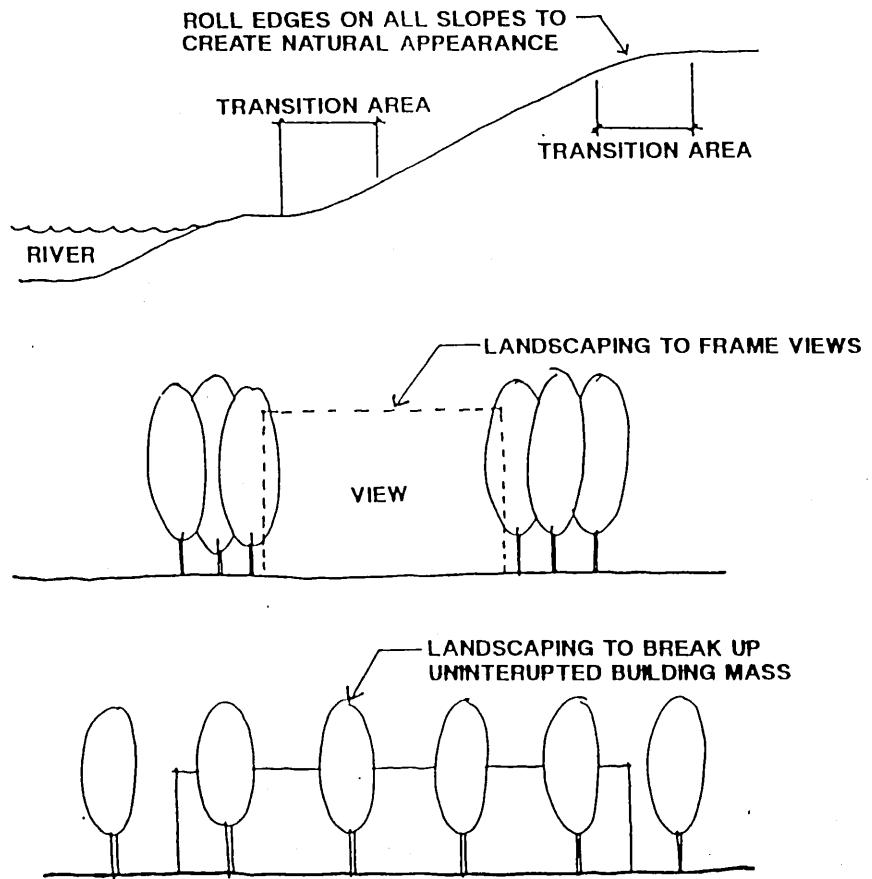
- Bicycle lanes are encouraged along secondary roadways and the adjacent canals. These lanes should be a minimum of eight feet wide and clearly marked and should conform to the City's classification of Type I or Type III.

C. SITE PLANNING AND GRADING

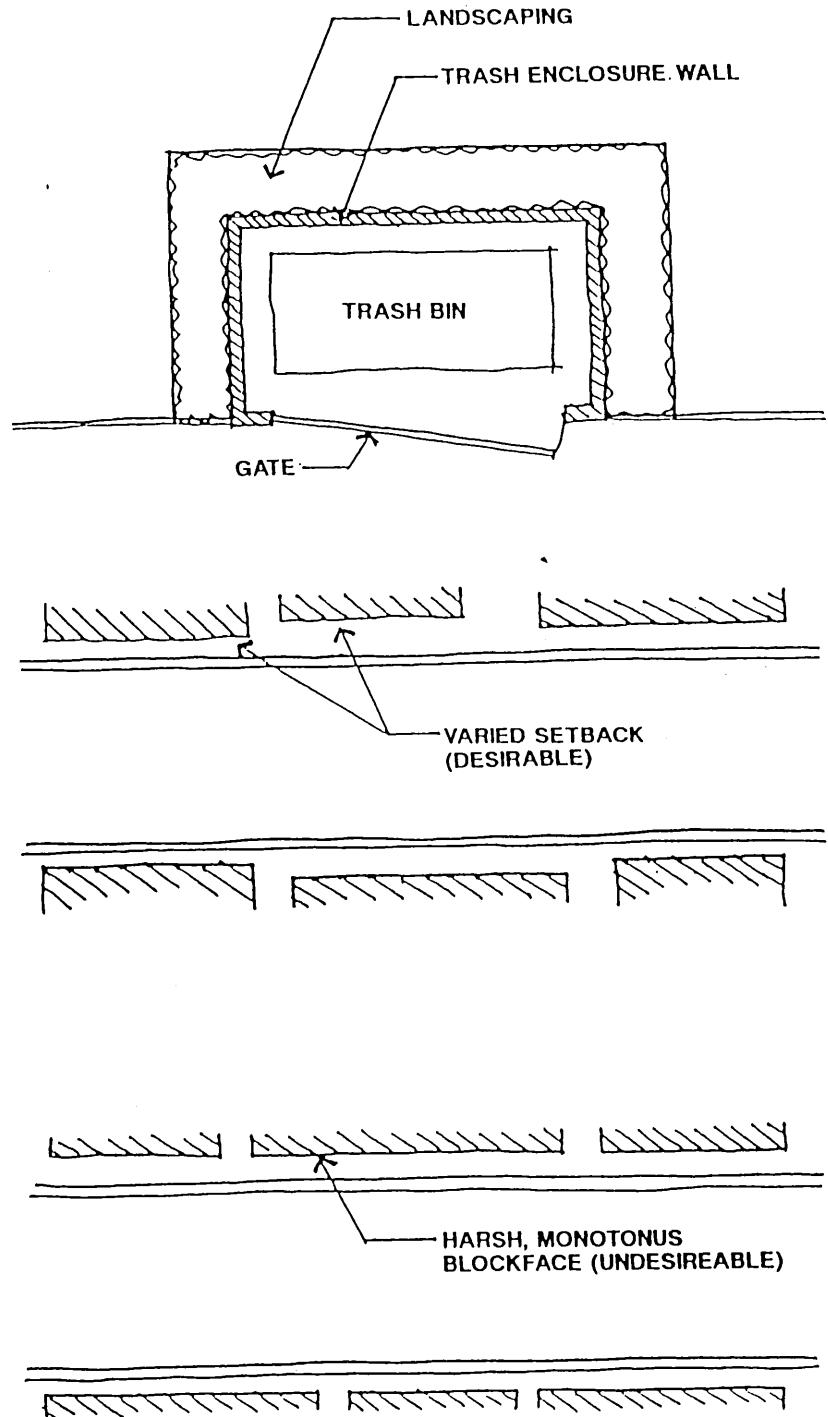
Physical space is modified with the placement of architecture through site planning. In the Mitchell Road project area, appropriate site planning and setbacks will assure and protect the character of the City of Ceres.

Guidelines

- The transitions at top and toe of manufactured slopes (for example, near the river in the North Gateway Design District) shall be rounded. Abrupt, squared off manufactured slope transitions shall be avoided.
- Slope along river shall appear as natural as possible. Where retaining wall is unavoidable, the use of river rock or natural stone retaining walls at the top or toe of natural stone retaining walls at the top or toe of natural slopes is acceptable.



- Landscaping shall be used to break up otherwise uninterrupted building mass, frame views, and connect with development on adjacent pads.
- The visual impact of parking areas shall be reduced by landscaping, hedges, berming, planted islands and fingers.
- All parking spaces shall be clearly outlined on the surface of the parking facility with painted white lines or stone, brick, textured or smooth concrete banding.
- Entries into development areas should reinforce the layout of the adjoining development by using the repetition of paving materials, accent trees and shrubs.
- Ancillary structures and service areas such as trash enclosures shall be screened from view through the combination of landscaping and walls.
- Individual parcels will be graded in such a way as to direct runoff away from buildings and into drainage facilities or on-site retention ponds.
- Careful consideration of building and site planning relationships can improve the quality of space and create interest, such as positioning the narrow side of buildings toward the roadway to increase the visible open space area to the passerby.
- Buildings should be sited such that they compliment adjacent buildings and landscape.



- Building placement on adjoining parcels along a street should be varied to avoid creating a harsh, monotonous blockface or streetscape.
- Solar exposure of buildings on adjacent properties must be protected. The potential for passive solar design and orientation is to be considered.

III. ARCHITECTURAL GUIDELINES

A. ARCHITECTURAL CHARACTER

In the built environment, architecture is the most prominent element in defining physical space. The intent of these architectural guidelines is not to stifle individual creativity, but rather, to create a framework for a strong collective statement. Building forms, elements (i.e., roofs, walls, doors and windows), colors and materials specified in this document will be utilized to obtain the desired architectural character and continuity within the Mitchell Road Corridor.

The emerging architectural style of the corridor is contemporary and this will be integrated as part of the overall design concept. From a design perspective, the corridor is divided into three Districts: South Gateway Design District, Community Commercial Design Districts and North Gateway Design District. The transition between these design districts will be gradual and generally follows a more urbanized character to a more natural character when traveling from the south to the north.

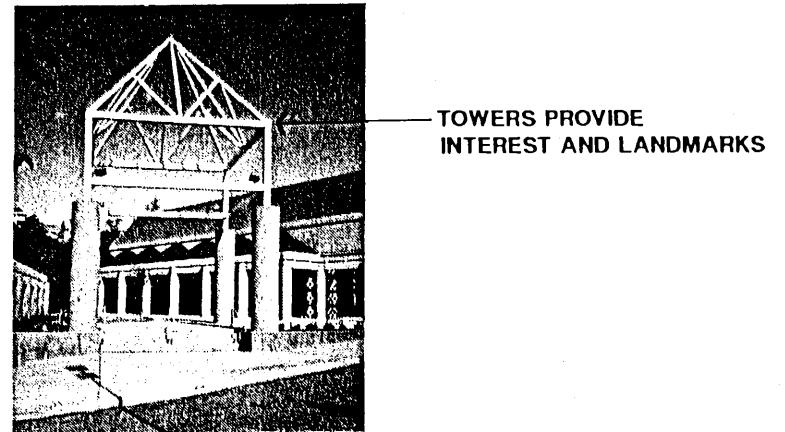
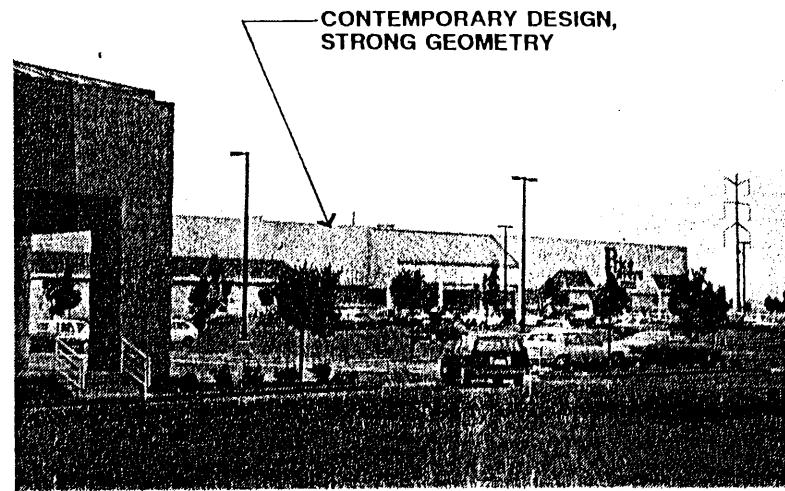
B. SOUTH GATEWAY DESIGN DISTRICT

The South Gateway Design District represents the highest intensity uses within the planning area. Zoning within this area includes regional Commercial and Industrial Park.

Due to the proximity to Highway 99 and the large amount of vacant land, the zone is targeted as a strong commercial identity and gateway to the Mitchell Road corridor. The design will be expressive of upscale, regional commercial uses. The architecture character will be "strong, massive, simple and contemporary."

1. Building Form

- Building masses are simple in form and of strong geometry.
- Building complexes shall be organized so that entry accents and usable courts and gardens are created by building site placement.
- Buildings will be taller here than elsewhere on the Corridor to be visible from Highway 99 and to exaggerate the bold character of the overlay area.
- Buildings will be set back from the street to provide generous parking and a foreground setting for these big buildings.
- Towers in select locations can provide interest and landmarks.



2. Building Materials

- The basic building materials should be restricted to 2 or 3 elements for each building to emphasize the simple contemporary design concept.
- Use smooth surface materials as the base materials. Concrete should be the primary building material. Textured surface should only be used as accent.
- The following materials are permitted:
 - Poured-in-place, tilt-up, or precast concrete
 - Exterior plaster smooth or light texture
 - Ceramic, concrete or clay tile (as accent or in moderation)
 - Metal panels, such as "CorTen"
 - Metal Tubes (trellis structure)
 - Fiberglass (as accent)
 - Plastic (as accent)
 - Reflective glass in "colors" (limited use only)
 - Non-reflective glass (but not glass curtain walls)
 - Concrete block with sandblasted finish
 - Stone products, e.g., granite marble (as accent only)
 - Brick
 - Wood Siding
 - Wood shakes or shingles (fire resistant)
- The following materials are not permitted:
 - Silver or "shiny" reflective glass
 - Plywood or unfinished wood
 - Corrugated metal
 - Asphalt shingles

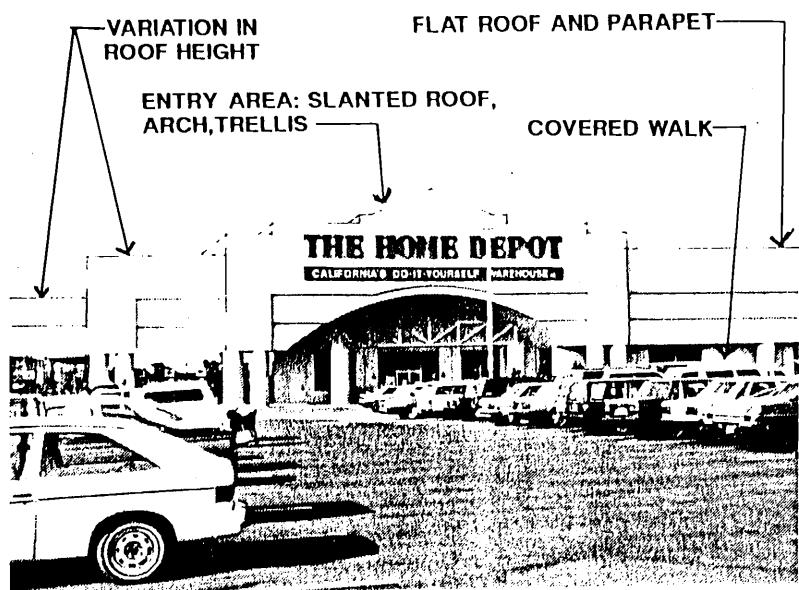
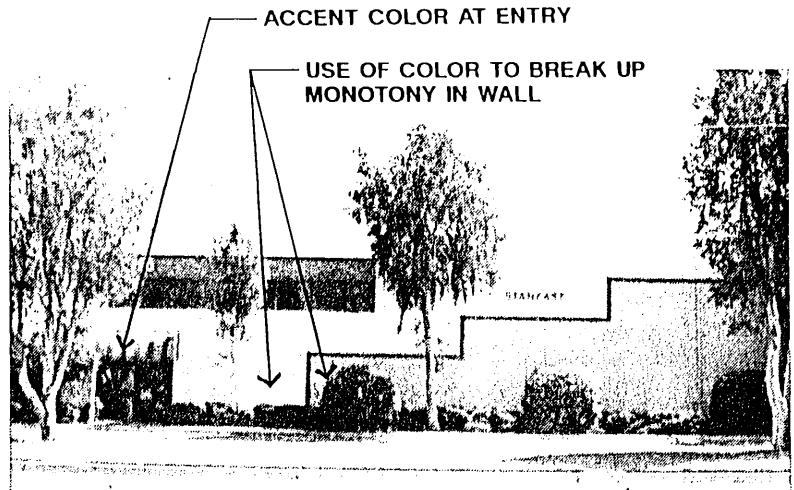
- Any material not listed above will be subject to review and approval by the Planning Commission.

3. Colors

- Overall color theme should be pastel in color. Example colors include: white, cream, pale blue, gray, pale yellow, pink, tan, and mist green. No extreme colors shall be used in the overall color theme.
- Lighter color should be used as base tones and brighter complimentary colors should be used as accents at building entries, covered walls and special details.

4. Roofs

- Flat roofs will predominate. Rooflines on flat roofs are to be adorned by cornice, molding, parapet, or coping.
- Slanted roofs, arches, canopies or trellis structures will be used to call attention to entry areas and bring the buildings to pedestrian scale.
- Covered walkways, arcades, canopies or awnings are encouraged along the public face of buildings for weather protection and to provide transitions from parking lots into buildings. These forms are to be integrated into the building design so as not to look like an add on.



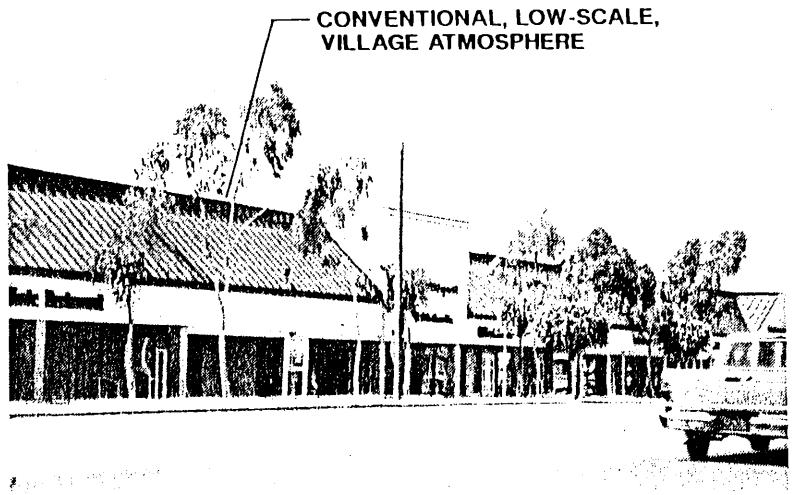
- Variations in roof height and profile are encouraged to conceal mechanical equipment or define entry.
- All mechanical equipment will be screened from view as is practical. Where possible, mechanical screens are to be integrated with the facade of buildings. Visible screening and exposed equipment will be painted to "match" the building.
- Roof mounted ventilators are to be a maximum of one and one half (1-1/2) feet above the point to which attachment is made. Ventilators are to be painted or prefinished consistent with the color scheme of the building.

5. Walls and Windows

- Building walls will be large vertical planes with windows predominantly on the public face of the building.
- All visible elevations of a building should have adequate but simple enrichment.

C. COMMUNITY COMMERCIAL DESIGN DISTRICT

The Community Commercial Design District stretches from Don Pedro Road in the south to Hatch Road in the north. This area is a composite of Community Commercial, Residential, Community Commercial and Mixed Use 1 (commercial, residential and business park zoning). Currently, this area is largely strip commercial, vacant lot or residential uses.



This design district is a middle transitional area between the more urban character South Gateway Design District and the more natural character North Gateway Design District.

The design character of this District will be expressive of conventional, low-scale, village atmosphere. The architecture will have more individual expression, detail, and variety in style.

The building orientation shall continue with the closed in character established by the existing residential walls.

1. Building Form

- Building forms should be of simple geometry with traditional rectangular forms.
- Buildings will be low, one to three stories, and pedestrian oriented in scale.
- Courtyards, atriums and outdoor gathering and eating areas are encouraged. Creating outdoor vitality will improve the pedestrian experience.
- Recesses that create interplay of light and shadow, covered walkways, colonnades, arcades, and openings that create interest are encouraged.

2. Building Materials

- The basic building materials should be restricted to 2 or 3 elements for each building to emphasize the simple contemporary design concept.



- Use medium textured materials as the base materials. Smooth or rough textured surface should be used as accents.
- The following materials are permitted:
 - Poured-in-place, tilt-up, or precast concrete.
 - Medium textured exterior plaster.
 - Concrete block with medium texture finish.
 - Metal panels, such as "CorTen"
 - Non-reflective glass.
 - Stone products, i.e., granite marble.
 - Brick
 - Wood Siding
 - Wood shakes or shingles
- The following materials are not permitted:
 - Silver or "shiny" reflective glass
 - Fiberglass
 - Plastic
 - Plywood or unfinished wood
 - Corrugated metal
 - Asphalt shingles
- Any material not listed above will be subject to approval by the Planning Commission.

3. Colors

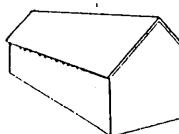
- Overall color theme should be lighter earthtone (i.e., beige, buff, grays, cream) or pastel color. No

extreme colors shall be used in the overall color theme.

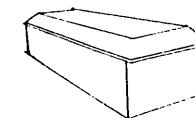
- ° Lighter color should be used as base tones and darker complimentary color should be used as accents at building entries, covered walls and special details.
- ° Harmonious and complimentary colors will be used. Dissident or distracting colors will be avoided. Highly contrasting graphic patterns on facades will not be allowed.
- ° Richer shades shall be limited to signs, doors, window trim, and other detailing related to pedestrian areas.

4. Roofs

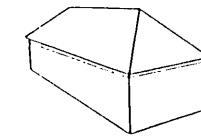
- ° Simple gable, hip or mansard-style roofs are preferred over flat roofs.
- ° High quality, dark colored roofing, such as sealed nonreflective metal, wood shingles or shakes, clay or concrete tile and asphalt/concrete composites are recommended.
- ° Shade structures, such as heavy timber trellises or ivy covered walls are encouraged.
- ° The appropriate use of awnings is encouraged as a way to add color to pedestrian areas.



GABLE ROOF



MANSARD ROOF



HIP ROOF



- Mechanical equipment shall be screened from public view.

5. Walls and Windows

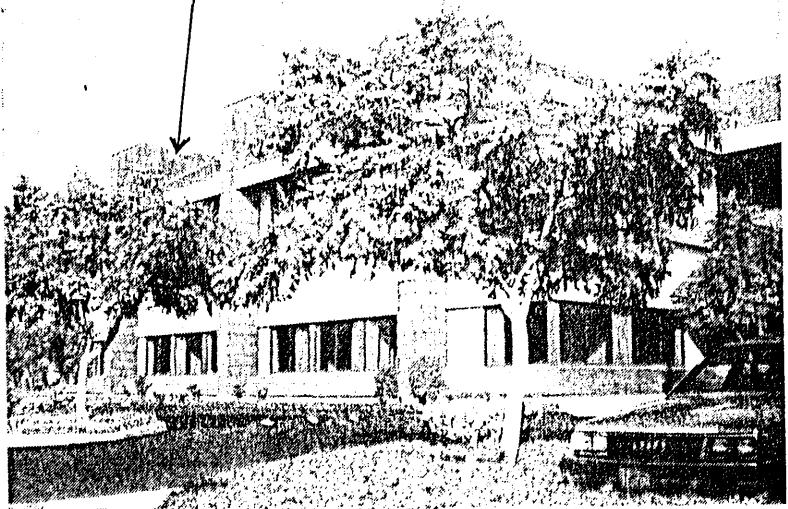
- Ground level building elevations that overlook rear yard service and trash areas, should have limited window area.
- Large recessed windows may be used, especially at the ground level, to afford pedestrians protection from the weather and allow window shopping.
- Buildings fronting on Mitchell Road shall have display windows. Window display shall serve to attract motorists but not be disruptive to the flow of traffic.

D. NORTH GATEWAY DESIGN DISTRICT

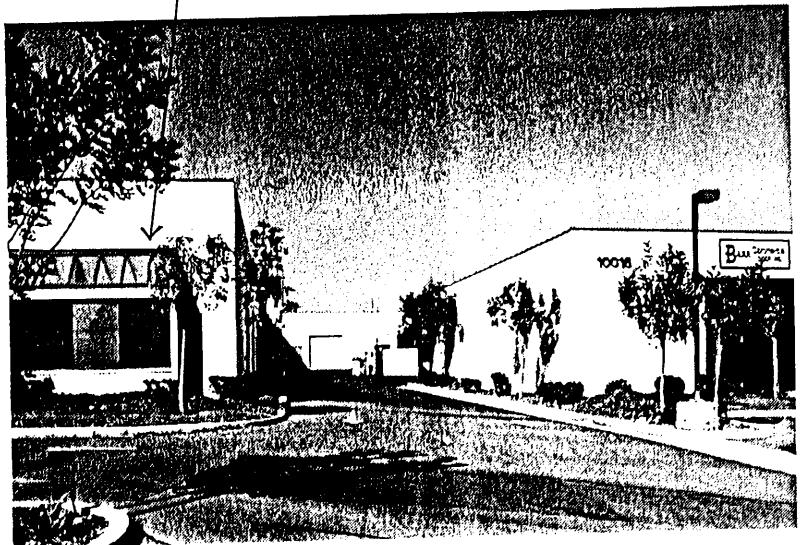
The North Gateway Design District stretches from Hatch Road in the south to Tuolumne River in the north. This area is a composite of Industrial Park, Community Commercial and Mixed Use 2 (residential, business park, recreational).

Due to its close proximity to golf course and river, this District will be more natural in character. The rich floral character will be maintained and enhanced. The zone can support heavier landscaping as it will enhance the desired ambiance and would not interfere with the area's land uses (no commercial uses). The design character of this zone will be expressive of conventional, simple, natural, village atmosphere. The architecture shall be blended into and in harmony with the landscape.

NATURAL CHARACTER, VILLAGE ATMOSPHERE
FOR BUSINESS PARK & RESIDENTIAL



SIMPLE CONVENTIONAL STYLE FOR
COMMERCIAL AND BUSINESS PARK



1. Building Form

- Building forms should be of simple geometry with traditional rectangular forms.
- Buildings will be one to two stories, and pedestrian oriented in scale.
- Courtyards, atriums and outdoor gathering and eating areas are encouraged. Creating outdoor vitality will improve the pedestrian experience.

2. Building Materials

- The basic building material should be restricted to 2 or 3 elements for each building to emphasize the simple natural design concept.
- Use natural materials such as wood, concrete and stone as the primary building materials.
- The following materials are permitted:

- Poured-in-place, tilt-up, or precast concrete.
- Medium textured exterior plaster.
- Wood Siding
- Wood shakes or shingles
- Metal panels, such as "CorTen"
- Concrete block with medium texture finish.
- Stone products, i.e., granite marble.
- Brick
- Non-reflective glass.

- The following materials are not permitted:
 - Silver or "shiny" reflective glass
 - Fiberglass
 - Plastic
 - Plywood or unfinished wood
 - Corrugated metal
 - Asphalt shingles
- Any material not listed above will be subject to approval by the Planning Commission.

3. Colors

- Overall color theme should be earthtone colors. Examples include: tan, beige, brown (all shades), rust, yellow, orange, brick red, and forest green. Extreme colors shall not be used in the overall color theme.
- Accent colors at building entries, covered walls and for special details, should contrast the base colors.
- Harmonious and complimentary colors will be used. Dissident or distracting colors will be avoided. Highly contrasting graphic patterns on facades will not be allowed.
- Richer shades shall be limited to signs, doors, window trim, and other detailing related to pedestrian areas.

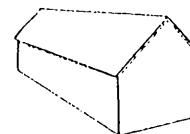
4. Roofs

- In the business complexes, simple gable or hip roofs are preferred over flat roofs. In the industrial complexes, flat roofs are preferred. In general a mixture and contrast of roof style is desired.
- High quality, dark colored roofing, such as sealed nonreflective metal, wood shingles or shakes, clay or concrete tile and asphalt/concrete composites are recommended.
- Shade structures, such as heavy timber trellises or ivy covered walls are encouraged.
- All mechanical equipment should be screened from view or placed at ground level in rear yard service areas where feasible.

5. Walls and Windows

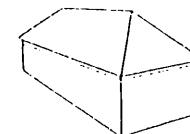
- In industrial complexes windows should be limited to building front and entry area.
- In business complexes, window placement should maximize view potential.
- Large recessed windows may be used at the entryways.

RECOMMENDED BUSINESS
PARK ROOF STYLE

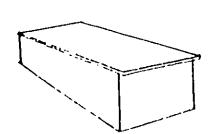


GABLE ROOF

RECOMMENDED INDUSTRIAL
PARK ROOF STYLE

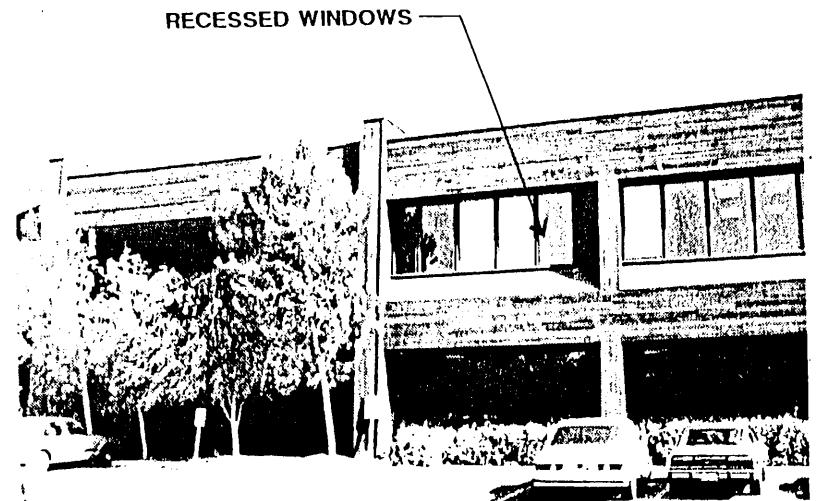


HIP ROOF



FLAT ROOF

RECESSED WINDOWS



IV. LANDSCAPE GUIDELINES

A. LANDSCAPE CONCEPT

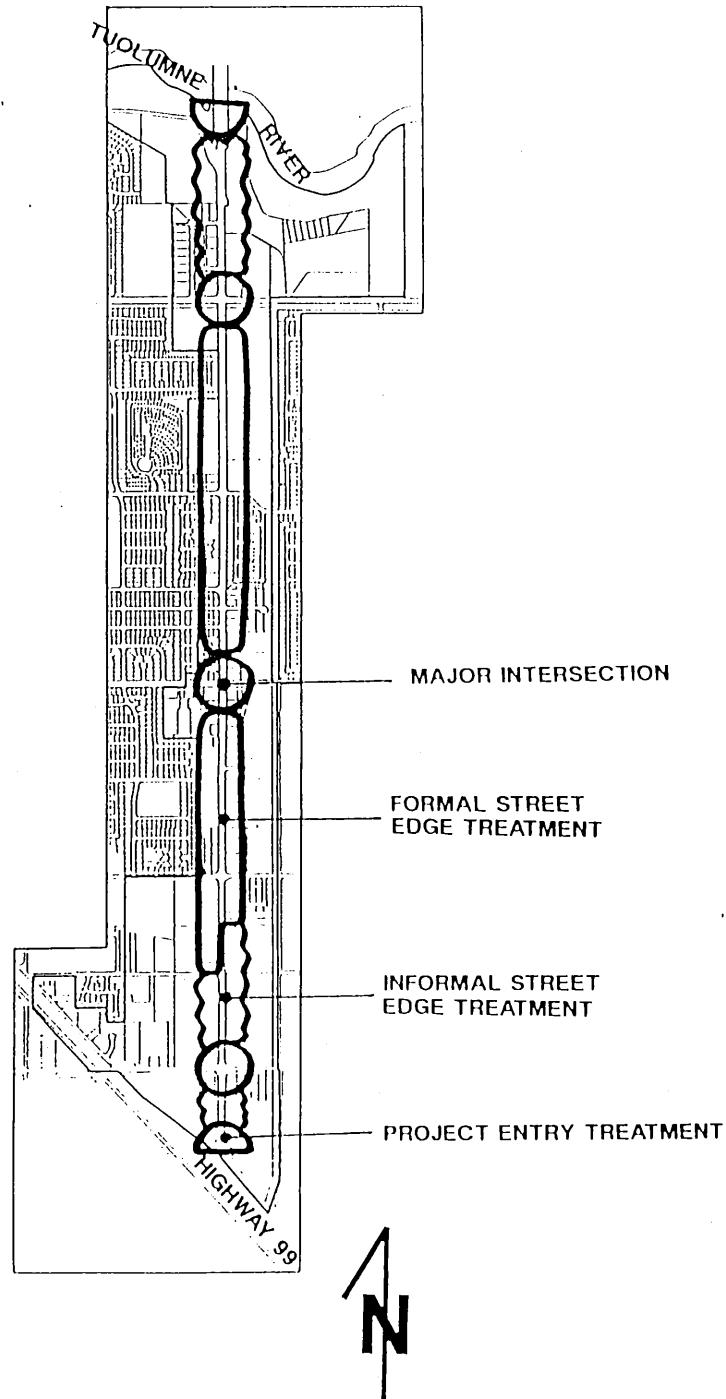
The landscape treatments will further define the image and provide an overall visual linkage for Mitchell Road. Flexibility is needed due to the limitation of existing conditions. The corridor is divided into six major categories of landscape treatments: street edges (formal and informal), project entry, major intersections, parking lots, building perimeters and landscape buffers. Other areas of treatments included in this section are: walls and fences, street furniture and pavement and recreation facilities. These Landscape Guidelines supplement the Ceres Water Efficient Landscape Standards and Guidelines.

B. STREET EDGE LANDSCAPING

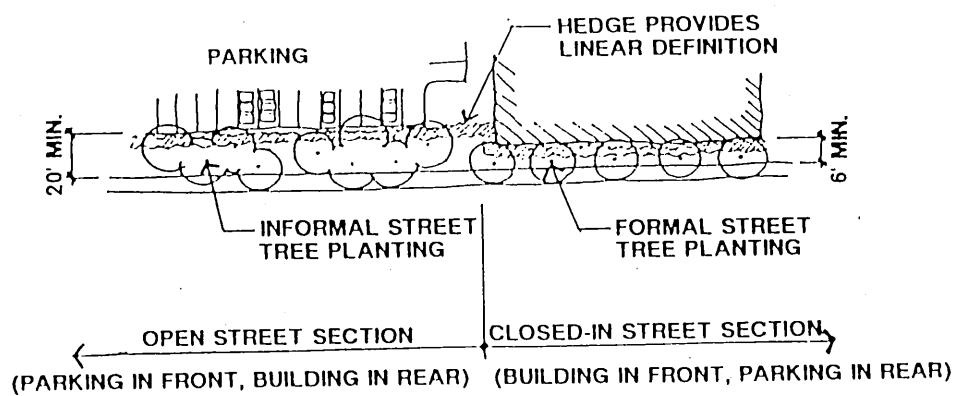
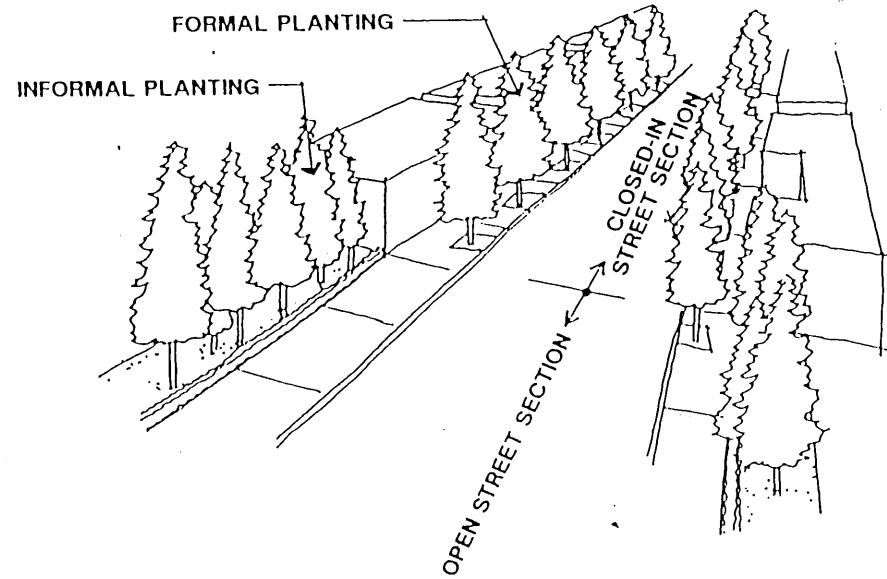
The overall image of Mitchell Road is set by the vertical tree planting along the street edge, either within the street right-of-way or on the private properties. The vertical trees provide spatial definition for the corridor and serve as visual linkage throughout the site.

Guidelines

- Every effort should be made by individual developments to integrate with the streetscape plantings.
- *Pinus canariensis* (Canary Island Pine - a vertical evergreen tree) is the theme tree for Mitchell Road. Other vertical pines may be substituted subject to the approval of the City.



- Pistacia chinensis (Chinese Pistache - a vertical deciduous tree) will be used as accent tree for the intersections or entry areas.
- Formal planting is encouraged along the closed-in section of Mitchell Road where buildings or walls are abutting the street right-of-way. A minimum six-foot (6) landscape setback is required for the tree planting for the H-C Zone and 15 feet for all other zones.
- Informal planting or randomly clustered planting is encouraged for the open section of the road such as at the south gateway and north gateway zone. A minimum of twenty feet (20) of landscape setback is required.
- One tree planting is required for maximum every thirty-five (35) linear feet for formal planting and maximum every twenty-five (25) linear feet for informal planting.
- All trees shall be minimum 15 gallon size or larger. Variation of sizes is encouraged for the informal treatment areas.
- Along commercial areas, tree planting shall allow visual access to store signage.
- Berm and hedge are encouraged along Mitchell Road to provide linear Globe definition.



C. PROJECT ENTRY TREATMENT

Special treatments should occur at the two ends of the corridor to identify these areas as major points of entry to the corridor and set the tone for the thoroughfare.

Guidelines

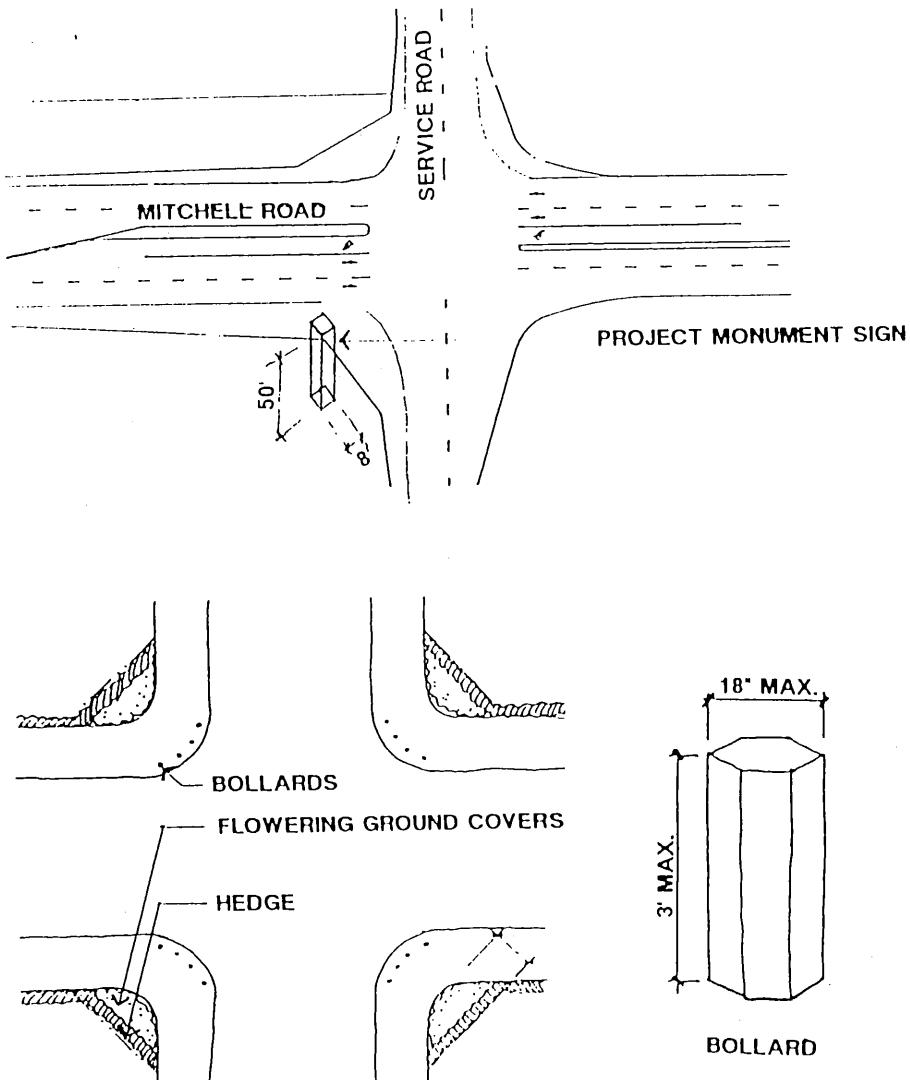
- The project entries will have monument signs which will serve as vertical accents. The monument sign symbolizes the gateway to the city and the high concentration of activities.
- Shrubs and ground covers with annual or perennial color should be used to highlight key areas, such as at the base of Project Entry Monument Signs.
- Clear views for traffic safety and project signage will be maintained.

D. MAJOR INTERSECTION TREATMENT

Major intersections will receive special treatments at the ground level of the street corners. The three major intersections along Mitchell Road are at Hatch Road, Whitmore Avenue and Service Road.

Guidelines

- Street corners will be accented with flowering ground covers, enriched paving (such as stamped concrete) and bollards.



- A minimum of five (5) feet landscape setback measured from the back of sidewalk is required for corner treatment.
- The four corners of the street intersection shall receive the same treatments.

E. PARKING LOT LANDSCAPING

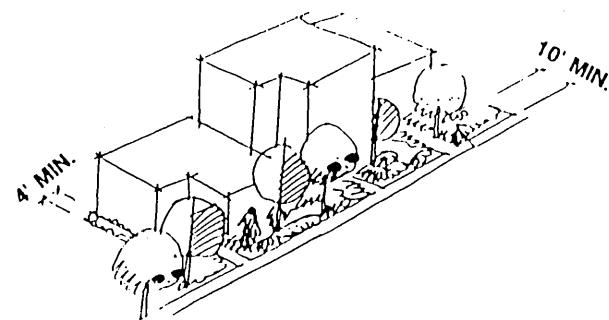
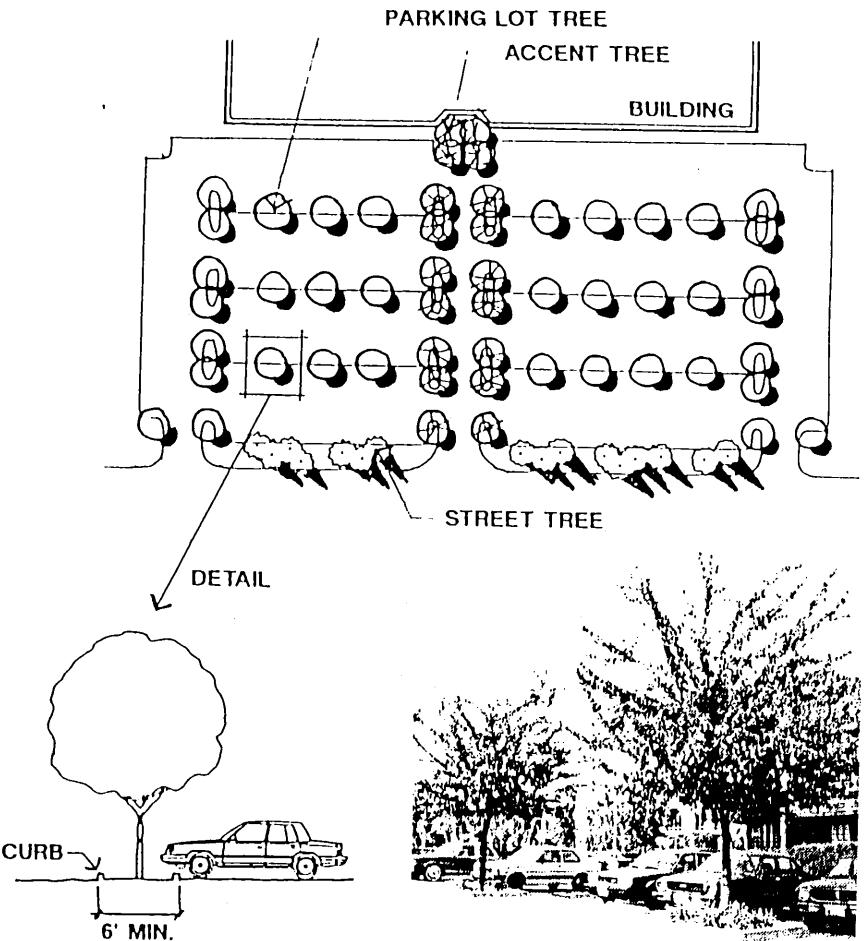
Landscape treatments at the parking lots will soften the large areas of paving, provide shade for the cars while allowing for visual penetration to the store front.

Guidelines

- Each parking lot shall be planted with predominately one type of evergreen or deciduous canopy trees.
- No more than one type of accent trees for the entry or major driveway areas are allowed.
- Trees will be planted at a rate of one for every eight (8) parking spaces at a minimum.
- Parking lot planting "islands" between bays of parking will be curbed and shall measure six feet (6') from the outside edge of the curb.

F. BUILDING PERIMETER LANDSCAPING

Perimeter landscaping around all buildings, with the exception of window display areas (retail uses) and areas where sidewalks abut buildings (i.e., retail plazas), shall be provided.



Landscaping around the building perimeter will focus in the ground treatments at the pedestrian level.

Guidelines

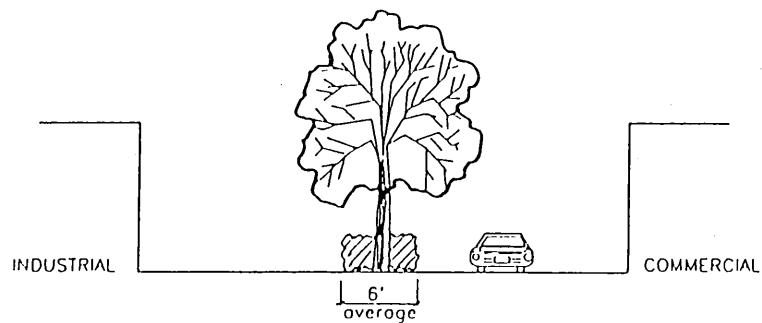
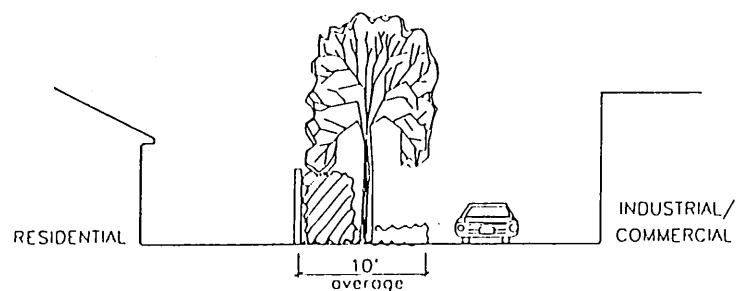
- Trees and vines should be planted to soften large, plain building walls.
- Along commercial areas, tree planting shall allow visual access to store signage, and shall not obscure entryways.
- A minimum ten-foot (10') wide planting area is required in perimeter areas where trees are planted. A minimum four-foot (4') wide planting area is required in perimeter areas where shrubs and ground cover are planted without trees.

G. LANDSCAPE BUFFER

Landscape buffer is required where different land uses abut.

Guidelines

- A minimum of ten feet (10') buffer zone is required where commercial or industrial park uses abut residential uses.
- A minimum of six feet (6') buffer zone is required between other land uses.
- Evergreen trees or tall shrubs with dense foliage is encouraged.



H. WALLS AND FENCES

Walls and fences provide property definition noise attenuation and visual screening. In the Mitchell Road Corridor, especially along the Community Commercial Design District, walls and fences represent an important part of the overall streetscape concepts. Limited use of berms and /or mounds may be employed as a visual feature to break up long flat stretches.

Guidelines

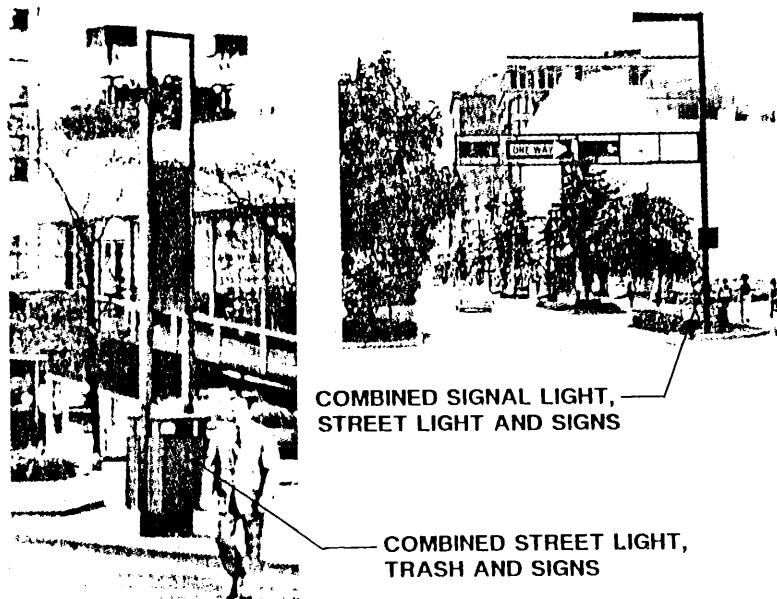
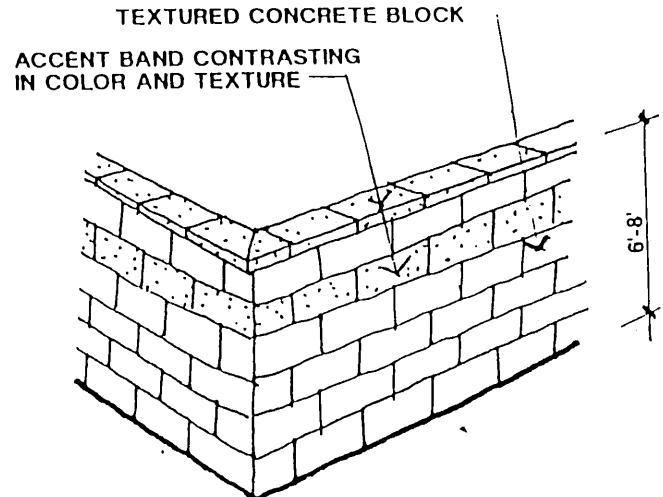
- Walls and fences are required where residential uses are adjacent to major streets or abut commercial or industrial park uses.
- Walls along Mitchell Road shall be solid masonry walls with simple design and pastel earthtone color.
- Walls and fences shall be of a minimum of six feet (6') in height and a maximum eight feet (8') in height unless a higher height is required by a noise analysis.

I. STREET FURNITURE AND PAVEMENT

Special pavement and street furniture are encourage at major activity nodes such as street intersections, entries and courtyards.

Guidelines

- Special paving treatments shall be rich in texture and color, such as stamped concrete or brick pavers.



- Street furniture is not encouraged along the streets except benches in areas of high pedestrian use and to facilitate dial-a-ride and future mass transit users. However, it is encouraged for the activity nodes such as courtyards.
- Where possible, the street furniture shall be clustered and multiple use furniture is encouraged to simplify the design and avoid cluttering. For example, combination of light post and trash can, combination of planter and bench, etc.

V. SIGN STANDARDS

A. GENERAL SIGN GUIDELINES

This section describes the city's standards regarding signs as provided in Chapter 18.42 of the Ceres Municipal Code, the city's Sign Ordinance. With the additions and/or modifications described below, the regulations and standards of the Sign Ordinance also apply to properties in the Mitchell Road Corridor. A sign program shall be approved by the Planning Commission during the Specific Plan Site Plan Review process for new construction of commercial and industrial buildings with four or more tenants. Individual signs, excluding those that are exempt, are reviewed by the Planning Department.

These standards establish a system of reasonable, nondiscriminatory criteria to regulate and control the size, location, type and quality of both permanent and temporary signs within the corridor.



Guidelines:

1. The sign guidelines are specifically established in the Ceres Sign Ordinance, Chapter 18.42 of the Ceres Municipal Code. The Ceres Sign Ordinance is conveyed in a more readable form, including illustrations, in the *Property Owner's and Business Owner's Guide to Sign Standards in the City of Ceres*.

VI. LIGHTING GUIDELINES

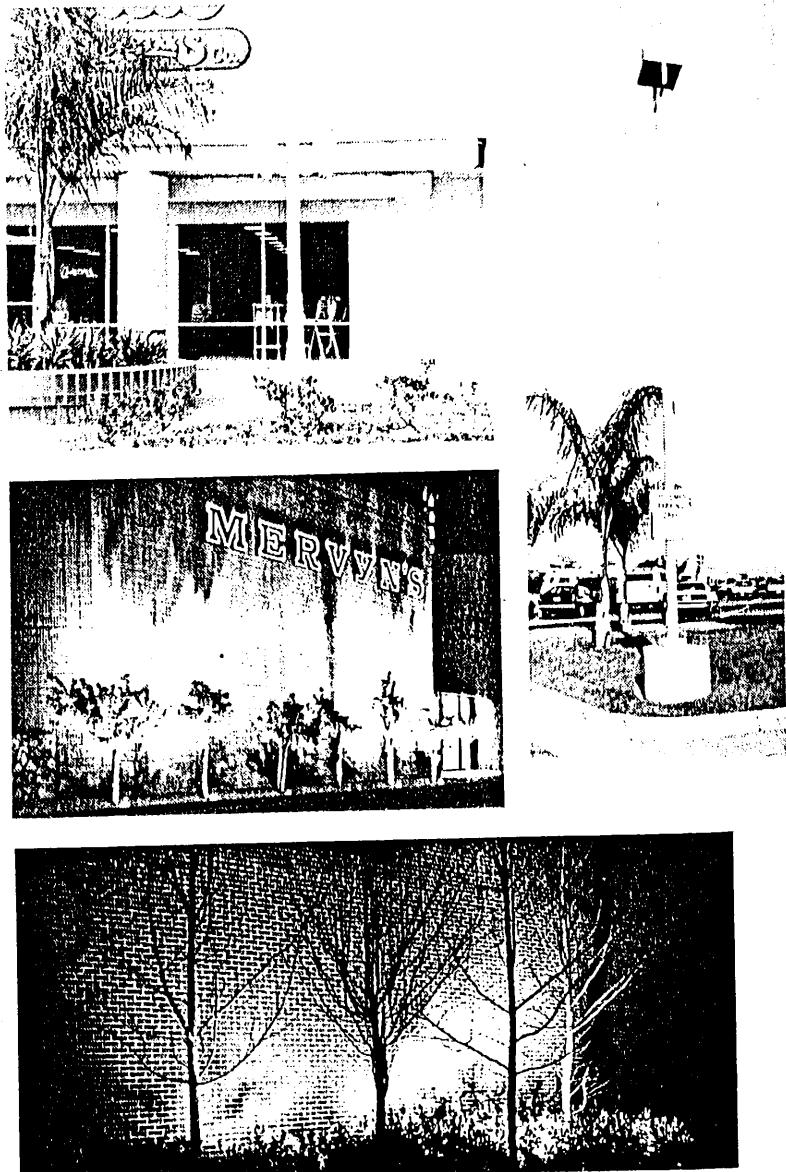
A. GENERAL LIGHTING CONSIDERATIONS

In the design lighting for the Mitchell Road Corridor project area consideration should be given to the careful siting of lights to emphasize safety and coordination with projects architectural character.

Lighting plays an important role in defining the nighttime appearance of the project area. The lighting concept provides a hierarchy of lighting effects which will contribute to the overall cohesiveness of the community image. Roadway, parking, building, pedestrian and landscape lighting will be identified.

Guidelines:

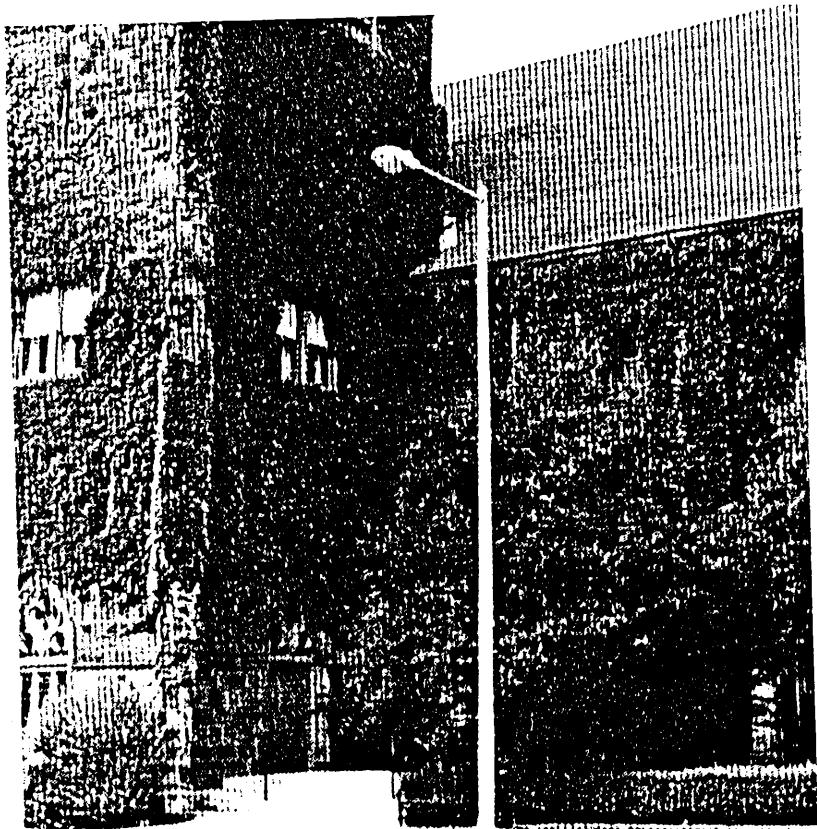
- o Warm white or high pressure sodium lighting is encouraged. Bright colored or blinking lights shall not be allowed.



- Light standards should be attractive to look at during daytime hours when the pole, base and light add an architectural dimension to the street scene.
- Light standards should blend architecturally with buildings, pedestrian areas and other hardscape elements.
- Road light standards and fixtures shall be painted a uniform metallic gray or white color throughout the Mitchell Road Corridor. Project related lighting such as parking area lighting, entry area and pedestrian level lighting, and building or signage lighting should be painted to match project theme color.
- Fixtures and standards shall conform to state and local safety and illumination requirements.
- Design and placement of site lighting must minimize glare affecting adjacent properties, buildings, and roadways.
- Automatic timers on lighting should be designed to maximize personal safety during nighttime use while saving energy.
- A well coordinated lighting program will unify the lighting hierarchy and reinforce the perceived identity of the Mitchell Road Corridor.

B. ROADWAY LIGHTING

Lighting fixtures and standards within the public road right-of-way play a crucial role in defining the identity of the



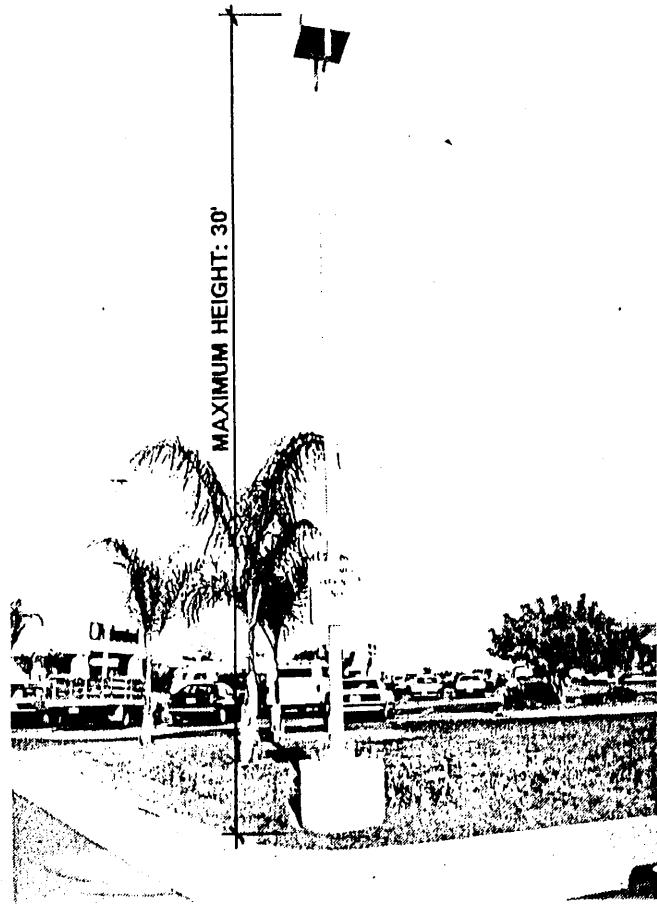
Mitchell Road Corridor and level or quality of the developments along streets. Streetlight and traffic signal standards should incorporate a design of simplicity, utilizing "cobra-head" lights on extended arms and metal standards.

Guidelines:

- Lighting should be positioned to enhance the safety of vehicular and pedestrian flows at key points along the roadway. Light should be concentrated at intersections and pedestrian crosswalks.
- The maximum height of roadway lighting shall be thirty feet with a minimum clearance of sixteen feet provided above roadways.
- All street lighting along Mitchell Road shall be of a singular design placed at regular intervals, custom mounted atop a concrete or metal pedestal, and installed per city specifications.
- If an eight foot sidewalk adjacent to Mitchell is provided along Mitchell Road, street light standards shall be placed within the three foot curbside buffer area.

C. PARKING AREA LIGHTING

Parking areas should be illuminated with soft light from numerous small fixtures. In order to transition the user from the roadway to the building the elevation of parking area light standards will be lower than roadway lights.



Guidelines:

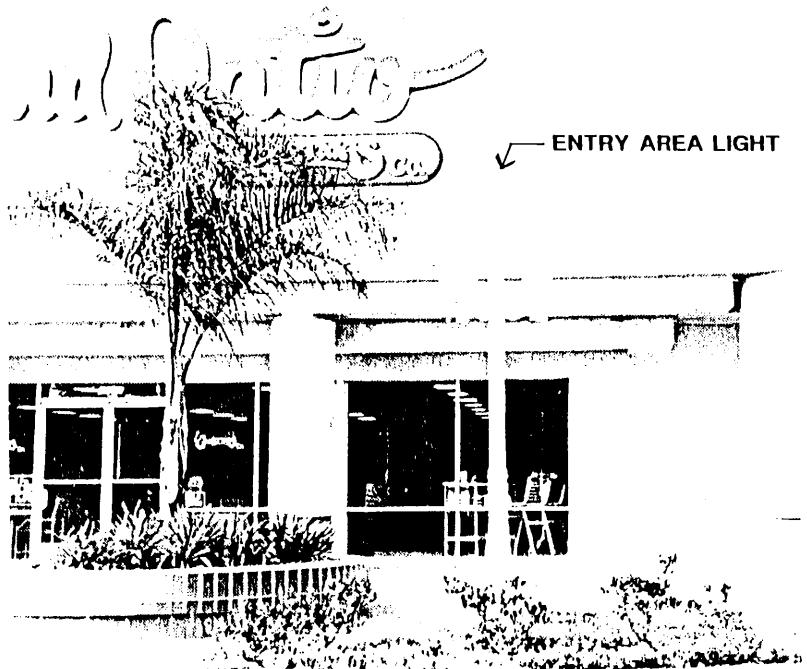
- Lighting that uses a zero cut off shield, such as the "shoe box" style fixtures, are encouraged for most parking areas.
- Retail buildings, offices, industrial, hotels, and restaurants must use lighting fixtures in parking areas that are a continuation of building and pedestrian level theme lights.
- The maximum height of parking area lights shall be thirty feet.
- Metal or concrete bases limited to eighteen inches in height may be used in parking area islands to protect lighting from damage.

D. ENTRY AREA AND PEDESTRIAN LEVEL LIGHTING

1. Entry Area Lighting

This lighting will provide illumination at plaza areas and main building entries.

- For all uses within the Mitchell Road Corridor entry area light standards should not exceed ten feet in height.
- All entry area lighting must compliment the architectural style of the adjacent buildings.



2. Pedestrian Level Lighting

This lighting will provide low-level lighting along sidewalks, drop-off points, and to further define entry areas.

- Pedestrian level lights may be placed in bollards, inset in walls, or on low level standards.
- This low-level lighting is preferred along walk-ways over the higher entry area lighting because it produces a more pedestrian point-to-point light pattern.
- These lights should be simple concrete, wood or metal material to complement the architectural style of the building.

E. BUILDING OR SIGNAGE HIGHLIGHTING

To highlight monument signs or architectural features such as walls, entryways, or lobbies dramatic lighting should be used.

1. Building Lighting

Wall and ceiling lights may be used to accentuate building entries. Spotlighting techniques may also be used in moderation.

- Wall lighting will be down-lit and shielded or use opaque white glass to minimize glare.

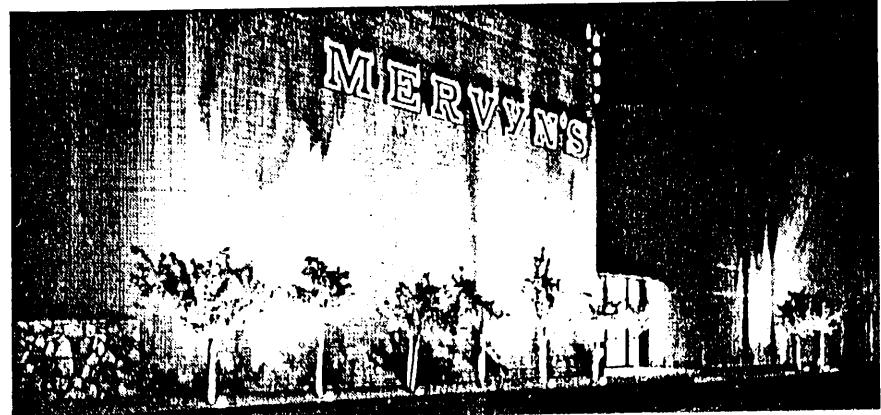
- The use of neon tube lighting to accentuate building roof lines, eaves or profiles shall generally not be encouraged except within the activity center and will be limited within the Corridor. Requests will be decided on a case by case basis.
- In direct wall lighting, "wall washing" techniques or overhead downlighting is encouraged.
- Building lighting should be carefully integrated into building details or concealed flush with grade and should not be visually apparent during the daytime.

2. Sign Lighting

- All lighted signs shall be so located or shielded to prevent glare to surrounding properties or public streets.
- Spotlighting of monument or building signs is permitted, but discouraged for individual tenant signs.

F. LANDSCAPE HIGHLIGHTING

Landscape lighting can be used to create special effects and to highlight landscape features such as specimen trees or pedestrian areas. As with architectural lighting, light sources must be concealed flush with grade or otherwise concealed during the daytime. Fixtures shall be painted dark brown or black. String lights, with non-flashing small white bulbs, can be used to highlight trees within public plazas and outdoor eating areas at night to create excitement and a festive ambiance. The following techniques are encouraged:



- Downlighting: the most common and efficient technique to create the effect of sun or moon light is to use a concealed light source aimed straight down to highlight plant material or create patterns on ground surfaces.
- Uplighting: dramatic landscape effects can be achieved by placing lights on or in the ground to accent foliage or tree structure.
- Backlighting: subtly lighting the background wall, fence or rocky hillside allows plants to be seen in silhouette.

