

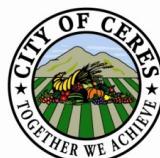
Click this link to join the webinar:

<https://us02web.zoom.us/j/84149698544?pwd=BiXobOO3dLv7d8CqjJOrdmeCpBBeSN.1>

Webinar ID: 841 4969 8544; Passcode: 921017; or Dial: (669) 900-6833

REMOTE PUBLIC COMMENT IS AVAILABLE FOR THE PLANNING COMMISSION MEETING BY EMAILING THE PLANNING DEPARTMENT AT PLANNING@CERES.GOV BEFORE 4:00 PM THE DAY OF THE MEETING. INCLUDE THE AGENDA ITEM NUMBER OR PUBLIC COMMENT PERIOD IN THE SUBJECT LINE OF THE EMAIL. THE CLERK MAY READ WRITTEN COMMENTS INTO THE RECORD, IF SPECIFICALLY REQUESTED TO DO SO AT THE BEGINNING OF YOUR EMAIL. YOUR WRITTEN COMMENT WILL BE DISTRIBUTED TO THE PLANNING COMMISSION AND KEPT ON FILE AS PART OF THE OFFICIAL RECORD OF THE PLANNING COMMISSION MEETING.

If you have any questions on how to attend virtually or are encountering difficulties connecting to the meeting, please contact the Planning Office at planning@ceres.gov.



AGENDA

PLANNING COMMISSION SPECIAL MEETING CITY OF CERES, CALIFORNIA

**City Council Chambers, 2701 Fourth Street
Tuesday, February 17, 2026, at 6:00 p.m.**

Mailing Address: Ceres City Hall, 2220 Magnolia Street, Ceres, CA 95307-3292
Phone: (209) 538-5774 Fax: (209) 538-5675

CALL TO ORDER

Chairperson Condit

ROLL CALL

Chair: Gary Condit
Vice Chair: R.J. Jammu
Commissioners: Bob Gobble; Francisco Mireles; Dorie Perez

PLEDGE OF ALLEGIANCE

Pledge of Allegiance led by Chairperson Condit.

AGENDA
Planning Commission Special Meeting
February 17, 2026
Page 2

CITIZEN COMMUNICATIONS to the Commission on matters not included on the agenda (five minutes).

While the Planning Commission welcomes and encourages participation in Planning Commission meetings, adopted rules allow no more than 5 minutes (Resolution No. 2012-132) for expression of non-agenda items. Matters under the jurisdiction of the Planning Commission, and not on the posted agenda, may be addressed by the general public; however, California law prohibits the Planning Commission from taking action on any matter which is not on the posted agenda unless it is determined to be an emergency by the Planning Commission. Citizens are entitled to address the Planning Commission on any agenda item subject to the 5-minute provision.

CONFLICT OF INTEREST DECLARATION

CONSENT CALENDAR

All matters listed on the consent calendar are considered routine in nature and will be enacted by a single motion unless otherwise requested by an individual Commissioner or the public for special consideration. Otherwise, the recommendation of staff will be accepted and acted upon by roll call vote.

1. Clerks Report of Posting. The agenda of the February 17, 2026, Planning Commission Special Meeting was Posted on Wednesday, February 11, 2026. *(Planning Technician)*
2. Waive Readings. All Readings of Ordinances and Resolutions are Waived. (Waive readings in full of all ordinances and resolutions on the agenda and declare that said titles which appear on the public agenda shall be determined to have been read by title). *(Planning Technician)*
3. Approval of Minutes for the February 2, 2026, Regular Planning Commission Meeting. *(Planning Technician)*

CONSIDERATION OF ITEM(S) REMOVED FROM THE CONSENT CALENDAR

PUBLIC HEARING

4. Adopt an Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15164; and

Adopt a Resolution approving a Tentative Parcel Map for 1670 Mitchell Road (APN: 069-011-009).

CEQA Status: Addendum to the Mitchell Road Corridor Specific Plan Project Impact Report (SCH # 1988053107) per CEQA Section 15164

Required Action: Adopt PC Resolution 2026-XX

AGENDA
Planning Commission Special Meeting
February 17, 2026
Page 3

5. Determine that the project is consistent with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH# 1988053107) pursuant to CEQA Guidelines Section 15162; and

Adopt a Resolution approving 25-44 Conditional Use Permit and 25-47 Site Plan Application for the Dutch Bros Coffee Shop located at 1670 Mitchell Road (APN: 069-011-009).

CEQA Status: Determination of Consistency with the Addendum to the Mitchell Road Corridor Specific Plan Project Impact Report (SCH # 1988053107) per CEQA Section 15162

Required Action: Adopt PC Resolution 2026-XX

6. Determine that the project is consistent with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH# 1988053107) pursuant to CEQA Guidelines Section 15162; and

Adopt a Resolution approving 25-31 Conditional Use Permit and 25-48 Site Plan Application for Take 5 Oil Change located at 1670 Mitchell Road (APN: 069-011-009).

CEQA Status: Determination of Consistency with the Addendum to the Mitchell Road Corridor Specific Plan Project Impact Report (SCH # 1988053107) per CEQA Section 15162

Required Action: Adopt PC Resolution 2026-XX

UNFINISHED BUSINESS

None

NEW BUSINESS

None

MATTERS INITIATED BY PLANNING COMMISSION AND STAFF

None

REPORTS

At this time, any Commissioner or City staff will make an announcement, or report briefly on his/her activities.

- Chairperson
- Commission
- City Attorney
- City Staff

ADJOURNMENT

The next regularly scheduled Planning Commission Meeting will be held on March 2, 2026, at 6:00 p.m. in the City Council Chambers located in the Community Center at 2701 Fourth Street, Ceres, CA.

ANY DECISION OF THE PLANNING COMMISSION CAN BE APPEALED TO THE CITY COUNCIL

Such an appeal of a Planning Commission action from this meeting must be filed by 5:00 p.m., February 27, 2026. Any person who challenges any of the following actions in court, may be limited to raising only those issues that they or someone else raised at the Public Hearing, or in written correspondence delivered to the City of Ceres at, or prior to, the Public Hearing. The appeal must be filed in writing with a \$1,000.00 filing fee.

AFFIDAVIT OF POSTING

I, Stephani Davis, Planning Technician, for the City of Ceres, declare under penalty of perjury that the foregoing agenda for the Regular Planning Commission Meeting was posted on Wednesday, February 11, 2026, at the following locations in Ceres:

- Community Center Display Case – 2701 Fourth Street



Stephani Davis, Planning Technician

Planning Commission Agenda:

The Planning Commission agenda is available for public review on the City's website at www.ci.ceres.ca.us and posted at the time and place noted above.

Related Materials:

Any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda will be made available for public inspection at the Planning Division at City Hall located at 2220 Magnolia Street, Ceres, CA during normal business hours. Persons with questions concerning any agenda item may call the Planning Division at (209) 538-5774 or email at planning@ceres.gov.

AGENDA
Planning Commission Special Meeting
February 17, 2026
Page 5

Notice regarding Americans with Disabilities Act:



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting or if you need the agenda and/or the documents in the agenda packet provided in an alternative format, please contact the city clerk's office at (209) 538-5731 at least 48 hours prior to the meeting to ensure that reasonable arrangements can be made (28CFR 35.102-35.104 ADA TITLE II).

Únete al seminario web de ZOOM para participar en vivo en:
<https://us02web.zoom.us/j/84149698544?pwd=BiXobOO3dLv7d8CqjJOrdmeCpBBeSN.1>
ID del seminario web: 841 4969 8544; Código de acceso: 921017; or Llame: (669) 900-6833

PARA PARTICIPAR EN LA SESIÓN DE LA COMISIÓN DE PLANIFICACIÓN VIA COMMENTARIO PÚBLICO REMOTE ENVÍE UN CORREO ELECTRÓNICO A PLANNING@CERES.GOV ANTES DE LAS 4:00 P.M. EL DÍA DE LA REUNIÓN. INCLUYA EL NÚMERO DEL PUNTO DE LA AGENDA O MENCIONE "PERÍODO DE COMENTARIOS DEL PÚBLICO" EN EL ASUNTO DEL CORREO ELECTRÓNICO. LA SECRETARIA PODRÁ LEER LOS COMENTARIOS POR ESCRITO DURANTE LA SESIÓN, SI ASÍ LO SOLICITA EXPRESAMENTE AL COMIENZO DE SU CORREO ELECTRÓNICO. SU COMENTARIO ESCRITO SE DISTRIBUIRÁ A LOS MIEMBROS DE LA COMISIÓN DE PLANIFICACIÓN Y SE ARCHIVARÁ COMO PARTE DEL ACTA OFICIAL DE LA REUNIÓN.

Si tiene alguna pregunta sobre cómo asistir virtualmente o si tiene dificultades en conectarse a la reunión, comuníquese con el División de Planificación a través de la dirección de correo electrónico planning@ci.ceres.ca.us



AGENDA

REUNIÓN ESPECIAL DE LA COMISIÓN DE PLANIFICACIÓN CIUDAD DE CERES, CALIFORNIA

**Sala del Concejo Municipal, 2701 Fourth Street
Martes, 17 de Febrero de 2026, a las 6:00 p.m.**

Dirección de envío: Ceres City Hall, 2220 Magnolia Street, Ceres, CA 95307-3292
Teléfono: (209) 538-5774 Fax: (209) 538-5675

LLAMAR AL ORDEN

Presidente Condit

PASA DE LISTA

Presidente: Gary Condit
Vicepresidente: R.J. Jammu
Comisionados: Bob Gobble; Francisco Mireles; Dorie Perez

JURAMENTO A LA BANDERA

Juramento a la bandera dirigido por Presidente Condit.

COMENTARIOS PÚBLICOS a la Comisión sobre asuntos no incluidos en la agenda (cinco minutos).

AGENDA
Reunión Especial de la Comisión de Planificación
17 de Febrero de 2026
Página 2

La Comisión de Planificación da la bienvenida y fomenta la participación en sus reuniones. Las reglas adoptadas permiten un máximo de 5 minutos (Resolución n.º 2012-132) para la expresión sobre temas no incluidos en la agenda. El público general puede dirigirse aseverar de asuntos que estén bajo la jurisdicción de la Comisión de Planificación y que no estén incluidos en la agenda que fue publicada; sin embargo, la ley de California prohíbe que la Comisión de Planificación tome decisiones sobre cualquier asunto que no esté en la agenda publicada, a menos que la Comisión de Planificación lo declare como una emergencia. Cuidadosos tienen el derecho a dirigirse a la Comisión de Planificación sobre cualquier punto de la agenda, con la limitación de 5 minutos.

DECLARACIÓN DE CONFLICTO DE INTERESES

CALENDARIO DE CONSENTIMIENTO

Todos los asuntos incluidos en el Calendario de Consentimiento se consideran de rutina ordinaria y se aprobarán por una sola moción, a menos que algún Comisionado o miembro del público solicite su consideración por separado. Si no, se aceptará la recomendación del personal y se procederá a la votación nominal.

1. Informe de la Secretaría Municipal sobre la publicación de la agenda. La agenda de la Reunión Especial de la Comisión de Planificación del 17 de Febrero de 2026 se publicó el miércoles 11 de Febrero de 2026. (*Técnica de Planificación*)
2. Renuncia procesal de lectura. Renuncia de la lectura de todas las ordenanzas y resoluciones. (Se renuncia la lectura completa de todas las ordenanzas y resoluciones incluidas en la agenda y se declara que los títulos mencionados en la agenda pública se considerarán leídos por título). (*Técnica de Planificación*)
3. Aprobación de la acta de la Reunión Regular de la Comisión de Planificación del 02 de Febrero de 2026. (*Técnica de Planificación*)

CONSIDERACIÓN DE LOS ASUNTOS RETIRADOS DEL CALENDARIO DE CONSENTIMIENTO

AUDIENCIA PÚBLICA

4. Adoptar un Adenda al Informe de Impacto Ambiental del Plan Específico del Corredor de Mitchell Road (SCH # 1988053107) conforme a la Sección 15164 de las Directrices CEQA; y

Adoptar una Resolución aprobando un Mapa Provisional de Parcelas para 1670 Mitchell Road (APN: 069-011-009).

Estado CEQA:

Apéndice al Informe de Impacto del Plan Específico del Corredor de Mitchell Road (SCH # 1988053107) según la Sección 15164 de CEQA

AGENDA
Reunión Especial de la Comisión de Planificación
17 de Febrero de 2026
Página 3

Acción requerida: Adoptar la Resolución del PC 2026-XX

5. Determinar que el proyecto es coherente con el Anexo al Informe de Impacto Ambiental del Proyecto del Plan Específico del Corredor de Mitchell Road (SCH# 1988053107) conforme a la Sección 15162 de las Directrices CEQA; y

Adoptar una Resolución aprobando la Solicitud del Permiso de Uso Condicional 25-44 y el Plano de Ubicación 25-47 para la cafetería Dutch Bros situada en 1670 Mitchell Road (APN: 069-011-009).

Estado CEQA: Determinación de la coherencia con el anexo al Informe de Impacto del Plan Específico del Corredor Mitchell Road (SCH # 1988053107) según la Sección 15162 de CEQA

Acción requerida: Adoptar la Resolución del PC 2026-XX

6. Determinar que el proyecto es coherente con el Anexo al Informe de Impacto Ambiental del Proyecto del Plan Específico del Corredor de Mitchell Road (SCH# 1988053107) conforme a la Sección 15162 de las Directrices CEQA; y

Adoptar una Resolución aprobando la solicitud del Permiso de Uso Condicional 25-31 y del Plan de Ubicación 25-48 para el cambio de aceite Take 5 ubicados en 1670 Mitchell Road (APN: 069-011-009).

Estado CEQA: Determinación de la coherencia con el anexo al Informe de Impacto del Plan Específico del Corredor Mitchell Road (SCH # 1988053107) según la Sección 15162 de CEQA

Acción requerida: Adoptar la Resolución del PC 2026-XX

ASUNTOS PENDIENTES

Ninguno

ASUNTOS NUEVOS

Ninguno

ASUNTOS INICIADOS POR LA COMISIÓN DE PLANIFICACIÓN Y EL PERSONAL

Ninguno

INFORMES

En este momento, cualquier miembro de la Comisión de Planificación o del personal municipal podrá hacer un anuncio o informar brevemente sobre sus actividades.

- Presidente
- Comisionados
- Abogada Municipal
- Personal Municipal

APLAZAMIENTO

La próxima reunión regular de la Comisión de Planificación será el 02 de Marzo de 2026, a las 6:00 p.m., en la sala del Consejo Municipal ubicada en el Centro Comunitario, en 2701 Fourth Street, Ceres, CA.

CUALQUIER DECISIÓN DE LA COMISIÓN DE PLANIFICACIÓN PUEDE SER APELADA ANTE EL CONCEJO MUNICIPAL

Cualquier apelación contra una decisión de la Comisión de Planificación tomada en esta reunión deberá presentarse antes de las 5:00 p.m. del 27 de Febrero de 2026. Cualquier persona que impugne judicialmente cualquiera de las siguientes decisiones podrá estar limitada a plantear únicamente las cuestiones que ellos u otra persona hayan planteado en la Audiencia Pública, o en la correspondencia escrita entregada al Ayuntamiento de Ceres en la fecha de la Audiencia Pública o con anterioridad a la misma. La apelación deberá presentarse por escrito, acompañada de una tarifa de presentación de \$1,000.00.

DECLARACIÓN JURADA DE PUBLICACIÓN

Yo, Stephani Davis, Técnica de Planificación de la Ciudad de Ceres, declaro bajo pena de perjurio que la agenda para la Sesión Regular de la Comisión de Planificación fue publicada el miércoles 11 de Febrero de 2026 en los siguientes lugares de Ceres:

- Vitrina del Centro Comunitario – 2701 Fourth Street



Stephani Davis, Técnica de Planificación

web de la ciudad en www.ci.ceres.ca.us y se publica en la fecha y los lugares indicados anteriormente.

Materiales relacionados:

Cualquier escrito o documento proporcionado a la mayoría de la Comisión de Planificación en relación con cualquier punto de esta agenda estará disponible para inspección pública en la División de Planificación, ubicada en el Ayuntamiento, en 2220 Magnolia Street, Ceres, CA, durante el horario comercial normal. Las personas que tengan preguntas sobre cualquier punto de la agenda pueden llamar a la División de Planificación al (209) 538-5774 o envía un correo a planning@ceres.gov.

Act: Aviso acerca de la Ley de Estadounidenses con Discapacidades:



En conformidad con la Ley para Estadounidenses con Discapacidades, si necesita asistencia especial para participar en esta reunión o si necesita que la agenda o los documentos incluidos en el paquete de la agenda se le proporcionen en una forma alternativa, comuníquese con la oficina de la secretaría municipal al (209) 538-5731 con al menos 48 horas en anticipación a la reunión para garantizar que se puedan realizar los arreglos necesarios (28 CFR 35.102-35.104, Título II de la ADA).



DRAFT MINUTES
PLANNING COMMISSION REGULAR MEETING
CITY OF CERES, CALIFORNIA
(via ZOOM and in-person)

City Council Chambers, 2701 Fourth Street
Monday, February 2, 2026, at 6:00 p.m.

Mailing Address: Ceres City Hall, 2220 Magnolia Street, Ceres, CA 95307-3292
Phone: (209) 538-5700 Fax: (209) 538-5780

CALL TO ORDER

Chair Condit called the February 2, 2026, meeting to order at 6:00 p.m.

ROLL CALL

Chair: Gary Condit
Commissioners: Bob Gobble; R.J. Jammu; Francisco Mireles; Dorie Perez

Commissioner Gobble	Commissioner Jammu	Commissioner Mireles	Commissioner Perez	Chairperson Condit
Present	Absent	Present	Present	Present

PLEDGE OF ALLEGIANCE

Pledge of Allegiance led by Chair Condit.

ELECTION OF CHAIRPERSON AND VICE-CHAIRPERSON

Commissioner Perez nominated Chairperson Condit and Vice-Chairperson Jammu remain as Chairperson and Vice-Chairperson until the end of the year, when the term is up. Chairperson Condit accepted the nomination on behalf of himself and Vice Chair Jammu.

Action: Motion by **Chairperson Condit**, seconded by **Commissioner Perez**, approving **Chair Condit and Vice Chair Jammu**. Motion carried **4/0/1/0** by the following roll call vote:

Commissioner Gobble	Commissioner Mireles	Commissioner Perez	Chairperson Condit
Yes	Yes	Yes	Yes

CITIZEN COMMUNICATIONS to the Commission on matters not included on the agenda (five minutes)

Planning Technician Davis, read into record one email from Annie Montgomery, regarding administrative corrections to the Planning Commission Meeting minutes.

CONFLICT OF INTEREST DECLARATION

None

CONSENT CALENDAR

1. Clerks Report of Posting. The agenda of the February 2, 2026, Planning Commission Meeting was Posted on Wednesday, January 28, 2026. (*Planning Technician*)
2. Waive Readings. All Readings of Ordinances and Resolutions are Waived. (Waive readings in full of all ordinances and resolutions on the agenda and declare that said titles which appear on the public agenda shall be determined to have been read by title). (*Planning Technician*)
3. Approval of Minutes for the December 15, 2025, Regular Planning Commission Meeting. (*Planning Technician*)
* with administrative corrections included in the green sheet

Action: Motion by **Chair Condit**, seconded by **Commissioner Gobble**, approving items 1 – 3. Motion carried **4/0/1/0** by the following roll call vote:

Commissioner Gobble	Commissioner Jammu	Commissioner Mireles	Commissioner Perez	Chairperson Condit
Yes	Absent	Yes	Yes	Yes

PUBLIC HEARING

4. **Resolution No. 2026-XXX**, Recommending the City Council Adopt a Zoning Ordinance Text Amendment Adding Chapter 18.43, Cannabis Conditional Use Permits, to Title 18, Zoning, and Amending Applicable Chapters of Title 18, Zoning, of the Ceres Municipal Code. (*City Attorney*)

* Continued from the December 15, 2025, Regular Planning Commission Meeting.

City Attorney, Thomas Remlinger, introduced the item and gave a brief presentation.

Chairperson Condit opened the public hearing. The following members of the public spoke:

John Warren, Ceres citizen - spoke regarding allowable zones for the use and required length of retention for surveillance footage for these uses.

Chairperson Condit closed public comment.

Commission and staff discussion included minimum State requirements for surveillance video retention and clarification of Planned Community zoning in regard to cannabis uses.

Action: Motion by **Commissioner Perez**, seconded by **Commissioner Gobble**, approving item 4. Motion carried **4/0/1/0** by the following roll call vote:

Commissioner Gobble	Commissioner Jammu	Commissioner Mireles	Commissioner Perez	Chairperson Condit
Yes	Absent	Yes	Yes	Yes

UNFINISHED BUSINESS

None

NEW BUSINESS

None

MATTERS INITIATED BY PLANNING COMMISSION AND STAFF

None

REPORTS

- Chair

Chairperson Condit welcomes new Commissioners Gobble and Mireles and thanks the City Attorney for attending the Planning Commission meeting in person.

- Commission

Nothing to report.

- City Attorney

Nothing to report.

- City Staff

Nothing to report.

ADJOURNMENT

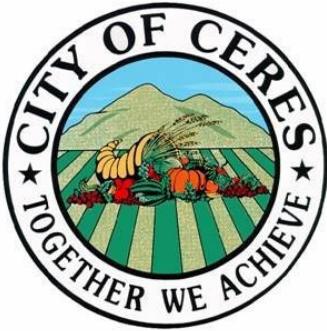
The next scheduled Planning Commission Meeting will be held on February 17, 2026, at 6:00 p.m. in the City Council Chambers located in the Community Center at 2701 Fourth Street, Ceres, CA.

There being no further business, Chair Condit adjourned the meeting at 6:24 p.m.

Gary Condit, Chair

Stephani Davis, Planning Technician

Consistent with Council Policy, the minutes referenced above are in Action Format. The complete recording of the meeting can be viewed at http://www.ci.ceres.ca.us/agenda_center.



City of Ceres Planning Commission
AGENDA REPORT

MEETING DATE: February 17, 2026

PROJECT NAME: Tentative Parcel Map for 1670 Mitchell Road

APPLICATION NUMBER: 25-46 TPM

RECOMMENDATION:

Staff recommend the Planning Commission:

1. Adopt an Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15164; and
2. Adopt a Resolution approving a Tentative Parcel Map for 1670 Mitchell Road (APN: 069-011-009).

PROJECT INFORMATION	
Applicant	Vintage Properties, LP
Property Owner	Vintage Properties, LP
Property Address	1670 Mitchell Road, Ceres, CA 95307
APN	APN: 069-011-009
Existing General Plan:	CC Community Commercial
Existing Zoning District:	CC Community Commercial
Existing Use:	Vacant shopping Center (former Wal-Mart)
Proposed Use:	No use proposed; subdivision of land for future uses.
Parcel Size	13.9 acres
Adjacent Uses:	North: E. Hatch Road, Commercial Uses South: Commercial Uses East: TID Canal; Single-Family Residential West: Mitchell Road; Commercial Uses; Single-Family Residential
CEQA Determination	Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107)

PROJECT DESCRIPTION AND LOCATION

The proposed project is a Tentative Parcel Map to subdivide an existing 13.9-acre commercial parcel (former Wal-Mart site) into seven additional parcels to support future commercial development and improvements, including individual commercial pads, and a large shared-use parcel accommodating parking, ingress/egress, utilities, and landscaping. The new parcels will range in size from 16,288 square feet to 438,790 square feet.

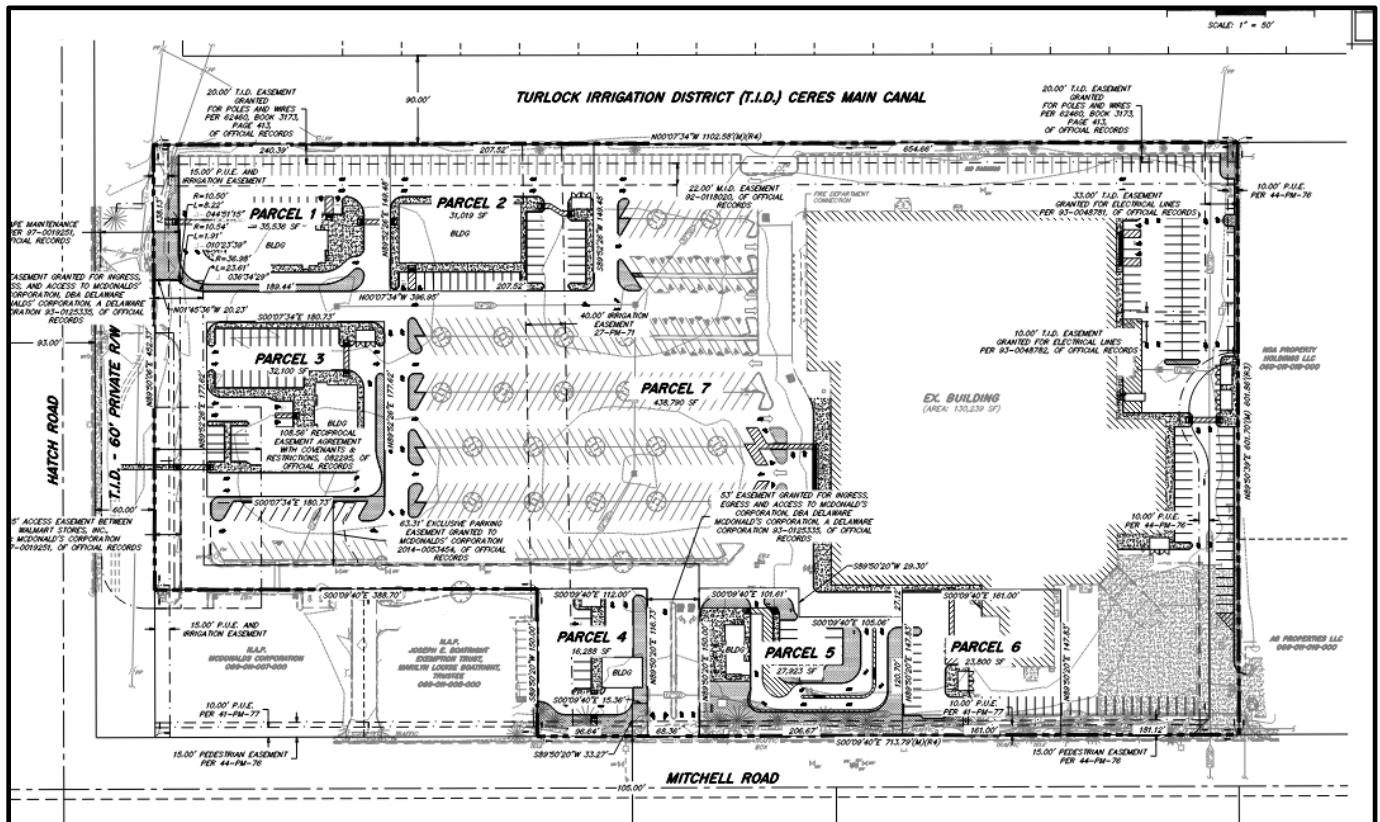


Figure 1: Tentative Parcel Map

The site is currently developed with an abandoned, one-story commercial building that is approximately three acres in size with associated parking. The site has been underutilized for an extended period of time and contains deteriorated paving, vacant structures, and aging infrastructure.

The existing McDonald's and the commercial building to the south of McDonald's are not part of the 13.9-acre shopping center as they are under different ownership and are different parcels. There is cross-access available between the project site and these two existing parcels.



Figure 2: Aerial of Project Site

PROJECT ANALYSIS

The proposed Tentative Parcel Map project has been analyzed with respect to conformance with:

1. Ceres General Plan
2. Mitchell Road Corridor Specific Plan
3. Ceres Municipal Code
4. California Environmental Quality Act (CEQA)

GENERAL PLAN CONFORMANCE

The project site has a land use designation of CC Community Commercial, as noted in Figure 3 below. The Community Commercial land use designation applies to the use of land for a full range of retail and service uses, including retail stores, food and drug stores, apparel stores, specialty shops, motor vehicle sales and service, home furnishings, real estate offices, restaurants, hotels, and other similar uses that serve a community-wide market. The goal of this land use is to provide convenient services and retail options to the community in one location to limit the need to go to multiple locations to meet a variety of retail needs.

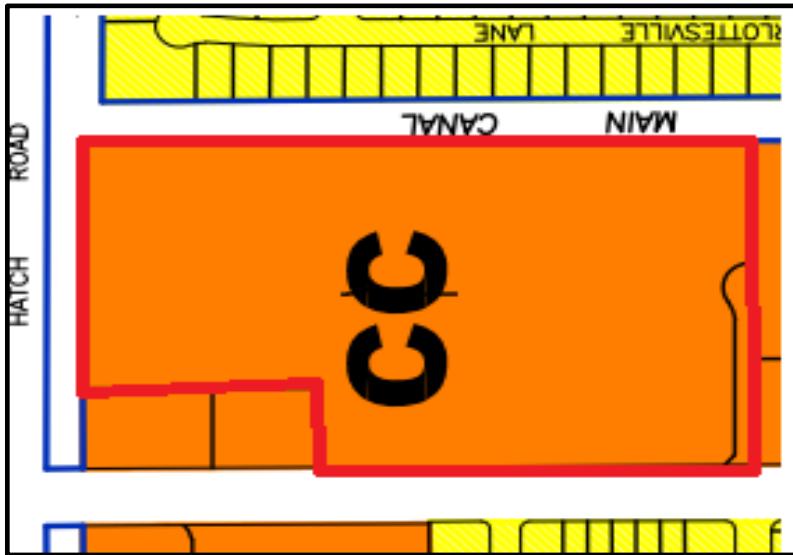


Figure 3: Land Use Diagram

The current site is home to the abandoned Wal-Mart, which closed its doors in 2021 once its new location opened on the northwest corner (NWC) of Service and Mitchell. The way this shopping center was constructed in the 1990s was based on the outdated model of “Big Box + Ocean of Parking”. Today’s pad-based centers reflect changes in consumer behavior, real estate economics and even city planning. The old model of shopping centers assumed that people would drive long distances to get to their destination and stay in one location for hours. Today, individuals want to drive a short distance, and make multiple small stops, not one long trip. By allowing the subdivision of the project site into multiple parcels for future pad buildings, the site will now support high-frequency, short-stay trips (like coffee, fast casual, clinics, banks, drive-throughs, car services).

With a shift toward online shopping, many big-box retailers have had to downsize, close, or convert their store into multiple tenant options just to sustain viability of the center. Oversized big box stores are becoming economically obsolete. Through the subdivision of this center, pad development will match current retailer needs of smaller format stores that are between 2,600-6,000 square feet and will help keep vacancies low. By making more pads on this site, the shopping center is still allowing a larger retailer to come, but the site becomes more marketable and more business friendly as new retailers are attracted to smaller spaces.

Most importantly, reducing the amount of concrete in a shopping center like this will help reduce the heat island effect, remove dead project frontages, and make a shopping center more walkable- all goals that the City strives to achieve every day. Through the approval of this tentative parcel map, this underutilized, underperforming center will become a commercial hub for this area of the City.

In addition to supporting the CC Community Commercial land use designation, the proposed Tentative Parcel Map is also consistent with the following goals and policies of the General Plan:

1. Infill Goals and Policies

Goal 2.D. Promote infill development to protect farmland; enhance community character; optimize City investment in infrastructure; provide pedestrian and bicycle friendly neighborhoods; and enhance economic vitality.

Policy 2.D.1. Promote Infill. Promote infill development and reuse of underutilized parcels in the city to reduce pressure to develop on farmland or other “greenfield” sites on the periphery.

Analysis: The tentative parcel map will facilitate the redevelopment of an existing, fully urbanized commercial site rather than expansion into agricultural or greenfield land on the City’s periphery. By enabling new commercial development on the new parcels, the project will absorb market demand within the City; it will reduce pressures to rezone or extend infrastructure into farmland areas; and most importantly aligns with the General Plan’s growth strategy of inward redevelopment first, outward expansion last.

Further, the redevelopment of this site will improve streetscape quality, safety, and the visual character of the Mitchell Road Corridor (discussed below). These improvements directly support the General Plan’s objective of replacing low-activation development with active commercial hubs in the City.

The pad-based development model will support short internal walking trips, multiple destination shopping, better building orientation, and improve existing sidewalks per the conditions of approval. The redevelopment of this infill site is only possible with this tentative map project which is why the parcelization of this site can be supported by the General Plan.

2. Economic Development Goals

Goal 7.A Support a wide range of economic activity in Ceres that capitalizes on the city’s location, strengthens the City’s tax base, and supports and enhances quality of life.

Analysis: It is no secret that parcelization makes redevelopment economically possible. Without parcelization, a site will stay vacant, the old big box will rot, investment in the area never comes, and growth shifts outward. Through this parcel map and the development that will ultimately come, the site will experience a resurgence of activity. The area will become attractive to investors, safety, or the perception thereof will increase, and jobs and business will come to the area.

Based on the above goals and policies, staff believes that the proposed project is supported by the General Plan.

MITCHELL ROAD CORRIDOR SPECIFIC PLAN

The project site is located in the Mitchell Road Corridor Specific Plan Area (MRCSP or Plan) and has a land use designation of Community Commercial. The Mitchell Road Corridor was adopted by the Ceres City Council in 1989 and Amended in 1995. The MRCSP establishes a comprehensive guide for development of about 450 acres located along a 2.5 mile stretch of Mitchell Road between Highway 99 and the Tuolumne River. The Plan provides development controls as it relates to design, site planning, architecture, circulation, open space and lighting throughout the corridor area. The goal of the plan is to guide development in a manner that will produce an urban corridor that integrates industrial, commercial, and residential uses into a vibrant community. The MRCSP is divided into seven land uses along the corridor that are meant to encourage specific goals or policies of the Plan. The area south of Hatch Road to Roeding Road is meant to support large-scale, region-service commercial uses. As such, the land use diagram notes this area as Community Commercial in Figure 4 below. Community Commercial is intended to accommodate those uses that serve the needs of the community for retail goods and services used on a daily basis.

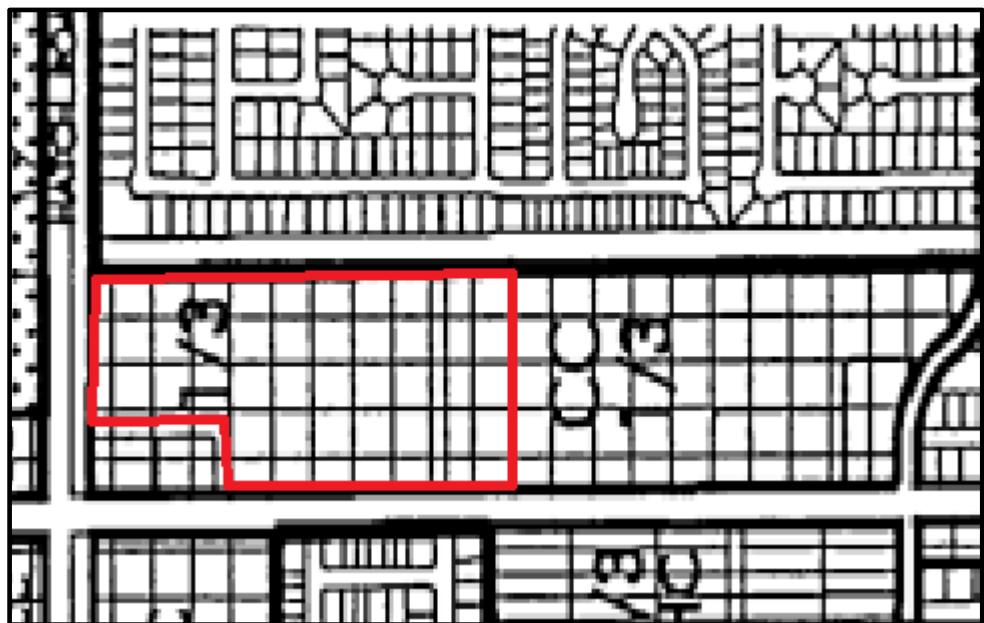


Figure 4: Specific Plan Designation

As discussed above, a parcel map will only enhance the site's ability to provide local and community-wide retail and services needed in this part of the City. With multiple commercial tenants, a range of retail and service uses, and shared access and parking, the Center will become an attractive place for business and visitors, creating the hub of local commercial activity the MRCSP hoped to see in this location.

The parcelization of the shopping center will also better meet the intent of the MRCSP Design Standards by placing buildings closer to the street, creating multiple store fronts, and increasing the walkability of the site.

ZONING CODE CONFORMANCE

Allowable Uses

The project site has a zoning district designation of CC Community Commercial, as noted in Figure 5 below. This zone is intended to provide for and promote concentrations of community-oriented commercial uses as well as appropriate public, quasi-public, and administrative professional uses within the City. There is no development proposed at this time for the site, but as discussed above, the proposed parcelization of this site will only make it more viable for this type of development.

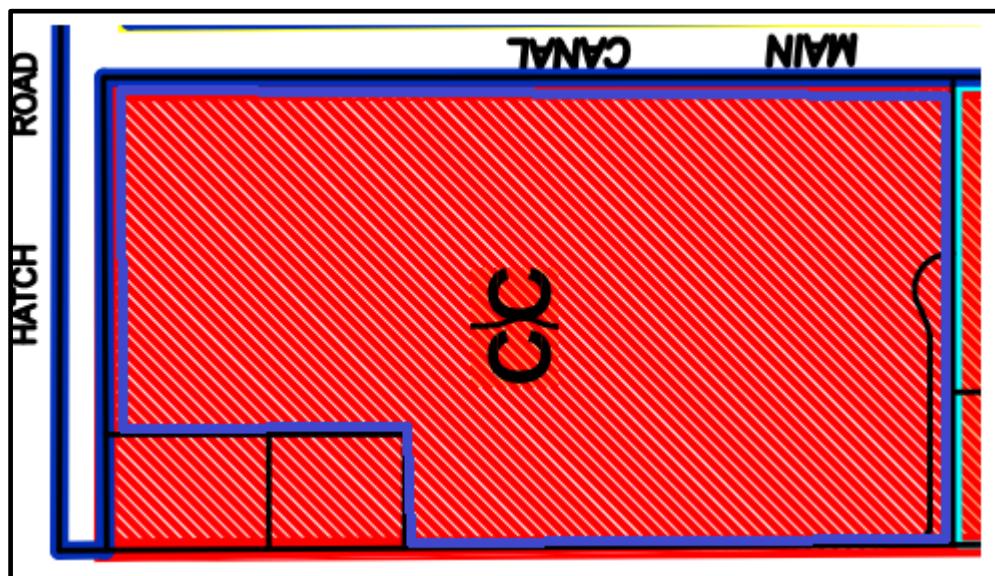


Figure 5: Zoning District Designation

As future development is proposed on this site, the projects will need to conform to the development standards of the Community Commercial zoning district. These standards include conformance with lighting, landscaping, setbacks, height, and design.

As proposed, the Tentative Parcel Map is consistent with the Community Commercial zoning district designation.

Tentative Parcel Map

Since this is a parcel map, per section 17.05.110 of the Ceres Municipal Code, the Planning Commission must make reverse findings for approval of a Tentative Parcel Map. This means that the Commission is looking for a way to disapprove a map, rather than approving the map. In this case, the Commission shall disapprove of a map only if it can make any of the findings below. If none of the findings can be made for disapproval, the Commission may approve the map. The necessary findings are below:

- 1. That the proposed map is inconsistent with the general plan or any applicable specific plan, or other applicable provisions of this Code;**

Analysis: As discussed above, the project is consistent with the General Plan. This finding cannot be made.

- 2. That the site is not physically suitable for the type of development;**

Analysis: As discussed above, the site is physically appropriate for the proposed development.

- 3. That the site is not physically suitable for the proposed density of development;**

Analysis: The proposed project is a non-residential project and there is no density calculation to consider. There is no project proposed at this time.

- 4. That the design of the subdivision or the proposed improvements is likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat;**

Analysis: An Addendum was prepared to contemplate future uses of the parceled areas. No additional mitigation measures are needed and there are no new impacts proposed by the project.

- 5. That the design of the subdivision or the type of improvements is likely to cause serious public health problems;**

Analysis: There is no proposed development at this time that will cause serious health problems as this is an existing site, already connected to City utilities with appropriate trash disposal.

- 6. That the design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of property within the proposed subdivision. In this connection, the Commission may recommend approval of a map if it finds that alternate easements, for access or for use, will be provided, and that these will be substantially equivalent to ones previously acquired by the public. This subsection shall apply only to easements of record or to easements established by judgment of a court of competent jurisdiction, and no authority is hereby granted to the Commission to determine that the public at large has acquired easements for access through or use of property within the proposed subdivision; or**

Analysis: There is no public access easement on this property.

7. Subject to section 66474.4 of the Subdivision Map Act, that the land is subject to a contract entered into pursuant to the California Land Conservation Act of 1965 (commencing with California Government Code section 51200) and that the resulting parcels following a subdivision of the land would be too small to sustain their agricultural use (California Government Code section 66474).

Analysis: There are no California Conservation contracts or easements on this site.

Since none of the findings for the proposed Tentative Map can be made, the Commission has no basis for disapproving the proposed tentative map. As such, staff makes a recommendation of approval for the Tentative Map.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

An Addendum was prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The document was prepared to serve as an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse SCH # 1988053107) for the Mitchell Road Corridor Specific Plan Project (Original Project). This Addendum not only considered the current parcelization of the subject property, but future subdivision of the existing big box building for new uses, and potential additional smaller retailer users of the pad sites.

For the City to consider an amendment to the Original Project, the City must ensure that, if needed, environmental review be consistent with the requirements of the California Environmental Quality Act (CEQA) and the State CEQA Guidelines have been completed. Because the City has previously complied with CEQA for the Original Project and the new discretionary action before the City would be a change in an already-approved project, the City can rely on information in the certified EIR previously prepared for the Original Project, to the extent they remain adequate. Consistent with the requirements of CEQA Guidelines Section 15162, the City must, therefore, determine whether any changed circumstances or “new information of substantial importance” will trigger the need for a subsequent EIR.

As stated, the document takes into account not only the parcelization of the project site, but potential new users that could come to the site for the new pad areas. As demonstrated in the environmental analysis provided in Section 4 (Environmental Analysis) of the Addendum found in Attachment B, the proposed changes do not meet the criteria for preparing a subsequent EIR or negative declaration. An addendum is appropriate here because, as explained in Section 4, none of the conditions calling for preparation of a subsequent EIR or negative declaration have occurred.

Additionally, no further mitigation measures are required for the proposed parcelization and for the additional commercial uses contemplated with this Addendum.

STAFF RECOMMENDATION

Staff recommends that the Planning Commission:

1. Adopt an Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15164; and
2. Adopt a Resolution approving a Tentative Parcel Map for 1670 Mitchell Road (APN: 069-011-09).

Attachments:

1. Attachment A – Draft PC Resolution 2026-XX
2. Attachment B – EIR Addendum – Ceres Mitchell Road Redevelopment Project

Reviewed by:

/s/Lea C. Simvoulakis _____ 2/ 9 /26 _____
Lea C. Simvoulakis, Planning Consultant Date

**DRAFT
RESOLUTION NO. PC 2026-XX**

**RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF CERES
ADOPTING AN ADDENDUM TO THE MITCHELL ROAD CORRIDOR
SPECIFIC PLAN PROJECT ENVIRONMENTAL IMPACT REPORT (SCH
#1988053107) AND APPROVING A TENTATIVE PARCEL MAP FOR 1670
MITCHELL ROAD, CERES CA 95307
(APN: 069-011-009)
FILE NO. 25-46 TPM**

WHEREAS, the Planning Commission held a duly noticed special public hearing on February 17, 2026, at 6:00 p.m. and considered an Addendum and Tentative Parcel Map for 1670 Mitchell Road; and

WHEREAS, the property affected by this resolution is located at 1670 Mitchell Road, Ceres, CA 95307, identified by Assessor's Parcel Number 069-011-009; and

WHEREAS, the Planning Commission based on the evidence within the staff report and project file find that:

1. The proposed Tentative Parcel Map is consistent with the purpose and intent of the CC Community Commercial land use designation of the Ceres General Plan in that the CC Community Commercial designation supports the parcelization of large properties.
2. The proposed Tentative Parcel Map is consistent with the purpose and intent of the CC Community Commercial land use designation as outlined in the Mitchell Road Corridor Specific Plan in the parcelization of the larger commercial site will further the goals and policies of the Specific Plan by making the center more attractive to new retailers, improving walkability, and improving safety in the area.
3. The proposed project is consistent with the CC Community Commercial Zoning District in that this zoning district allows the parcelization of property.
4. The Commission is unable to make the findings necessary to disapprove the tentative parcel map.
5. An Addendum was prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The document was prepared to serve as an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse SCH #

1988053107) for the Mitchell Road Corridor Specific Plan Project (Original Project). An addendum is appropriate here because, as explained in Section 4, none of the conditions call for preparation of a subsequent EIR or negative declaration have occurred.

6. The conditions stated in the decision are deemed necessary to protect the public health, safety, and general welfare of the community.

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Ceres as follows:

Section 1. The Planning Commission hereby confirms that the project requires no further environmental review per Section 15162 Subsequent EIR and Negative Declarations of the California Environmental Quality Act (CEQA) in that no new substantial changes are proposed in the project that require revision of the previous EIR as the proposed project was contemplated in the environmental document; there are mitigation measures previously adopted to mitigate and reduce any impacts; and there have been no other substantial changes that merit a revision of the previously certified document.

Section 2. The operation of the proposed project requires compliance with the conditions of approval contained in this resolution and attached as **Exhibit "A"** on file in the Community Development Department, and the approved Tentative Map attached as **Exhibit "B"** which shall be binding on all heirs, assignees, and successors in interest of said property.

Section 3. The Director of Community Development or designee is hereby directed to record this Resolution at the office of the County Recorder of the County of Stanislaus.

Section 4. The conditions of project approval set forth herein include certain fees, dedication requirements, reservation requirements, and other exactions. Pursuant to Government Code Section 66020 (d) (1), these conditions constitute written notice of a statement of the amount of such fees, and a description of the dedications, reservations, and other exactions. You are hereby further notified that the ninety (90) day appeal period in which you may protest these fees, dedications, and reservations other exactions, pursuant to Government Code Section 66020 (a), has begun. If you fail to file a protest within this ninety (90) day period complying with all of the requirements of Section 66020, you will be legally barred from later challenging such exaction.

I HEREBY CERTIFY that the foregoing Resolution was passed and adopted by the Planning Commission of the City of Ceres at a special public hearing held on the 17th day of February 2026, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

GARY CONDIT, CHAIRPERSON
CITY OF CERES
PLANNING COMMISSION

ATTEST:

STEPHANI DAVIS
ADMINISTRATIVE SECRETARY
CITY OF CERES

ATTACHMENTS:

Exhibit A: Conditions of Approval
Exhibit B: Approved Tentative Map

EXHIBIT A



City of Ceres Community Development Department Conditions Of Approval- Planning Commission

Approved by the Planning Commission on: February 17, 2026

Project File Nos: 25-46 TPM Tentative Parcel Map

Project Name: Tentative Parcel Map for 1670 Mitchell Road

Project Address: 1670 Mitchell Road, Ceres, CA 95307
APN: 069-011-009

Property Owners: Vintage Properties, LP
3501 Coffee Road, Suite #4
Modesto, CA 95355

Project Applicant: Same as Property Owners

This list of conditions is not intended to be all-inclusive or a comprehensive list of City regulations. All conditions are referenced to the Tentative Parcel Map 1670 Mitchell Road project on file with the City of Ceres, Community Development Department, Planning Division.

City of Ceres Community Development Department: Planning Division

- Approval.** This Tentative Parcel Map is approved subject to these specific conditions. Any variation from the approved plans, proposals, supporting documents or presentations is subject to review and approval prior to implementation.
- Conformance to Plans.** This approval is dependent upon and limited to the proposals and plans contained, supporting documents submitted, and presentations made to staff as affirmed to by the applicant and presented on the plans. Any variation from these plans, proposals, supporting documents or presentations is subject to review and approval prior to implementation.
- Expiration.** This approval shall expire two years from this date on **February 17, 2028**, unless altered and/or extended per State law. Prior to expiration, the owner/developer may apply for an extension, to be approved by the Community Development Director, or their Designee, not to exceed a total of two (2) years from the original date of expiration. **It shall be the**

EXHIBIT A

responsibility of the applicant/property owner to be aware of the expiration of this entitlement and take the necessary steps to ensure the approval of this entitlement remains valid. It shall not be the City of Ceres' responsibility to notify the applicant/property owner of the project's expiration date. If an approved entitlement expires, the applicant/property owner shall be required to submit a new application request for this project.

4. **Owner Obligation.** All conditions of approval shall be satisfied by the owner/developer. If at any time, any of the Conditions of Approval are found to be in non-compliance, City enforcement action may be implemented. Costs associated with compliance with the conditions shall be at the owner/developer's expense.
5. **Fees.** The developer shall pay all applicable processing fees, permit fees, City development fees, fire fees, school fees, drainage fees, habitat conservation fees and other public entity fees in effect at the time of the issuance of a building permit.
6. **Utility Companies.** The applicant is responsible for contacting all appropriate utility companies to obtain agreements for extension and/or relocation of services necessary for the proposed development.
7. **Signs.** No business identification signs or any advertising signs shall be installed or displayed without the review and approval of the Community Development Department, and all proposed advertising signs shall comply with the Sign Standards found in Chapter 26 of Title 18 of the Zoning Ordinance.
8. **Vested Rights.** This approval does not vest applicant's rights regarding future development. All ordinances, resolutions, rules, regulations and official policies governing design, improvement and construction standards and specifications applicable to the project and public improvements to be constructed by the Developer shall be those in force and effect at the time the applicable plan or permit approval is granted.
9. **Responsible Agencies.** The Developer is responsible for contacting all responsible and commenting agencies and ensuring compliance with any applicable fees and/or rules.
10. **Subsequent Development.** All activities undertaken in accordance with this approval shall comply with the City's General Plan and Municipal Code. In cases of conflict between the City's Municipal Code or map-specific conditions of approval, the governing priority shall be, to the extent legally permitted, as follows: 1) Municipal Code regulations, 2) project-specific conditions, and 3) standard conditions. The applicant shall comply with all regulations and code requirements of the Community Development Director, City Engineer, and Building Official, the Fire Chief, the Police Chief, and any other agencies requiring review of the project. If required, these agencies shall be supplied copies of the final maps, site plans, public improvement plans, grading plans, and building plans.
11. **Amendments.** The Planning Division may approve minor amendments to the project, provided that the amendment is still in substantial conformance with

EXHIBIT A

the original approval of this entitlement. A request for a minor amendment shall be submitted in writing to the Planning Division. Any request that is not deemed minor in nature must be presented to the Planning Commission through the amendment process and the applicant shall pay the necessary processing fees.

12. **Building Plans.** The project developer shall write all conditions of approval for this project on all building permit plan check sets submitted for review and approval. These conditions of approval shall be on, at all times, all grading and construction plans kept on the project site. It is the responsibility of the building developer to ensure that the project contractor is aware of, and abides by, all conditions of approval. Prior approval from the Director, or their Designee, must be received before any changes are constituted in site design, grading, building design, building colors or materials, etc.
13. **Additional Information.** Upon Building Permit submittal, the applicant is required to show grading, access, paving, striping, storm drain system, frontage landscaping, fencing, sewer and water connections, and backflow devices to the satisfaction of the City Engineer. Civil drawings illustrating the required improvements for the project are required to be submitted with the building plan submittal. Any changes resulting from the review of these plans that cause substantial change to the site plan will require the project return to Planning Commission for a Site Plan Amendment.
14. **Easements.** As determined by the City, the applicant shall secure all necessary easements for reciprocal access, parking, landscaping and/or public utilities over the proposed property. Where required, location of all easements shall be agreed to with the City prior to the issuance of building permits. These easements shall be recorded prior to issuance of a Certificate of Occupancy.
15. **Failure to Comply.** Should the project be found, at any time, not to comply with any of the Conditions of Approval, or should the applicant construct or operate this development in any way other than as specified in the Application or Supporting documents or presentations to staff, the Planning Commission, or City Council, as modified by the Conditions of this Approval, then the terms of this Approval shall be considered to be violated.
16. **Indemnification.** The applicant shall indemnify and hold harmless the City, its council members and commissioners, officers, agents, employees, and representatives from liability for any award, damages, costs and fees, including without limitation attorneys' fees, incurred by the City and/or awarded to any plaintiff in any action related to or arising out of the City's approval of this project or subdivision Map or any environmental or other documentation related to this project. The applicant further agrees to provide a defense for the City in any such action.
17. **Limits of Approval.** Approval of this application does not constitute approval of any other entitlement or any other necessary permit, license, or approval.
18. **Compliance with Local and State Laws.** The subject use shall be conducted in full compliance with all local and state laws. No part of this approval shall be construed to permit a violation of any part of the Ceres Municipal Code.

EXHIBIT A

19. **Changes in Law.** This approval shall not preclude the application to development of the property of changes in City laws, regulations, plans or policies, the term of which are specifically mandated and required by changes in State or Federal laws or regulations.
20. **Other entitlements.** Approval of this application does not constitute approval of any other entitlement or any other necessary permit, license, or approval.
21. **Paving.** The City will require the applicant to construct the paving improvements (for parking area/access driveway) for the project as proposed on the Site Plan, which shall be completed to the satisfaction of the City Engineer and Building Official.
22. **SJVAPCD.** The applicant shall comply with the requirements of the San Joaquin Valley Air Pollution Control District.

City of Ceres Community Development Department: Building Division

23. **Building Area:** The floor area of a building shall be determined based on the type of construction, occupancy classification, whether there is an automatic sprinkler system installed throughout the building and the amount of building frontage on public way or open space. With the addition of Parcel 6 creating a "Zero lot line" on the west side of the existing building and Parcel 5 decreasing the area along the northwest corner of the existing building the maximum allowable area of the existing building may be compromised due to the modification of the open space. Any proposed tenant(s), single or mixed use, for the existing space, shall require an area analysis of the existing building to be completed to verify compliance with Chapter 5 of the California Building Code
24. **Building Requirement.** Any construction shall be in compliance with the most current adopted California Code of Regulations Title 24 at the time of the building permit application submittal date.
25. **Demolition Permits.** The applicant shall secure the necessary demolition permit(s) from the City of Ceres to demolish any existing structures on site.
26. **Fire Code.** All construction and design shall meet or exceed the California Fire Code for fire and life safety requirements, including exiting and other life safety concerns.
27. **Addresses.** Building addresses for all buildings shall be posted and visible from the roadway in accordance with the California Fire Code and Ceres Municipal Code.

City of Ceres Engineering Department

28. **General Compliance.** The applicant shall comply with all applicable requirements of the City of Ceres, including the Municipal Code, City Engineering Design Standards, the Subdivision Map Act, and all other applicable federal, state, and local regulations pertaining to parcel division and industrial development.

EXHIBIT A

29. **Parcel Map Preparation and Recordation**

- a. The Parcel Map shall be prepared by a licensed land surveyor or civil engineer authorized to practice land surveying in the State of California.
- b. The final Parcel Map shall be submitted to and approved by the City Engineer prior to recordation.
- c. All existing and proposed easements, including public utility easements (PUE), drainage easements, and access easements, shall be shown on the Parcel Map and dedicated as necessary.

30. **Public Improvements**

- a. No public frontage improvements (e.g., curb, gutter, sidewalk, pavement widening, utilities, or street lights) are required at the time of Parcel Map recordation. All required public improvements shall be constructed at the time of site development or building permit issuance.
- b. A note shall be added to the Parcel Map stating: "All required public improvements along project frontages shall be completed in conjunction with future site development. No public improvements are required for Parcel Map approval."

31. **Access and Circulation**

- a. Legal and physical access shall be provided to each parcel. All access points will be reviewed and conditioned as part of future site development review.
- b. Cross-access easements and internal circulation routes shall be established as needed to support multi-tenant or phased industrial development and shall be recorded prior to issuance of building permits.
- c. Emergency vehicle access shall be reviewed and approved by the City Engineer and Fire Department during site plan review.

32. **Utility Services**

- a. All water, sewer, storm drainage, and other utility connections shall be evaluated and installed at the time of site development. The Parcel Map shall reflect utility easements as needed to accommodate future service lines.
- b. Shared utility easements may be required if a single service laterals or joint utility corridors serve multiple parcels.

33. **Drainage and Stormwater Compliance**

- a. No on-site grading or drainage improvements are required at the Parcel Map stage. All stormwater management and low-impact development (LID) features shall be incorporated during the site development process, in compliance with the City's stormwater regulations and State Water Resources Control Board requirements.
- b. Drainage easements shall be recorded if off-site or shared drainage paths are proposed in the future development.

34. **Right-of-Way and Dedications**

- a. Any right-of-way dedications consistent with the City's General Plan, Circulation Element, or Zoning requirements shall be shown on the Parcel Map.

EXHIBIT A

- b. Physical frontage improvements, including pavement widening, curb and gutter, sidewalk, and landscaping, will be required at the time of development and shall not be deferred beyond the project-level entitlement.

35. Fees and Improvement Securities

- a. No improvement bonds or construction securities are required at the Parcel Map stage.
- b. All applicable development impact fees, utility connection fees, and securities for public improvements shall be collected and processed during the site development phase or at building permit issuance.

36. Phasing and Project Coordination

- a. If development is phased, all conditions applicable to each phase, including frontage and utility improvements, shall be clearly addressed through site plan review and associated improvement plans.
- b. Parcel configuration shall not preclude orderly development of the site or adjacent parcels.

37. Civil Drawings. Civil drawings illustrating required improvements (i.e. water and sewer services, a storm drain system, paved parking area, and commercial curb cuts) are required to be submitted with the building plan submittal for each building.

38. City Standards. The applicant shall install all required improvements (including parking area, driveway approaches, and curb, gutter, sidewalk) to the Standards of the Engineering Services Department. The applicant shall obtain approval of an Encroachment Permit through the Engineering Services Department prior to constructing these improvements.

- a. The applicant shall install all required (including curb, gutter, and sidewalk, as well as meter and backflow devices) improvements to the Standards of the Engineering Services Department.

39. Utility Easements. Additional utility easements might be needed for sewer, water, and storm drainage.

40. Encroachment Permit. Prior to any work being conducted within the City right-of-way and prior to issuance of building permits, the applicant shall obtain an Encroachment Permit from the City. Prior to issuance of the Encroachment Permit, the applicant shall submit to the City of Ceres for review and approval, improvement plans prepared by a registered Civil Engineer; shall pay plan check and inspection fees; shall provide a cost estimate of the improvements; and shall post sufficient surety guaranteeing the construction of the improvements.

41. Traffic Control Plans. The applicant shall provide traffic control plans, prepared by a registered civil engineer, for all proposed work within the City right-of-way.

42. Water Wells. The applicant shall indicate, in writing to the City, the disposition of any water well(s) and any other water that may exist within the site. If any wells are proposed to be abandoned, or if they are abandoned and have not been properly sealed, they must be destroyed per County of Stanislaus requirements.

EXHIBIT A

City of Ceres Fire Department

43. **Uniform Fire Code.** All construction shall conform to the requirements of the City of Ceres Building Division and City of Ceres Ordinance 84-625, as amended pertaining to the Uniform Fire Code and Ordinance 80-546, as amended, pertaining to Automatic sprinklers.
44. **Fire Hydrants.** Fire hydrant(s) are required. All fire hydrant(s) shall be spaced 300 feet apart in which both on and off site location to be approved by Ceres Fire Department. Fire hydrants shall also be within 50 feet of the Fire Department Connection on the same side roadway for the Automatic Fire Sprinkler System. They shall be connected to a minimum main size of 8 inches for a dead-end hydrant and 6 inches for a looped system. Each hydrant shall be Rich 960 or equal and be within 50 feet of each Fire Department Connection (FDC). Fire Department Connections (FDC's) shall be located per Ceres Fire Department specifications.
45. **EVA.** All Emergency Vehicle Access points and proposed utilities shall ensure cross-easements and shall be located per the proposed development plans unless modified by these conditions. All Emergency Vehicle Access shall be constructed of an all-weather material approved by the Ceres Fire Department.
46. **Security Lighting.** The applicant shall provide on-site security lighting to the satisfaction of the Ceres Fire Department.
47. **Fire Inspections.** A minimum of 48-hour notice is required for inspections.
48. **Vertical Obstructions.** All vertical obstructions shall be higher than 13 feet 6 inches.
49. **Address Numbers.** Address numbers on the building shall clearly be visible from the street in contrasting color. Front and rear doors to have address numbers (or Suite numbers) 6" in size.
50. **Fire Lanes.** The applicant shall mark fire lanes within the project in the locations as approved by the Fire Marshall
51. **All Weather Signage.** Permanent all weather signage (minimum 3 inch letters/numbers – red background with white letters – shall be installed at:
 - a. Main Electrical Disconnect
 - b. Main Gas Shutoff
 - c. Electrical Room

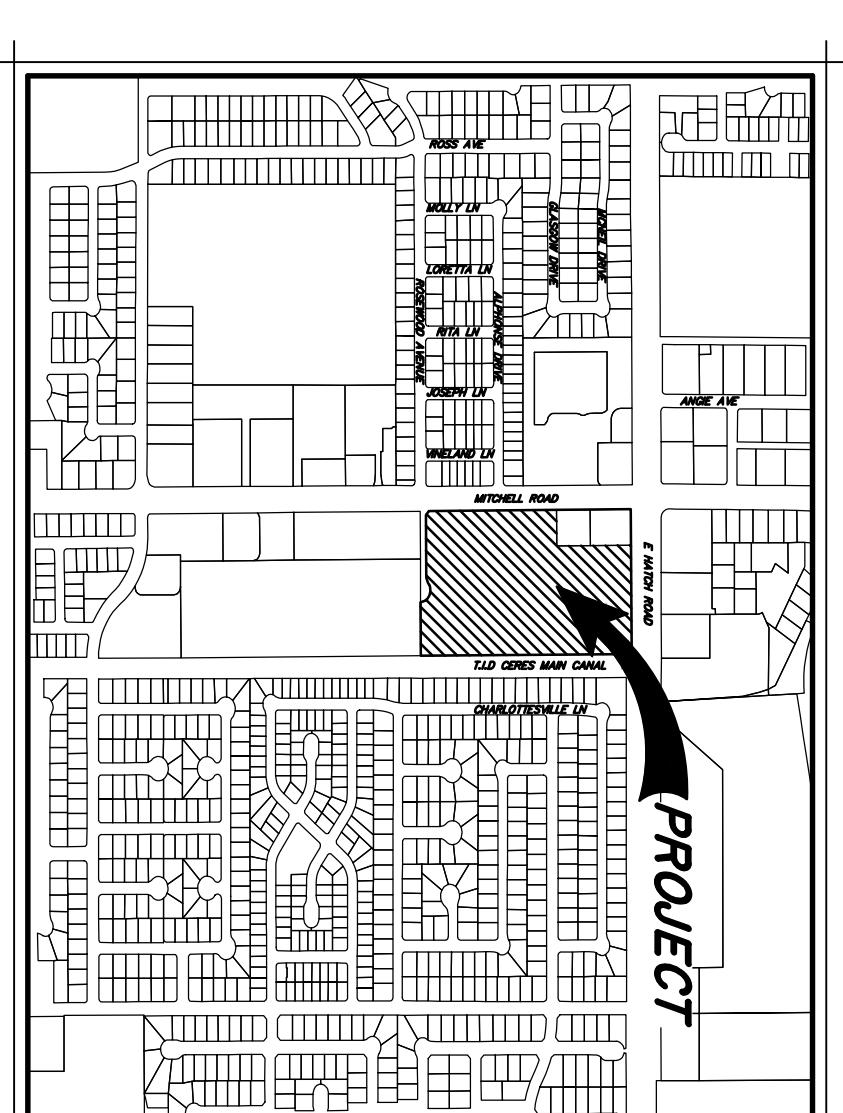
City of Ceres Police Department

52. **Public Peace, Safety, Morals.** The project will be required to conform to the provisions of the Public Peace, Safety, and Morals section of the Ceres Municipal Code.

1670 MITCHELL ROAD

VESTING TENTATIVE PARCEL MAP

CERES, CALIFORNIA



TURLOCK IRRIGATION DISTRICT (T.I.D.) CERES MAIN CANAL

20.00' T.I.D. EASEMENT
GRANTED
FOR POLES AND WIRES
PER 62460, BOOK 3773,
PAGE 413
OF OFFICIAL RECORDS

90.00'

20.732'

22.00' T.I.D. EASEMENT
GRANTED
FOR POLES AND WIRES
PER 62460, BOOK 3773,
PAGE 413
OF OFFICIAL RECORDS

65.466'

33.00' T.I.D. EASEMENT
GRANTED
FOR ELECTRICAL LINES
PER 93-0044781
OF OFFICIAL RECORDS

10.00' P.U.E.

PER 44-PM-76

27.52'

40.00' IRRIGATION
EASEMENT
PER 27-PM-77

27.52'

GRANTED
FOR ELECTRICAL LINES
PER 93-0044782
OF OFFICIAL RECORDS

0

50

100

150

SCALE: 1" = 50'

VICINITY MAP

N.T.S.

PROJECT INFORMATION

PROPERTY OWNER: VINTAGE PROPERTIES, LP
3501 COFFEE ROAD, SUITE #4
MODESTO, CA 95355
I: 209-330-3222

APPLICANT: MVE, INC.
117 L STREET
MODESTO, CA 95354
I: 866-526-4214

CURRENT ZONING: COMMUNITY COMMERCIAL (CC)

TOTAL SITE AREA: 605,456 SF

COMMERCIAL LOTS: 7 PARCELS

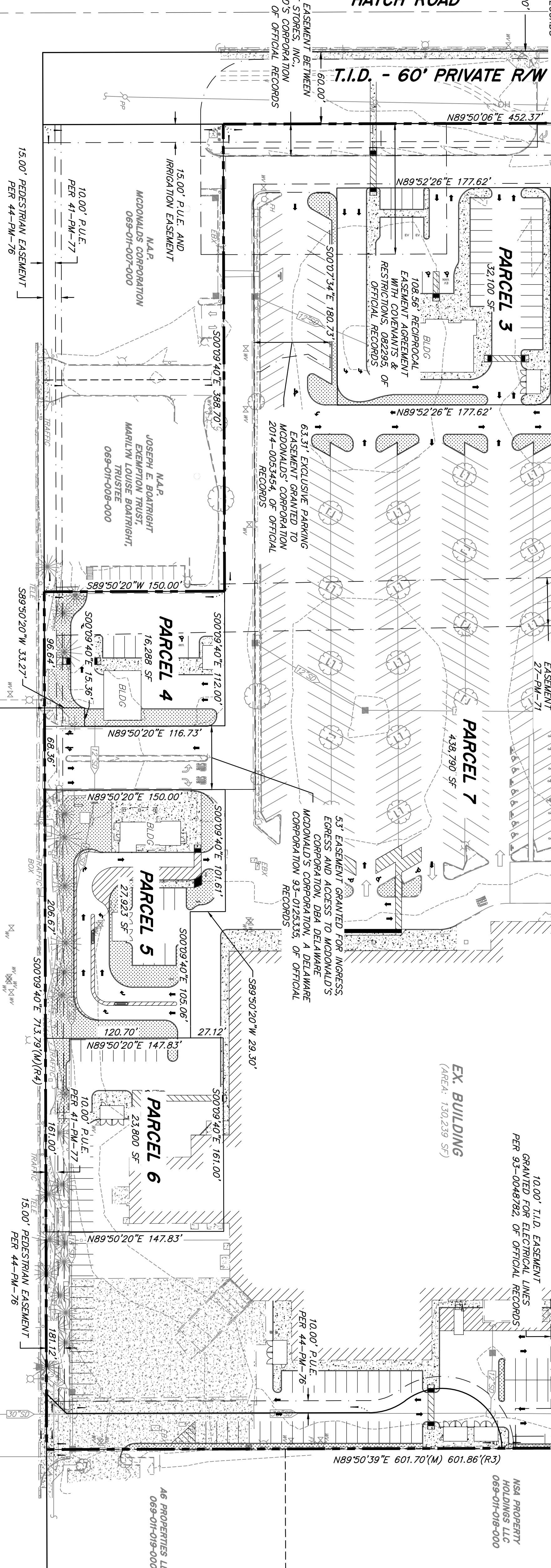
WRITTEN STATEMENTS

1. UTILITIES:
 - DOMESTIC WATER SYSTEM - CITY OF CERES
 - DOMESTIC SEWER SYSTEM - CITY OF CERES
 - STORM DRAINAGE SYSTEM - CITY OF CERES
 - GAS - PIPE
 - ELECTRIC - TURLOCK IRRIGATION DISTRICT (TID)
 - TELEPHONE - SBC
 - HIGH SCHOOL DISTRICT - CERES HIGH
 - ELEMENTARY SCHOOL DISTRICT - SAMUEL VAUGHN ELEMENTARY
2. THE SUBDIVIDER RESERVES THE RIGHT TO FILE "MULTIPLE SUBDIVISION MAPS" AS SET FORTH BY THE SUBDIVISION MAP ACT, ARTICLE 4, SECTION 65446.1.
3. EXISTING DITCHES, IRRIGATION LINES, AND POWER POLES TO BE RELOCATED PER TID REQUIREMENTS, IF POWER POLES TO BE
4. PARCEL 7 TO BE UTILIZED AS SHARED PARKING, INGRESS/EGRESS, UTILITIES, AND LANDSCAPING.
5. SUBJECT PARCEL WILL COUPLE WITH CITY'S SIGNAGE PROGRAM, SITE PLAN AND APPLICATION TO BE SUBMITTED SEPARATELY.

LEGEND

- Existing Easements
- Existing Parcel Lines
- Existing Edge of Pavement
- Proposed Lot Lines
- Right of Way
- Property Line

CONCRETE



PRIOR TO CONSTRUCTION THE CONTRACTOR
SHALL CALL UNDERGROUND SERVICE ALERT
FOR UNDERGROUND CLEARANCE USA WILL
NOT BE RESPONSIBLE FOR OR LIABLE FOR
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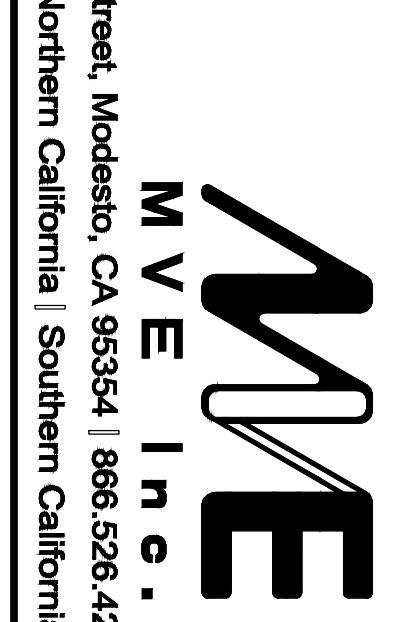
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EIR ADDENDUM

FOR THE

CERES MITCHELL ROAD REDEVELOPMENT PROJECT

NOVEMBER 11, 2025

Prepared for:

City of Ceres
Planning Division
2220 Magnolia Street
Ceres, CA 95307
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Prepared by:

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D e N o v o P l a n n i n g G r o u p

A Land Use Planning, Design, and Environmental Firm

ATTACHMENT B

EIR ADDENDUM

FOR THE

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EIR ADDENDUM

1 INTRODUCTION

This Addendum was prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. This document has been prepared to serve as an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse [SCH] # 1988053107) for the Mitchel Road Corridor Specific Plan Project (Original Project). The City of Ceres is the lead agency for the proposed amendments to the Original Project, which consists of repurposing an existing 130,373 sf building that was previously operated as a Walmart with new retail uses, and building 17,396 square feet of new retail space for a general retail store, two quick serve restaurants, and a Dutch Bros beverage shop on a 13.4 acre site within the Specific Plan area. The new buildings are intended to maximize the utilization of the site.

For the City to consider an amendment to the Original Project, the City must ensure that, if needed, environmental review consistent with the requirements of the California Environmental Quality Act (CEQA) and the State CEQA Guidelines has been completed. Because the City has previously complied with CEQA for the Original Project and the new discretionary action before the City would be a change in an already-approved project, the City can rely on information in the certified EIR previously prepared for the Original Project, to the extent they remain adequate. Consistent with the requirements of CEQA Guidelines Section 15162, the City must, therefore, determine whether any changed circumstances or “new information of substantial importance” will trigger the need for a subsequent EIR.

As described in CEQA Guidelines Section 15164, a lead agency shall prepare an addendum to a previously adopted EIR if some changes or additions to an EIR certified for a project are necessary, but none of the of conditions identified in CEQA Guidelines Section 15162 have occurred. No subsequent EIR shall be prepared for that project unless the lead agency determines, based on substantial evidence in the light of the whole record, one or more of the following conditions to be applicable:

- (1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- (2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
 - (A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;

EIR ADDENDUM

- (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
- (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
- (D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

If any of the triggers set forth above occurs, the City would be required to prepare a subsequent EIR, unless “only minor additions or changes would be necessary to make the previous EIR adequately apply to the project in the changed situation,” in which case a “supplement to an EIR” would suffice (see CEQA Guidelines, Section 15163). If there are no grounds for either a subsequent EIR or a supplement to an EIR, then the City must prepare an addendum pursuant to CEQA Guidelines Section 15164, explaining why “some changes or additions” to the EIR “are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.”

As demonstrated in the environmental analysis provided in Section 4 (Environmental Analysis), the proposed changes do not meet the criteria for preparing a subsequent EIR or negative declaration. An addendum is appropriate here because, as explained in Section 4, none of the conditions calling for preparation of a subsequent EIR or negative declaration have occurred.

Information and technical analyses from the EIR are utilized throughout this Addendum. Relevant passages from this document (consisting of the EIR) are cited and available for review at:

City of Ceres
2220 Magnolia Street
Ceres, CA 95307
Phone: 209-538-5700

2 PROJECT BACKGROUND AND PURPOSE OF THE EIR ADDENDUM

The Mitchel Road Corridor Specific Plan Project was finalized and approved by the City Council in 1989. Since certification of the EIR, much of the 436.7-acre area has developed with a mix of uses. This includes a 13.4-acre parcel located at the southeast corner of the intersection of Mitchell Road and Hatch Road. The parcel is APN 039-068-009 and is within Planning Area 2 as identified in the Mitchell Road Corridor Specific Plan. The 13.4-acre parcel was built out with a Walmart and other

retail operations; however, Walmart has since closed and new retail operations, including new buildings, are proposed. The following describes the original project.

ORIGINAL PROJECT

The Original Project consisted of adoption and implementation of the Mitchell Road Corridor Specific Plan, which included the development of 436.70 acres as described below.

LOCATION AND BOUNDARIES

The Mitchell Road Corridor study area was located on the eastern edge of the City of Ceres. Ceres is located adjacent to and south of the City of Modesto in Stanislaus County. The Mitchell Road Corridor includes a 2 1/2-mile stretch of Mitchell Road extending between Highway 99 on the south and the Tuolumne River on the north. The corridor is approximately 1/4 mile wide for most of its length but broadens to approximately half mile wide at the northern and southern ends to include large parcels in those areas.

The corridor was within the City jurisdiction except for several parcels in the north and south end which were within the unincorporated area of the County of Stanislaus. Only a small percentage of the corridor was currently developed at the time of approval, with existing commercial uses concentrated predominantly between Fowler and Roeding and at the Hatch/Mitchell and Don Pedro/Mitchell intersections.

STATEMENT OF OBJECTIVES

The Mitchell Road Corridor Specific Plan was intended to achieve the goals and objectives of the City of Ceres through the cost-effective physical and economic development of the corridor. The City intended for the Mitchell Road Corridor Specific Plan to:

1. Provide a means of controlling the direction of growth within the Mitchell Road Corridor.
2. Capitalize on and maximize the economic potential of the Mitchell Road Corridor so that the area can be viewed as an economic asset to the City.
3. Provide a land use scenario that is sensitive to existing and future residential uses in the corridor vicinity.
4. Provide guidelines which will suggest a desired aesthetic quality for the corridor area and incorporate existing and new urban design concepts.
5. Provide a circulation concept that utilizes the road-widening program and maximizes land use potential while minimizing traffic conflicts.
6. Create an image for the City that is distinctive and inviting.
7. Encourage public transit and pedestrian circulation.
8. Provide a varied and interesting building height profile as is suitable.
9. Provide for the short-term, as well as the long-term, needs of the corridor in a comprehensive planning approach.
10. Provide a plan that is sensitive to the noise attenuation needs of the corridor.
11. Provide an environment that is safe for the public at large.

EIR ADDENDUM

PROJECT CHARACTERISTICS

The Mitchell Road Specific Plan involved the intensification of the Mitchell Road Corridor from predominantly vacant and underutilized uses to a planned commercial strip. The table below details the types and quantity of the proposed land uses for the corridor based on maximum buildout of the Specific Plan.

Land Use	Acreage	Square footage
Neighborhood Commercial	52.89	691,200
Planned Commercial	138.45	1,809,300
Highway Commercial	28.86	377,100
Retail Commercial	61.62	805,300
Business Park	105.40	1,377,400
Residential	49.48	282 units
Church	4.44	1
	436.70	

The approach to the land use concept included:

- protecting existing viable residential neighborhoods;
- providing for a variety of commercial opportunities;
- enhancing and capitalizing on the key visual features within the corridor and surrounding area;
- preserving existing significant trees; and
- minimizing traffic conflicts along Mitchell Road.

The Corridor had been designated with a range of commercial land uses. In general terms, the Corridor can be divided into three distinct Planning Areas.

- Planning Area 1 is located between Roeding Road and Highway 99, is visualized as an intense commercial center to serve the City and regional needs.
- Planning Area 2, located between Hatch and Roeding Roads, is intended to serve as a local and regional target for residents to the east and west.
- Planning Area 3, between the Tuolumne River and Hatch Road, is intended to draw users and occupants from the airport and Modesto area.

APPROVALS

As part of the Original Project, the City undertook the following actions:

- Certified the Mitchell Road Corridor Specific Plan EIR;
- Adopted the Mitigation Monitoring and Reporting Program for the project; and

- Approved the Mitchell Road Corridor Specific Plan.

3. PROPOSED PROJECT

Project Site

Modifications to the Original Project are limited to an approximately 13.9-acre site located in the approved 436.7-acre Mitchell Road Corridor Specific Plan area (Modified Project site). The Modified Project site is located at the southeast corner of the intersection of Mitchell Road and Hatch Road and is APN 039-068-009. The Modified Project site is in Planning Area 2 as identified in the Mitchell Road Corridor Specific Plan.

Surrounding Uses

The surrounding area is characterized by a mix of commercial, retail, and service-oriented uses along the Mitchell Road and Hatch Road corridors. At the corner of Mitchell Road and Hatch Road, is APN 039-068-007 which contains a 3,524-square-foot (sf) McDonald's restaurant. Additionally, on APN 039-068-008 is a 3,763-sf car stereo store. Neither of these parcels are part of the Modified Project site, but are immediately adjacent and connected to the Modified Project site.

There are residential neighborhoods located to the east and west.

Existing Uses

Walmart previously occupied the existing 130,373 sf building and associated surface parking areas on-site; however, Walmart has since relocated, leaving the existing building vacant. The building has remained unoccupied since 2021.

Characteristics of the Modified Project

The Modified Project consists of repurposing the existing 130,373 sf building that was previously operated as a Walmart with the following new retail uses:

- 60,478 sf supermarket, likely to be occupied by Vallarta Supermarkets;
- 27,094 sf general retail store, likely to be occupied by Ross Dress for Less;
- 23,258 sf general retail store (tenant currently unknown);
- 9,147 sf general retail store, likely to be occupied by Five Below;
- 5,129 sf auto parts store (tenant currently unknown).

In addition, the Modified Project proposes the construction of the following new buildings on the site:

- 7,470 sf general retail store (tenant currently unknown);
- 4,137 sf (50-seat) fast-food restaurant with drive-through (tenant currently unknown);
- 3,515 sf (50-seat) fast-food restaurant with drive-through (tenant currently unknown);
- 1,288 sf quick oil change center, likely to be occupied by Take 5 Oil Change;

EIR ADDENDUM

- 986 sf (outdoor 12-seat) Dutch Bros specialty beverage shop with drive-through.

The project also proposes expanding the parking capacity to 684 stalls across the entire site. Site access would continue to be provided via the following four driveways:

- East Hatch Road driveway (full access);
- Mitchell Road northern driveway (right-turn in, right-turn out only access);
- Mitchell Road central driveway (full access);
- Mitchell Road southern driveway (full access).

No improvements to the existing driveways or street frontages along Mitchell Road and E. Hatch Road are proposed. The project site vicinity map is shown on Figure 1; the project site plan is provided in Figure 2.

4 ENVIRONMENTAL ANALYSIS

Analysis of Impacts Found to be Less Than Significant

Specific impacts within the following categories of environmental effects were found to be less than significant as set forth in more detail in the certified EIR. Following each bullet is a conclusion on whether the proposed revision changes the impact conclusion found in the original EIR.

Section 3.1: Soils and Geology

The modified project, like the original project, has the potential to directly or indirectly expose residents and property structures to mild ground shaking during seismic occurrences. The potential for the soil and geology impacts associated with the original project are discussed in section 3.1.1 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.1.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require all new structure associated with the modified project to be built in accordance with City Code, Older, existing structures which will remain should be modified to incorporate the latest building and safety standards. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.2: Air Quality

The modified project, like the original project, has the potential to directly or indirectly create air quality impacts. The potential for the impacts associated with the original project are discussed in section 3.2.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.2.3 was

INTRODUCTION

adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require air quality reduction measures for the modified project. This includes compliance with all rules and regulations by the San Joaquin Valley Air Pollution Control District. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.3: Hydrology

The modified project, like the original project, has the potential to directly or indirectly create drainage/flooding impacts, and/or water quality impacts. The potential for the impacts associated with the original project are discussed in section 3.3.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.3.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require a storm drainage plan that handles drainage/flooding impacts as well as water quality impacts associated with the modified project. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.4: Noise

The modified project, like the original project, has the potential to directly or indirectly create noise impacts from traffic, airports, and stationary sources. The potential for the impacts associated with the original project are discussed in section 3.4.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.4.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require stationary sources of noise to be reduced from compliance with City standards and best practices designed into the buildings of the modified project. The project modifications do not create significant changes to the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.5: Aesthetics & Light and Glare

The modified project, like the original project, has the potential to affect visual character, light, and glare. The potential for the impacts associated with the original project are discussed in section 3.5.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.5.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program

EIR ADDENDUM

to require design considerations, including lighting and setbacks in the commercial development of the modified project. The project modifications do not cause significant changes to the visual appearance of the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.6: Land Use

The modified project, like the original project, has the potential to affect land use. The potential for the impacts associated with the original project are discussed in section 3.6.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.6.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require setback requirements, consistency with the Airport Land Use Plan, and review for incompatible uses related to the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.7: Population & Housing

The modified project, like the original project, has the potential to affect population and housing. The potential for the impacts associated with the original project are discussed in section 3.7.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Mitigation Measure 3.7.3 was adopted, but is not relevant to the modified project. The project modifications do not cause significant changes to the land uses that would affect population or housing on the developed site or surrounding, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.8: Transportation and Circulation

The modified project, like the original project, has the potential to affect transportation and circulation. The potential for the impacts associated with the original project are discussed in section 3.8.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.8.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require traffic analysis related to the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR. However, additional VMT analysis is provided below given that such

INTRODUCTION

analysis was not a requirement at the time of the original EIR. The VMT analysis is included as an Appendix.

Vehicle Miles Traveled (VMT) Impacts caused by retail land uses discussed in TR-1 were found to be less than significant.

- The existing 130,373-square-foot building, previously occupied by a Walmart Superstore, remains entitled for similar commercial uses under current zoning. The proposed project would redevelop this building and construct additional retail space on-site, resulting in a net increase of approximately 17,396 sf of gross leasable floor area. Since the net increase is less than 50,000 sf, no further VMT analysis is required for the retail land uses, as their VMT impacts are considered less than significant. Therefore, the revisions do not increase the severity of any impact. However, the revisions do not change the environmental conclusion of less than significant that was provided in the certified EIR.

Conflicts with adopted policies, plans or programs regarding auto network improvements discussed in TR-2 were found to be less than significant.

- The revision does not propose frontage improvements along Mitchell Road or E. Hatch Road; instead, it will maintain the existing driveways as-is. The 2022 RTP/SCS identifies a planned improvement to enhance Mitchell Road to six lanes between SR 99 and the Tuolumne River. The minimum curb-to-curb width along the project site Mitchell Road frontage is 90 feet, which can accommodate a six-lane arterial without on-street parking. Similarly, E. Hatch Road is planned to be widened to a four-lane expressway between Mitchell Road and Faith Home Road, as outlined in the City of Ceres General Plan 2035 (2018). The existing E. Hatch Road frontage provides a minimum 80-foot curb-to-curb width, sufficient to accommodate a four-lane expressway without on-street parking. The revision would not conflict with these future planned auto network improvements along Mitchell Road or E. Hatch Road; therefore, the impact is considered less than significant.

The revision is expected to increase the number of trips using site driveways by 11,180 daily, including 602 during the AM peak hour and 941 during the PM peak hour, compared to prior operations when the Walmart Superstore was active on-site. This increase in peak hour trips at project driveways warrants optimizing traffic signal timings at the adjacent signalized intersections on Mitchell Road (at the Central Driveway and at Hatch Road) to reduce average vehicle delay at both intersections. With the optimization, the revisions do not change the environmental conclusion of less than significant that was provided in the certified EIR.

RECOMMENDATION TR-2: Coordinate with the City of Ceres Engineer to optimize traffic signal timings at the signalized intersections of Mitchell Road/Central Driveway and Mitchell Road/E. Hatch Road.

EIR ADDENDUM

Conflicts with adopted policies, plans or programs regarding bicycle or pedestrian facilities discussed in TR-3 were found to be less than significant.

- The Ceres Citywide Active Transportation Plan (2021) identifies planned Class 4 protected bike lane improvements on Mitchell Road between SR 99 and the Tuolumne River, as well as the installation of high-visibility crosswalks on all legs of the Mitchell Road/E. Hatch Road intersection. The existing 90-foot curb-to-curb width along the Mitchell Road frontage is sufficient to accommodate a six-lane arterial and Class IV protected bike lanes without on-street parking. The project also does not preclude the installation of high-visibility crosswalks at the adjacent Mitchell Road/E. Hatch Road intersection.

The revisions would not conflict with future bicycle or pedestrian facility planned improvements along Mitchell Road or E. Hatch Road; therefore, the impact is considered less than significant.

Inadequate transit service to meet demand discussed in TR-4 were found to be less than significant.

- The revision would not eliminate or adversely affect existing transit access. StanRTA currently operates Route 47, with bus stops located on Mitchell Road, adjacent to the project site. These existing bus stops are within a five-minute walking distance of the project site. Therefore, this impact is considered less than significant, and no mitigation is required

However, the existing StanRTA Route 47 serves a bus stop on northbound Mitchell Road, just north of the project's southern driveway. The bus stop includes signage, a bench, and a trash bin, but lacks a shelter

While not required under CEQA, installing a bus stop shelter could enhance convenience for transit users, particularly during extreme weather conditions, and may encourage greater transit use by project site workers and visitors.

RECOMMENDATION TR-4: Install bus stop shelter at existing StanRTA bus stop on northbound Mitchell Road on project site.

Increased hazards due to geometric design features discussed in TR-5 were found to be less than significant.

- The revision does not propose frontage improvements along Mitchell Road or E. Hatch Road; instead, it will maintain the existing driveways as-is. The revision is expected to increase the number of trips using site driveways by 602 AM and 941 PM peak hour trips compared to prior operations when the Walmart Superstore was active on-site. As shown in Table 2, approximately 2,070 vehicles were counted during the AM peak hour and 2,180 vehicles during the PM peak hour along the Mitchell Road frontage. Along the E. Hatch Road frontage, approximately 1,550

INTRODUCTION

vehicles were counted during the AM peak hour and 1,500 vehicles during the PM peak hour.

These high existing volumes may make it difficult for vehicles to exit the site and turn left at the unsignalized E. Hatch Road driveway and southern Mitchell Road driveway. However, if left-turn delays are significant at these unsignalized driveways, drivers have the option to exit via the signalized central Mitchell Road driveway.

General Plan Policy 3.A.7 supports the development of an expressway system, with E. Hatch Road classified as a Class C expressway. Additionally, Policy 3.B.1 requires that development and redevelopment projects consider the inclusion of medians within the existing roadway cross-section to limit turning movements along Mitchell Road and E. Hatch Road.

Although not required by CEQA, the project applicant should consider modifying the southern driveway on Mitchell Road and the E. Hatch Road driveway to prohibit left-turn movements out of the site through a combination of median and signage improvements. Restricting left-turns out of both driveways would reduce turning conflicts for vehicles and trucks, representing a safety improvement over existing condition.

RECOMMENDATION TR-5: Prohibit left-turn movements out of the southern Mitchell Road driveway and E. Hatch Road driveway to reduce turning movement conflict on these high-volume roadways, while maintaining left-turn access into both driveways to help minimize vehicle queuing within public right-of-way at nearby intersections.

Maintaining left-turn access into the site, as well as access to Rosewood Avenue (opposite the southern Mitchell Road driveway) and the self-storage facility (opposite the E. Hatch Road driveway), would help reduce queue lengths at the Mitchell Road/E. Hatch Road intersection and the central Mitchell Road driveway. Maintaining inbound access at the southern Mitchell Road driveway would also facilitate direct truck access to the loading docks located on the south side of the project site.

Any modifications to site access at project driveways must be approved by City of Ceres staff and designed in accordance with local design standards.

The queuing model developed for the proposed Dutch Bros shop estimates a 95th percentile queue length of 16 vehicles. All proposed drive-through facilities provide a minimum queueing capacity of 16 vehicles or more.

The revision does not currently propose modifications to site driveways. The 95th percentile drive-through queues anticipated at both the fast-food restaurants and the Dutch Bros location can be accommodated within the designated queuing areas

EIR ADDENDUM

without spilling back onto drive aisles. Additionally, all drive aisles meet minimum design standards (further details on parking lot design are provided below). The project would not increase hazards due to geometric design features; therefore, the impact is considered less than significant.

Although not required by CEQA, implementation of the driveway improvements specified in Recommendation TR-5 would reduce left-turn conflicts for vehicles exiting the project site, representing a safety improvement over existing conditions.

Inadequate emergency access discussed in TR-6 were found to be less than significant.

- The nearest fire station is City of Ceres Fire Station 18, located at 3101 Fowler Road, approximately 1,700 feet south of the project site. The project site includes four driveways, each providing a minimum clear width of 24 feet. The internal drive aisle network also maintains a minimum clear width of 24 feet to all on-site buildings. According to Appendix D of the 2022 California Fire Code, a minimum width of 20 feet is required for fire apparatus access roads serving buildings less than 30 feet in height. Therefore, the revision provides adequate emergency access, and the impact is considered less than significant.

The final site plan must be approved by the City of Ceres Fire Department to ensure the emergency access routes meet requirements to facilitate the safe movement of emergency vehicles. Therefore, this impact is considered less than significant, and no mitigation is required.

Cumulative impacts to transportation & circulation were found to be less than significant.

- A cumulative impact analysis evaluates the combined transportation effects of the proposed Project alongside other reasonably foreseeable developments in the surrounding area. Given the proposed project description and based on the findings of the project-level impact analysis described above, the cumulative analysis is expected to yield similar conclusions. The revisions are not anticipated to contribute substantially to cumulative transportation impacts beyond those already identified and addressed at the project level described above. As such, under cumulative conditions, the proposed Project is expected to result in less than significant transportation impacts pursuant to Appendix G of the CEQA guidelines.

Section 3.9: Public Services and Utilities

The modified project, like the original project, has the potential to affect public services and utilities. The potential for the impacts associated with the original project are discussed in section 3.9 of the Draft EIR. This section discusses: including police, fire, telephone, electricity, natural gas, water, sewer, solid waste, and hospitals. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally,

INTRODUCTION

Mitigation Measures were adopted and will remain applicable to the modified project. The mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require appropriate levels of public services and utilities to serve the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.10: Energy Conservation

The modified project, like the original project, has the potential to affect energy conservation. The potential for the impacts associated with the original project are discussed in section 3.10 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measures were adopted and will remain applicable to the modified project. The mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require appropriate energy conservation designed into the buildings of the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Analysis of Impacts Found to be Significant and Unavoidable

The DEIR indicates that implementation of the original project would create several adverse environmental impacts, which are documented in Section 3.0 of the DEIR documents. The DEIR recommends mitigation measures for most of these impacts which, if implemented, would reduce these impacts to a level of insignificance. However, several impacts were determined to be unavoidable, even within the implementation of mitigation measures described in the DEIR. This section briefly addresses these unavoidable adverse impacts.

Land Use

The original project implementation was determined to have a loss of productive agricultural land within the project site, and that loss would contribute to the cumulative loss of farmland in the Ceres area as the city grows. The modified project is on an already developed site, meaning there will be no loss of productive agricultural land. This significant and unavoidable impact is not applicable to the modified project.

Air Quality

The original project implementation was determined to have a increase activities that generate air pollutant emissions within the project site. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. There amount of construction is very limited and does not include significant site grading as was needed for the original development.

EIR ADDENDUM

Overall, the Modified Project will be required to comply with all rules and regulations of the San Joaquin Valley Air Pollution Control District. It is also notable that emissions associated with traffic under the current year is significantly lower on a per capita basis when compared to emissions in 1989 when the project was originally approved. This significant and unavoidable impact is not applicable to the modified project.

Traffic

The original project implementation was determined to have increased traffic. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. The revisions do not increase the severity of any impact. As mentioned prior, the existing 130,373-square-foot building, previously occupied by a Walmart Superstore, remains entitled for similar commercial uses under current zoning. The Modified Project would redevelop this building and construct additional retail space on-site, resulting in a net increase of approximately 17,396 sf of gross leasable floor area. Since the net increase is less than 50,000 sf, no further VMT analysis is required for the retail land uses, as their VMT impacts are considered less than significant. These revisions would be beneficial as it relates to this environmental topic. However, the revisions do not change the environmental conclusion of significant and unavoidable that was provided in the certified EIR.

○

Noise

The original project implementation was determined to have an increased activities that generate noise. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. The amount of construction is very limited and does not include significant noise generating operations as was needed for the original development. The on-going retail operations would have similar noise levels as occurred during the operation of the Walmart. Overall, the Modified Project will be required to comply with all rules and regulations of the City's Noise Ordinance. The revisions do not increase the severity of any impact. Any changes in site layout or tenant mix would result in negligible differences in overall noise levels compared to existing conditions. However, the revisions do not change the environmental conclusion of significant and unavoidable that was provided in the certified EIR.

Analysis of Growth Inducement

The EIR found that growth inducing impacts associated with the Original Project were primarily related to the economic aspects of the commercial development in that they would provide jobs and could attract additional growth to the area. The Modified Project site was developed and realized any growth inducement associated with the original Walmart. However, the closing of the Walmart resulted in a loss of jobs that

have not been replaced. The Modified Project is expected to provide a similar number of jobs on the site relative what was anticipated under the Original Project.

Analysis of Impacts Found to be Irreversible Changes

The EIR found that the original project would entail the commitment of natural resources, energy resources, and human resources, and that the ongoing maintenance of the project site by the occupants will entail further commitment of energy resources in the form of natural gas and electricity generated by coal, hydro-electric power, or nuclear energy. The EIR found that the original project would irreversibly alter vacant land and convert agricultural uses, and would preclude the use of the site from other uses or open space. The EIR found that these commitments constitute long-term obligations since it is generally impossible to return the land to its original condition once it has been developed. This irreversible change is not applicable to the modified project, as the site has already been converted and it was recognized that there would be a commitment of natural resources, energy resources, and human resources to the project and on-going maintenance.

EIR ADDENDUM



Figure 1. Regional Project Location

Source: R&D Inc Architects, 8-1-225.
Map Date: September 17, 2025.

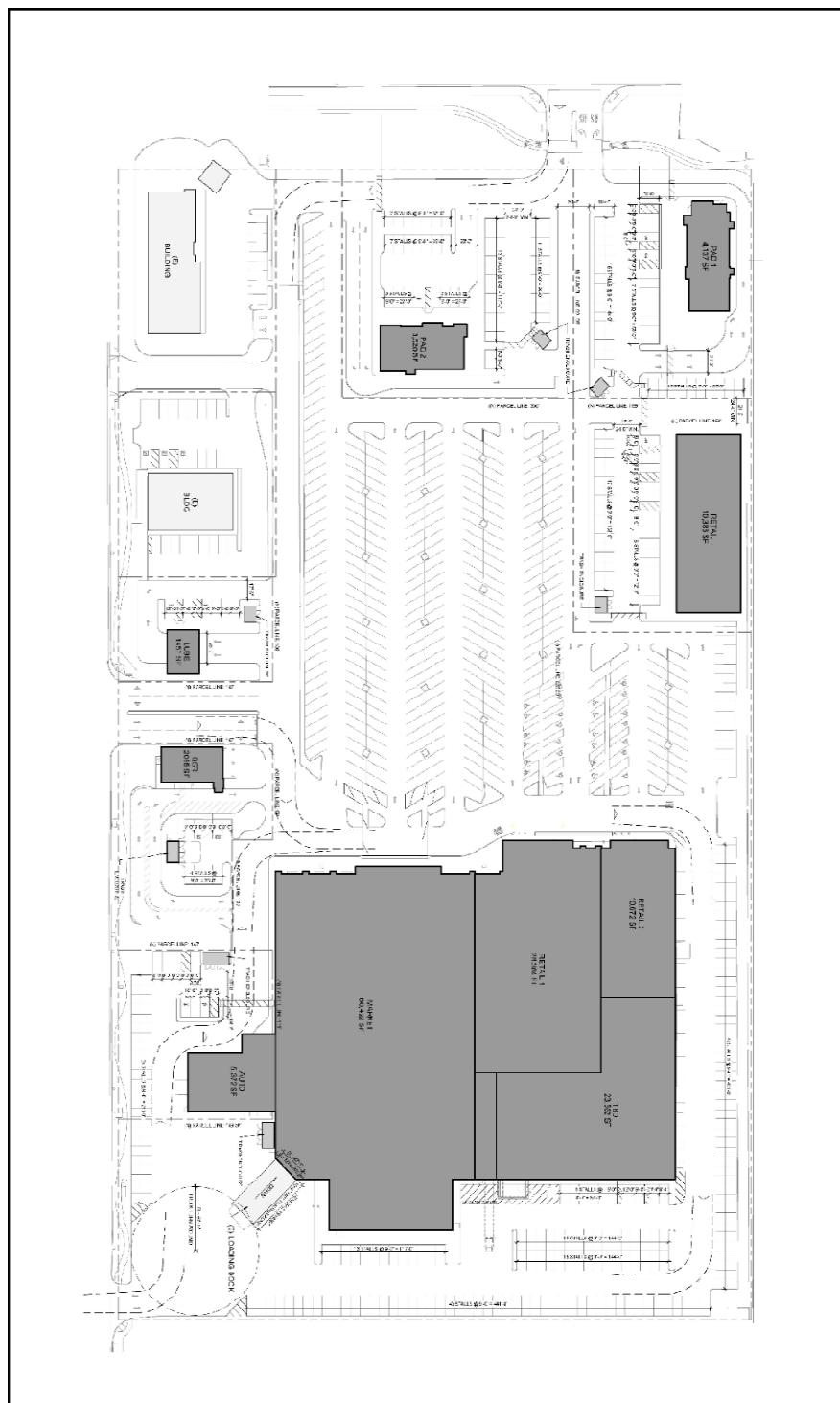


Figure 2. Site Plan

EIR ADDENDUM

Appendix A: CEQA Transportation Impact Analysis



City of Ceres Planning Commission
AGENDA REPORT

MEETING DATE: February 17, 2026

PROJECT NAME: Dutch Bros Coffee Shop

APPLICATION NUMBER: 25-44 CUP Conditional Use Permit and
25-47 SPA Site Plan Application

RECOMMENDATION:

Staff recommend the Planning Commission:

1. Determine that the project is consistent with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15162; and
2. Adopt a Resolution approving 25-44 Conditional Use Permit and 25-47 Site Plan Application for the Dutch Bros Coffee Shop located at 1670 Mitchell Road (APN: 069-011-009).

PROJECT INFORMATION	
Applicant	Sarah Resnick
Property Owner	Vintage Properties, LP
Property Address	1670 Mitchell Road, Ceres, CA 95307
APN	APN: 069-011-009
Existing General Plan:	CC Community Commercial
Existing Zoning District:	CC Community Commercial
Existing Use:	Vacant shopping Center (former Wal-Mart)
Proposed Use:	Coffee Shop
Parcel Size	0.57 acres
Adjacent Uses:	North: E. Hatch Road, Commercial Uses South: Commercial Uses East: TID Canal; Single-Family Residential West: Mitchell Road; Commercial Uses; Single-Family Residential
CEQA Determination	Determination of Consistency with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107)

PROJECT DESCRIPTION AND LOCATION

The proposed project is a Conditional Use Permit and a Site Plan Application for the construction of a new 1,025 square foot Dutch Bros Coffee Shop with a drive-through service window on a 0.57-acre parcel in a larger existing 13.9-acre commercial shopping center (former Wal-Mart site) located at 1670 Mitchell Road.

Site improvements include two drive-through lanes with stacking for up to 19 vehicles, a bypass/exit lane, a trash enclosure, and landscaping. The site detail is visible in Figure 2 below. The building will feature a customer walk-up window on the opposite side of the drive-through. Surface parking for seven vehicles is provided, including one ADA spot.

Figure 1: Project Location in Shopping Center

Figure 1 identifies the location of the site in the larger shopping center area.

As previously stated, the future coffee shop will be located on a new 0.57-acre parcel created by the Tentative Map heard previously by the Planning Commission on the same evening as this item. The new building location is currently vacant and previously served as a parking area and as a portion of the garden center area for the former Wal-Mart.

Access will be available directly from the north and south via existing parking lot drive aisles, with direct access from Mitchell Road. Access to the vehicle drive-through entrance will be located at the southeast portion of the site. Once customers enter the drive-through, they will travel west and wrap around to the Dutch Bros Coffee building in a clockwise direction to meet the drive-through service window. The Project will include



a separate customer window that is oriented to the north side of the building to serve walk-up traffic only.

There is approximately 397 feet of stacking space available behind the drive-through window. This is ample room to ensure no operational impacts to the overall site.

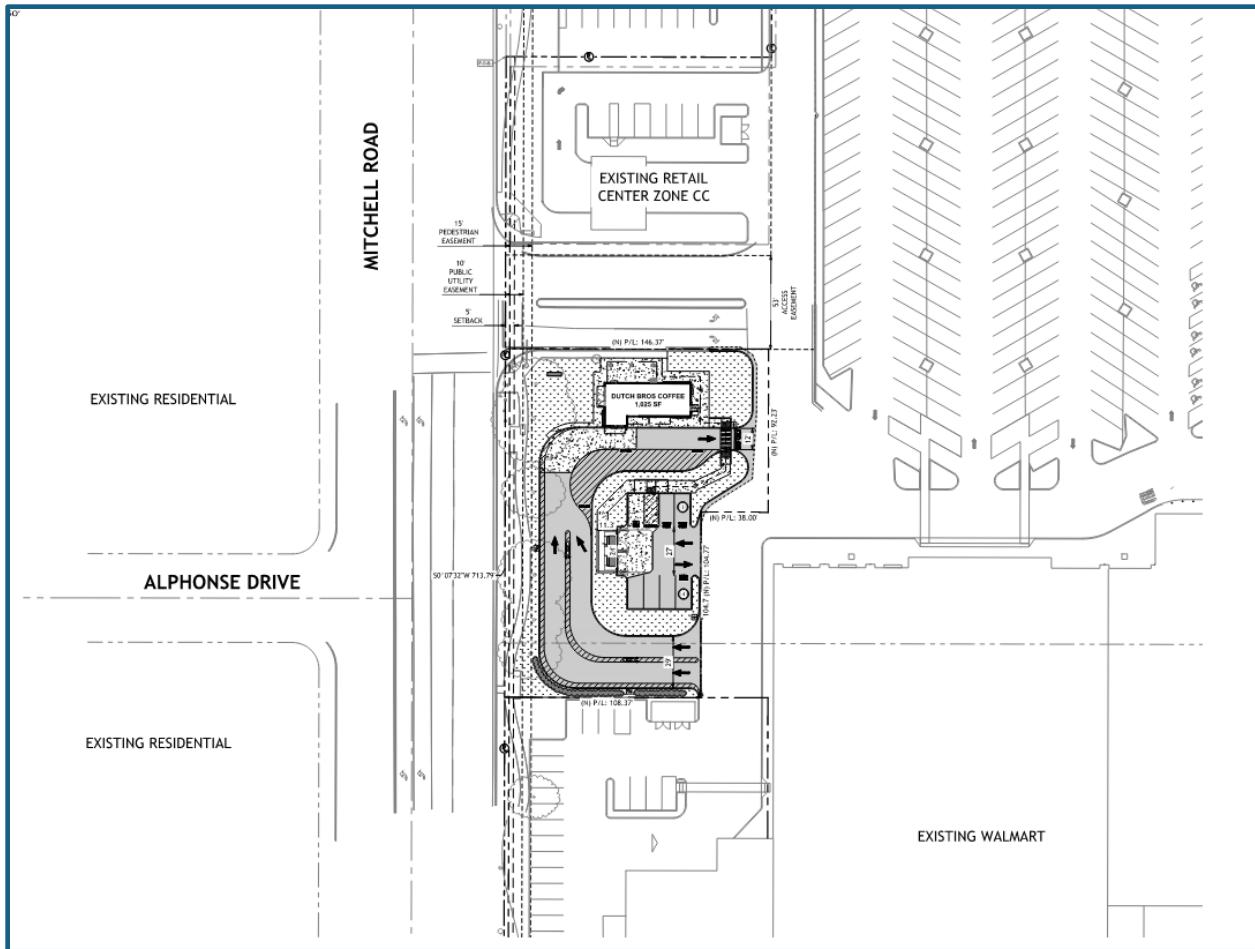


Figure 2: Aerial of Project Site

It is important to note that the drive-through will not include any speaker boxes. All customer orders are taken in person, either at the window or with a runner who carries a handheld device to transmit orders to the kitchen. This ordering process minimizes noise impacts and reduces the amount of vehicle idling. This is important given the residential neighbors directly across Mitchell Road as well as the residences behind the shopping center. While there is ample distance between the homes, it is encouraging that there is no possibility of noise blight from the ordering speaker traveling to these residences.

PROJECT ANALYSIS

The proposed has been analyzed with respect to conformance with:

1. Ceres General Plan
2. Mitchell Road Corridor Specific Plan
3. Ceres Municipal Code
4. California Environmental Quality Act (CEQA)

GENERAL PLAN CONFORMANCE

The project site has a land use designation of CC Community Commercial, as noted in Figure 3 below. The Community Commercial land use designation applies to the use of land for a full range of retail and service uses, including retail stores, food and drug stores, apparel stores, specialty shops, motor vehicle sales and service, home furnishings, real estate offices, restaurants, hotels, and other similar uses that serve a community-wide market. The goal of this land use is to provide convenient services and retail options to the community in one location to limit the need to go to multiple locations to meet a variety of retail needs.

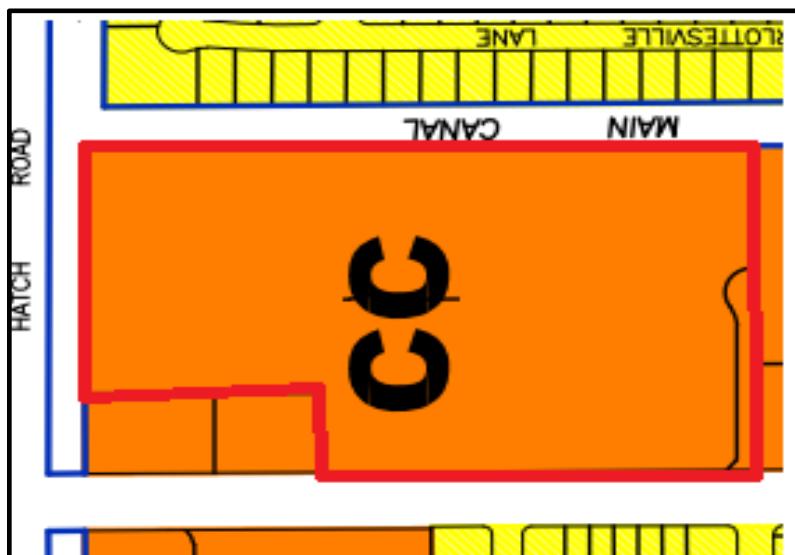


Figure 3: Land Use Diagram

The proposed project is an ideal use in the Community Commercial land use designation as it serves the existing surrounding neighborhood and provides a convenient service to the community. This use will create activity in this underutilized center without any major land use conflicts for the surrounding area. Specifically, there is no outdoor storage, there are no late-night impacts, there is minimal noise given the business model, and the business is utilizing an existing center, thus maximizing existing roadway and utility capacity. This new commercial use will enhance this underutilized site by bringing a highly branded, visually active use to a vacant, underutilized site.

In addition to supporting the CC Community Commercial land use designation, the proposed project is also consistent with the following goals and policies of the General Plan:

1. Infill Goals and Policies

Policy 2.D.1. Promote Infill. Promote infill development and reuse of underutilized parcels in the city to reduce pressure to develop on farmland or other “greenfield” sites on the periphery.

Analysis: The proposed project will be located on a new parcel within an existing shopping center. This project takes advantage of existing infrastructure and supports the General plan’s goals around compact development. Further, the redevelopment of this site will improve streetscape quality, safety, and the visual character of the Mitchell Road Corridor (discussed below). These improvements directly support the General Plan’s objective of replacing low-activation development with active commercial hubs in the City.

2. Economic Development Goals

Goal 7.A Support a wide range of economic activity in Ceres that capitalizes on the city’s location, strengthens the City’s tax base, and supports and enhances quality of life.

Analysis: The proposed project will promote a healthy and diverse local economy by providing a broad range of commercial services that meet the needs of residents and even visitors to the area. As a neighborhood-serving use, Dutch Bros contributes to economic vitality through job creation, increased sales tax generation, and private investment in an infill commercial site. The project will enhance the availability of daily-use retail services within the community, will reduce retail leakage to surrounding jurisdictions, and activate an existing commercial corridor that has fallen victim to vacancies.

Based on the above goals and policies, staff believes that the proposed project is supported by the General Plan.

MITCHELL ROAD CORRIDOR SPECIFIC PLAN

The project site is located in the Mitchell Road Corridor Specific Plan Area (MRCSP or Plan) and has a land use designation of Community Commercial. The Mitchell Road Corridor was adopted by the Ceres City Council in 1989 and amended in 1995. The MRCSP establishes a comprehensive guide for development of about 450 acres located along a 2.5 mile stretch of Mitchell Road between Highway 99 and the Tuolumne River. The Plan provides development controls as it relates to design, site planning, architecture, circulation, open space and lighting throughout the corridor area. The goal of the plan is to guide development in a manner that will produce an urban corridor that integrates industrial, commercial, and residential uses into a vibrant community. The MRCSP is divided into seven land uses along the corridor that are meant to encourage specific goals

or policies of the Plan. The area south of Hatch Road to Roeding Road is meant to support large-scale, region-service commercial uses. As such, the land use diagram notes this area as Community Commercial in Figure 4 below. Community Commercial is intended to accommodate those uses that serve the needs of the community for retail goods and services used on a daily basis.

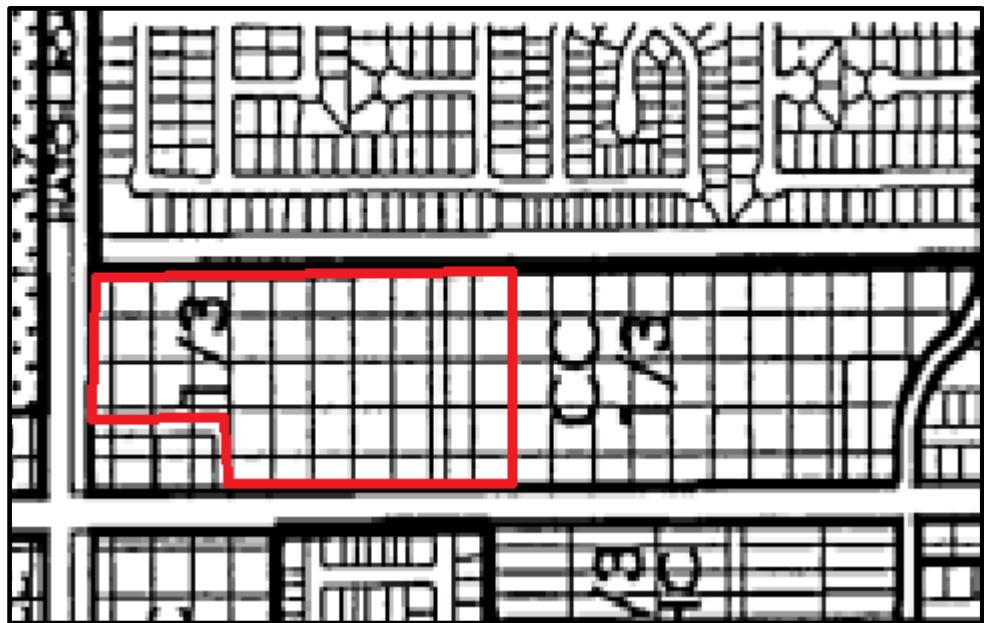


Figure 4: Specific Plan Designation

The proposed Dutch Bros Coffee shop is consistent with the goals and land use framework of the MRCSP as it helps further the vision for a vibrant, connected commercial corridor between Highway 99 and the Tuolumne River. By providing a neighborhood-serving retail/food service use that activates an infill commercial site, the project supports the Specific Plan's intent to encourage a diversity of compatible commercial uses, enhance corridor activity, and attract private investment that strengthens Ceres' economic vitality.

Dutch Bros pedestrian- and automobile-oriented design also aligns with the Plan's mixed-zoning approach and development standards that promote a cohesive, well-functioning commercial environment along Mitchell Road, enhancing the area's role as a key commercial destination in Ceres.

Design Standards

The MRCSP does have general design standards for Community Commercial uses in the Plan area. There is an expectation that buildings will be expressive of conventional, low-scale buildings. The architecture can have individual expression, detail, and variety depending on the user. There is no prohibition on corporate architecture like the proposed elevations in Figure 5.

The Plan suggests that buildings be restricted to two or three elements for each building to emphasize a simple contemporary design. As Figure 5 shows, there is a siding system and some stone veneer to accompany the metal awnings for a contemporary look. The Plan also suggests color themes with lighter earthtones like beige, buff, grays, and creams. As Figure 5 shows, the coloring scheme matches this requirement.

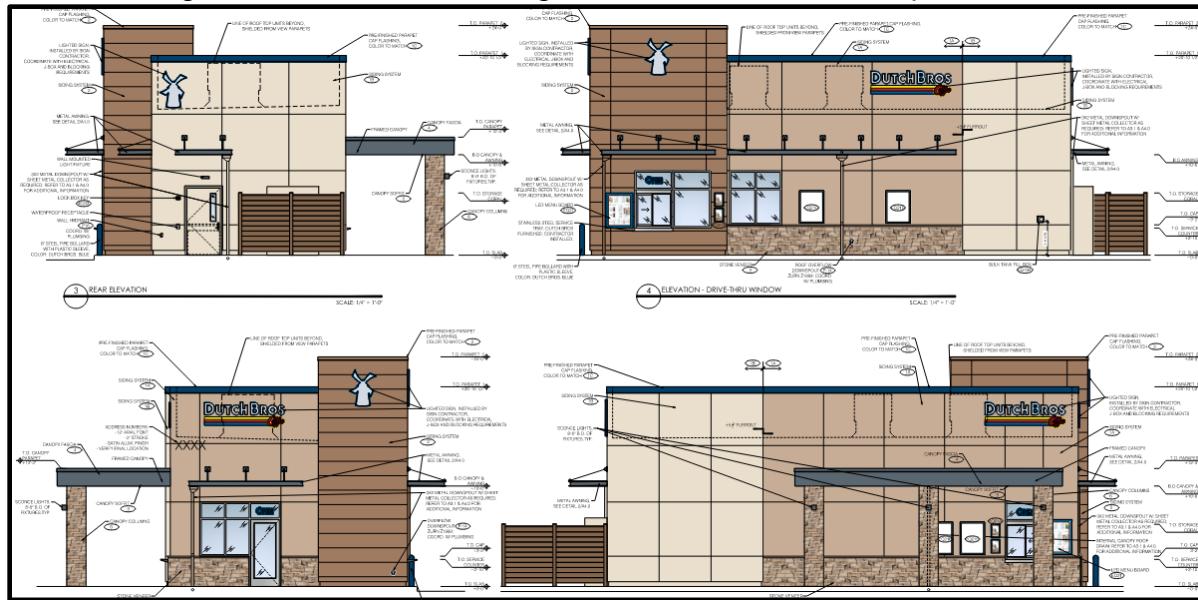


Figure 5: Dutch Bros Elevations

Based on the proposed elevations, the proposed project adheres to the design guidelines of the Specific Plan.

Development Standards

The proposed project is subject to the development standards of the Mitchell Road Corridor Specific Plan. For this zone in the Plan, a project must meet a 15-foot front setback, a 10-foot side setback, a 10-foot rear setback, and the height limit is 35 feet. The proposed project adheres to all of the setback requirements and height requirements.

The Plan also requires two parking spaces per 250 square feet of retail space. The proposed building is 1,025 square feet and requires 4.1 parking spaces. The project is providing seven spaces. Of those spaces, one space needs to be ADA accessible, and the site meets this requirement.

As discussed, the project is consistent with intent, design standards and development standards of the Specific Plan and can be approved by the Commission.

ZONING CODE CONFORMANCE

Allowable Uses

The project site has a zoning district designation of CC Community Commercial, as noted in Figure 6 below. This zone is intended to provide for and promote concentrations of community-oriented commercial uses as well as appropriate public, quasi-public, and administrative professional uses within the City. The proposed development is the type of development planned and expected to be located in the Community Commercial zoning district.

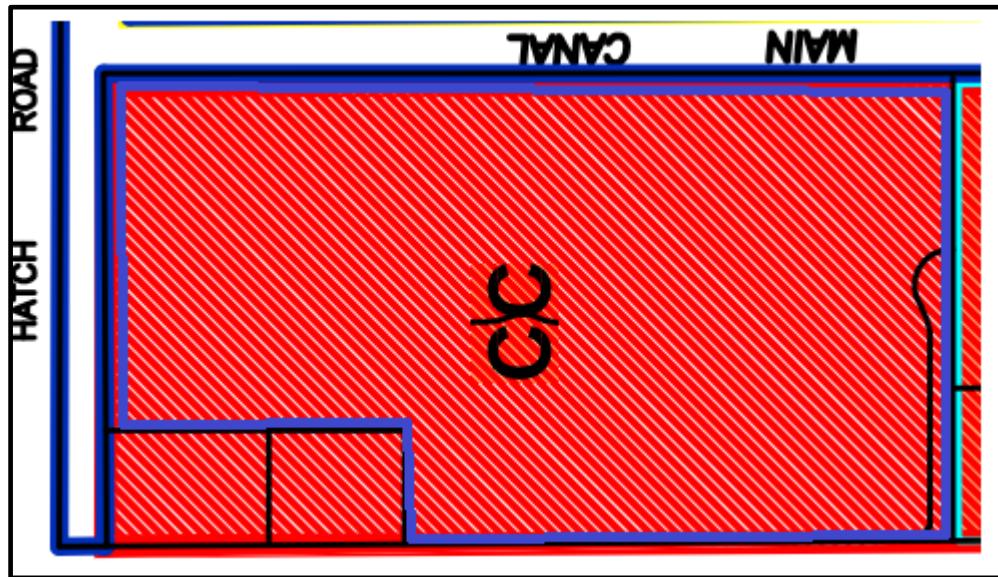


Figure 6: Zoning District Designation

Site Plan Approval

Per Section 18.16.060 (I) a new building in the Community Commercial Zoning District requires site plan approval. There are no specific findings that the Commission needs to make per the code to approve a site plan, but generally, to be able to approve a site plan, the project should be an allowable use in the zoning district; project should be consistent with the General Plan; the project should be consistent with the development standards of the zoning district; the project should provide appropriate and safe circulation in and out of the site and around the site; the project should be in compliance with the California Environmental Quality Act; and the project should generally be a compatible use in the community. As discussed in this staff report, the project conforms to the General Plan land use designation; the project is an allowable use in the zoning district; the project is in conformance with the Mitchell Road Corridor Specific Plan; the project adheres to all development standards; the site has appropriate fire access; the site has appropriate ingress and egress; and the project is consistent with the previously adopted Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15162 (as discussed below).

Conditional Use Permit

Under the Mitchell Road Corridor Specific Plan, a Conditional Use Permit is required when a property is located within 300 feet of a property occupied by residential units and/or is zoned for residential. The location of the project site is within 300 feet of the residential property located across Mitchell Road at Alphonse Drive. As such, this CUP is required. Per Section 18.30.090 of the Municipal Code, the Planning Commission must make the following findings in order to approve a Conditional Use Permit:

- 1. The site for the proposed use is adequate in size and shape to accommodate the use and all yards, spaces, walls and fences, parking, loading, landscaping, and other features required by this title to adjust the use with land and uses in the neighborhood;**

Analysis: The proposal was routed to various departments and agencies for review and was found to be in compliance with the City of Ceres standards for development. The proposed development is in conformity with the current Zoning District and General Plan Designation.

- 2. The site for the proposed use related to streets and highways is adequate in width and pavement type to carry the quantity and kind of traffic generated by the proposed use;**

Analysis: The proposal was routed to the Ceres Engineering Department and the Ceres Public Works Department for review. The site currently has access from a full-service driveway off of Mitchell Road and East Hatch Road. The site is located in an existing shopping center and the project layout will be integrated into the existing flow of traffic for the Center.

- 3. The proposed use will have no adverse effect on abutting property or the permitted use thereof;**

Analysis: The subject project is located in an existing shopping center that has been constructed since the 1990s. The location of the new building adheres to all setback requirements of the Specific Plan and the use is not introducing any new traffic that was not previously contemplated with the prior use. No additional noise will be introduced to the site that will adversely affect abutting property.

- 4. The conditions stated in the decision are deemed necessary to protect the public health, safety, and general welfare.**

Analysis: The proposed project was routed to various departments and agencies, and the Conditions of Approval were prepared based on the comments provided from each department and agency to ensure the protection of public health, safety, and general welfare.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

This project is consistent with the Addendum that was prepared and just adopted for the 1970 Mitchell Road Tentative Map (File No. 25-46 TPM). The Addendum was prepared to serve as an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse SCH # 1988053107) for the Mitchell Road Corridor Specific Plan Project (Original Project). This Addendum not only considered the current parcelization of the subject property, but future subdivision of the existing big box building for new uses, and potential additional smaller retailer users of the pad sites. The Dutch Bros was contemplated in this Addendum as the Dutch Bros was a potential project at the time the Tentative Map was under review. As demonstrated in the environmental analysis provided in Section 4 (Environmental Analysis) of the Addendum, the proposed changes did not meet the criteria for preparing a subsequent EIR or negative declaration. As such a Determination of consistency with the Addendum is appropriate because, as explained in Section 4, none of the conditions calling for preparation of a subsequent EIR or negative declaration occurred.

STAFF RECOMMENDATION

Staff recommends that the Planning Commission:

1. Determine that the project is consistent with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15162; and
2. Adopt a Resolution approving 25-44 Conditional Use Permit and 25-47 Site Plan Application for the Dutch Bros Coffee Shop located at 1670 Mitchell Road (APN: 069-011-009).

Attachments:

1. Attachment A - Draft PC Resolution 2026-XX
2. Attachment B - EIR Addendum – Ceres Mitchell Road Redevelopment Project

Reviewed by:

/s/Lea C. Simvoulakis 2/9/26
Lea C. Simvoulakis, Planning Consultant Date

**DRAFT
RESOLUTION NO. PC 2026-XX**

**RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF CERES
DETERMING THAT THE PROJECT IS CONSISTENT WITH THE ADDENDUM
TO THE MITCHELL ROAD CORRIDOR SPECIFIC PLAN PROJECT
ENVIRONMENTAL IMPACT REPORT (SCH #1988053107) AND APPROVING
A CONDITIONAL USE PERMIT AND SITE PLAN APPLICATION FOR THE
DUTCH BROS COFFEE SHOP LOCATED AT
1670 MITCHELL ROAD, CERES CA 95307
(APN: 069-011-009)
FILE NOS. 25-44 CUP and 25-47 SPA**

WHEREAS, the Planning Commission held a duly noticed special public hearing on February 17, 2026, at 6:00 p.m. and considered a Conditional Use Permit and Site Plan Application for Dutch Bros Coffee Shop located at 1670 Mitchell Road; and

WHEREAS, the property affected by this resolution is located at 1670 Mitchell Road, Ceres, CA 95307, identified by Assessor's Parcel Number 069-011-009; and

WHEREAS, the Planning Commission based on the evidence within the staff report and project file find that:

1. The proposed project is consistent with the purpose and intent of the CC Community Commercial land use designation of the Ceres General Plan in that the CC Community Commercial designation supports the parcelization of large properties.
2. The proposed project is consistent with the purpose and intent of the CC Community Commercial land use designation as outlined in the Mitchell Road Corridor Specific Plan in that the project is a new neighborhood-service use that will provide an amenity to the community and will enhance the visual character of the corridor area.
3. The proposed project is consistent with the CC Community Commercial Zoning District in that the project conforms to all development standards and use requirements of the district.
4. The Commission is able to make all of the Conditional Use Permit findings in that the existing shopping center is of adequate size and shape for the proposed project and any traffic it generates; the project will have no adverse impact on the surrounding properties; and the project has been

conditioned to protect the public health, safety, and general welfare of the community.

5. The project is determined to be consistent with the Addendum prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The Addendum is to the previously certified Environmental Impact Report (EIR) (State Clearinghouse SCH # 1988053107) for the Mitchel Road Corridor Specific Plan Project (Original Project. The previously adopted addendum contemplated the proposed use and determined that there would be no impacts beyond those previously studied under the EIR.
6. The conditions stated in the decision are deemed necessary to protect the public health, safety, and general welfare of the community.

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Ceres as follows:

Section 1. The Planning Commission hereby confirms that the project requires no further environmental review in that the project is consistent with the previously adopted Addendum and no new substantial changes are proposed in the project that require revision of the previous EIR or Addendum as the proposed project was contemplated in the environmental document; there are mitigation measures previously adopted to mitigate and reduce any impacts; and there have been no other substantial changes that merit a revision of the previously certified document.

Section 2. The operation of the proposed project requires compliance with the conditions of approval contained in this resolution and attached as **Exhibit "A"** on file in the Community Development Department, and the approved plan set attached as **Exhibit "B"** which shall be binding on all heirs, assignees, and successors in interest of said property.

Section 3. The Director of Community Development or designee is hereby directed to record this Resolution at the office of the County Recorder of the County of Stanislaus.

Section 4. The conditions of project approval set forth herein include certain fees, dedication requirements, reservation requirements, and other exactions. Pursuant to Government Code Section 66020 (d) (1), these conditions constitute written notice of a statement of the amount of such fees, and a description of the dedications, reservations, and other exactions. You are hereby further notified that the ninety (90) day appeal period in which you may protest these fees, dedications, and reservations other exactions, pursuant to Government Code Section 66020 (a), has begun. If you fail to file a protest within this ninety

(90) day period complying with all of the requirements of Section 66020, you will be legally barred from later challenging such exaction.

I HEREBY CERTIFY that the foregoing Resolution was passed and adopted by the Planning Commission of the City of Ceres at a special public hearing held on the 17th day of February 2026, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

GARY CONDIT, CHAIRPERSON
CITY OF CERES
PLANNING COMMISSION

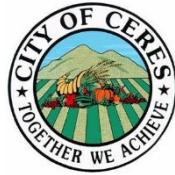
ATTEST:

STEPHANI DAVIS
ADMINISTRATIVE SECRETARY
CITY OF CERES

ATTACHMENTS:

Exhibit A: Conditions of Approval
Exhibit B: Approved Plan Set

EXHIBIT A



City of Ceres Community Development Department Conditions Of Approval- Planning Commission

Approved by the Planning Commission on: February 17, 2026

Project File Nos: 25-44 CUP Conditional Use Permit

25-47 SPA Site Plan Application

Project Name: Dutch Bros Coffee Shop

Project Address: 1670 Mitchell Road, Ceres, CA 95307

APN: 069-011-009

Property Owners: Vintage Properties, LP
3501 Coffee Road, Suite #4
Modesto, CA 95355

Project Applicant: Sarah Resnick, Construction Manager
Dutch Bros
1930 W. Rio Salado Parkway
Tempe, AZ 85281

This list of conditions is not intended to be all-inclusive or a comprehensive list of City regulations. All conditions are referenced to the Dutch Bros Coffee Shop- CA 0917 project on file with the City of Ceres, Community Development Department, Planning Division.

City of Ceres Community Development Department: Planning Division

- Approval.** This Conditional Use Permit and Site Plan Approval is approved subject to these specific conditions. Any variation from the approved plans, proposals, supporting documents or presentations is subject to review and approval prior to implementation.
- Conformance to Plans.** This approval is dependent upon and limited to the proposals and plans contained, supporting documents submitted, and presentations made to staff as affirmed to by the applicant and presented on the plans. Any variation from these plans, proposals, supporting documents or presentations is subject to review and approval prior to implementation.
- Expiration.** This approval shall expire two years from this date on **February 17, 2028**, unless altered and/or extended per State law. Prior to expiration, the

EXHIBIT A

owner/developer may apply for an extension, to be approved by the Community Development Director, not to exceed a total of two (2) years from the original date of expiration. **It shall be the responsibility of the applicant/property owner to be aware of the expiration of this entitlement and take the necessary steps to ensure the approval of this entitlement remains valid. It shall not be the City of Ceres' responsibility to notify the applicant/property owner of the project's expiration date. If an approved entitlement expires, the applicant/property owner shall be required to submit a new application request for this project.**

4. **Owner Obligation.** All conditions of approval shall be satisfied by the owner/developer. If at any time, any of the Conditions of Approval are found to be in non-compliance, City enforcement action may be implemented. Costs associated with compliance with the conditions shall be at the owner/developer's expense.
5. **Fees.** The developer shall pay all applicable processing fees, permit fees, City development fees, fire fees, school fees, drainage fees, habitat conservation fees and other public entity fees in effect at the time of the issuance of a building permit.
6. **Utility Companies.** The applicant is responsible for contacting all appropriate utility companies to obtain agreements for extension and/or relocation of services necessary for the proposed development.
7. **Signs.** No business identification signs or any advertising signs shall be installed or displayed without the review and approval of the Community Development Department, and all proposed advertising signs shall comply with the Sign Standards found in Chapter 26 of Title 18 of the Zoning Ordinance.
8. **Vested Rights.** This approval does not vest applicant's rights regarding future development. All ordinances, resolutions, rules, regulations and official policies governing design, improvement and construction standards and specifications applicable to the project and public improvements to be constructed by the Developer shall be those in force and effect at the time the applicable plan or permit approval is granted.
9. **Responsible Agencies.** The Developer is responsible for contacting all responsible and commenting agencies and ensuring compliance with any applicable fees and/or rules.
10. **Subsequent Development.** All activities undertaken in accordance with this approval shall comply with the City's General Plan and Municipal Code. In cases of conflict between the City's Municipal Code or map-specific conditions of approval, the governing priority shall be, to the extent legally permitted, as follows: 1) Municipal Code regulations, 2) project-specific conditions, and 3) standard conditions. The applicant shall comply with all regulations and code requirements of the Community Development Director, City Engineer, and Building Official, the Fire Chief, the Police Chief, and any other agencies requiring review of the project. If required, these agencies shall be supplied copies of the final maps, site plans, public improvement plans, grading plans, and building plans.

EXHIBIT A

11. **Amendments.** The Planning Division may approve minor amendments to the project, provided that the amendment is still in substantial conformance with the original approval of this entitlement. A request for a minor amendment shall be submitted in writing to the Planning Division. Any request that is not deemed minor in nature must be presented to the Planning Commission through the amendment process and the applicant shall pay the necessary processing fees.
12. **Building Plans.** The project developer shall write all conditions of approval for this project on all building permit check sets submitted for review and approval. These conditions of approval shall be on, at all times, all grading and construction plans kept on the project site. It is the responsibility of the building developer to ensure that the project contractor is aware of, and abides by, all conditions of approval. Prior approval from the Director must be received before any changes are constituted in site design, grading, building design, building colors or materials, etc.
13. **Additional Information.** Upon Building Permit submittal, the applicant is required to show grading, access, paving, striping, storm drain system, frontage landscaping, fencing, sewer and water connections, and backflow devices to the satisfaction of the City Engineer. Civil drawings illustrating the required improvements for the project are required to be submitted with the building plan submittal. Any changes resulting from the review of these plans that cause substantial change to the site plan will require the project return to Planning Commission for a Site Plan Amendment.
14. **Easements.** As determined by the City, the applicant shall secure all necessary easements for reciprocal access, parking, landscaping and/or public utilities over the proposed property. Where required, location of all easements shall be agreed to with the City prior to the issuance of building permits. These easements shall be recorded prior to issuance of a Certificate of Occupancy.
15. **Failure to Comply.** Should the project be found, at any time, not to comply with any of the Conditions of Approval, or should the applicant construct or operate this development in any way other than as specified in the Application or Supporting documents or presentations to staff, the Planning Commission, or City Council, as modified by the Conditions of this Approval, then the terms of this Approval shall be considered to be violated.
16. **Indemnification.** The applicant shall indemnify and hold harmless the City, its council members and commissioners, officers, agents, employees, and representatives from liability for any award, damages, costs and fees, including without limitation attorneys' fees, incurred by the City and/or awarded to any plaintiff in any action related to or arising out of the City's approval of this project or subdivision Map or any environmental or other documentation related to this project. The applicant further agrees to provide a defense for the City in any such action.
17. **Limits of Approval.** Approval of this application does not constitute approval of any other entitlement or any other necessary permit, license, or approval.

EXHIBIT A

18. **Compliance with Local and State Laws.** The subject use shall be conducted in full compliance with all local and state laws. No part of this approval shall be construed to permit a violation of any part of the Ceres Municipal Code. This Site Plan Approval- Staff Level shall be subject to revocation if the subject use is conducted in such a manner as to cause a nuisance.
19. **Changes in Law.** This approval shall not preclude the application to development of the property of changes in City laws, regulations, plans or policies, the term of which are specifically mandated and required by changes in State or Federal laws or regulations.
20. **Other entitlements.** Approval of this application does not constitute approval of any other entitlement or any other necessary permit, license, or approval.
21. **Performance Standards.** Developer shall ensure ongoing compliance with City of Ceres Performance Standards, pursuant to Ceres Municipal Code Chapter 16, Title 18.
22. **Parking, Loading Areas, On-Site Circulation.** All on-site parking, loading areas, and on-site circulation shall conform to the approved plans dated October 30, 2025.
23. **Storage, Screening, and Fencing.** All storage, screening, and fencing shall conform to the approved plans and conform to the Ceres Municipal Code.
24. **Roof-mounted Equipment.** Applicant/developer shall ensure all roof-mounted equipment shall remain screened from public street view.
25. **Lighting.** All on-site lighting for parking areas, pedestrian areas, and vehicular or pedestrian paths of travel shall be LED lighting. A photometric plan showing the project complies with Chapter 25, Title 18 of the Ceres Municipal Code is required at building permit. Any proposed exterior lighting will need to be shielded and/or directed downward and away, so as not to create any glare onto the neighboring properties.
26. **Site Maintenance.** The site shall be permanently maintained in a neat and clean manner free of weeds, trash, and debris.
27. **CPTED.** Landscaping and lighting shall be maintained to compliance with Crime Prevention through Environmental Design (CPTED) guidelines. Shrubs shall be limited to two feet in height, and mature trees shall be limbed up to six feet above the ground. Dead or dying plants shall be replaced with materials of equal size and similar variety.
28. **Paving.** The City will require the applicant to construct the paving improvements (for parking area/access driveway) for the project as proposed on the Site Plan, which shall be completed to the satisfaction of the City Engineer and Building Official.
29. **Temporary Outdoor Storage.** During the construction phase, the applicant shall ensure temporary outdoor storage of items will be screened with a fence or wall, as required. Depending on use, fences or walls shall not exceed eight (8') feet in height including security measures, unless required for environmental mitigation or approval of the Planning Commission or City Council.
30. **Paving.** The City will require the applicant to construct the paving improvements (for parking area/access driveway) for the project as proposed

EXHIBIT A

on the Site Plan, which shall be completed to the satisfaction of the City Engineer and Building Official.

31. **SJVAPCD.** The applicant shall comply with the requirements of the San Joaquin Valley Air Pollution Control District.

City of Ceres Community Development Department: Building Division

32. **Building Requirement.** The following proposal to construct this facility shall be required to be in compliance with the most current adopted California Code of Regulations Title 24 at the time of the building permit application submittal date.

33. **Circulation.** Any and all tenant space(s) with an occupancy category allowing public access shall have an exterior circulation path in accordance with Chapter 11B of Part 1, Volume 2 of the most current adopted California Code of Regulations Title 24.

34. **Building Plans.** The building plans shall be drawn by a licensed engineer or architect.

35. **Demolition Permits.** The applicant shall secure the necessary demolition permit(s) from the City of Ceres to demolish all/any existing structures on site.

36. **Fire Code.** All construction and design shall meet or exceed the California Fire Code for fire and life safety requirements, including exiting and other life safety concerns.

37. **Addresses.** Building addresses for both buildings shall be posted and visible from the roadway in accordance with the California Fire Code and Ceres Municipal Code.

38. **Restrooms.** The applicant shall provide on-site restroom facilities during construction.

39. **Accessible Parking.** Accessible parking stalls shall have a dimension of 17' (feet) x 19' (feet) with the access area on the passenger side. ADA accessibility to the proposed building shall be required per State and Federal Law and to the approval of the Building official.

40. **During Construction.** The following conditions shall apply:

a. If any hazardous waste is encountered during the construction of this project, then all work shall be immediately stopped and the Stanislaus County Environmental Health Department, the Fire Department, the Police Department, and the City Inspector shall be notified immediately. Work shall not proceed until clearance has been issued by all of these agencies.

b. The developer shall keep adjoining public streets free and clean of project dirt, mud, materials, and debris during the construction period, as is found necessary by the City Engineer.

c. If grading is to take place between October 1 and May 1, both temporary and permanent erosion control plans shall be submitted for review and approval along with the grading plan. Permanent erosion control measures shall include hydro-seeding of all graded slopes within 60-days of completion of grading.

EXHIBIT A

- d. Where soil or geologic conditions encountered in grading operations are different from that anticipated in the soil and/or geologic investigation report, or where such conditions warrant changes to the recommendations contained in the original soil investigation, a revised soil or geologic report shall be submitted for approval by the City Engineer.
- e. Sufficient surety guaranteeing the public improvements for a period of one year shall be provided.
- f. If there are substantial changes in the size, alignment, grades, etc. during construction, then original "as-built" plans on the standard size sheets will be certified by the Project Civil Engineer and returned to the City Engineering Services Department office.

City of Ceres Engineering Department

41. General Compliance. The applicant shall comply with all applicable requirements of the City of Ceres, including the Municipal Code, City Engineering Design Standards, the Subdivision Map Act, and all other applicable federal, state, and local regulations pertaining to parcel division and industrial development.

42. Access and Circulation

- a. Legal and physical access shall be provided to each parcel. All access points will be reviewed and conditioned as part of future site development review.
- b. Cross-access easements and internal circulation routes shall be established as needed to support multi-tenant or phased industrial development and shall be recorded prior to issuance of building permits.
- c. Emergency vehicle access shall be reviewed and approved by the City Engineer and Fire Department during site plan review.

43. Utility Services

- a. All water, sewer, storm drainage, and other utility connections shall be evaluated and installed at the time of site development.
- b. Shared utility easements may be required if a single service laterals or joint utility corridors serve multiple parcels.

44. Drainage and Stormwater Compliance

- a. All stormwater management and low-impact development (LID) features shall be incorporated during the site development process, in compliance with the City's stormwater regulations and State Water Resources Control Board requirements.
- b. Drainage easements shall be recorded if off-site or shared drainage paths are proposed in the future development.
- c. Physical frontage improvements, including pavement widening, curb and gutter, sidewalk, and landscaping, will be required at the time of development and shall not be deferred beyond the project-level entitlement.

EXHIBIT A

45. **Fees and Improvement Securities.** All applicable development impact fees, utility connection fees, and securities for public improvements shall be collected and processed during the site development phase or at building permit issuance.
46. **Phasing and Project Coordination.** If development is phased, all conditions applicable to each phase, including frontage and utility improvements, shall be clearly addressed through site plan review and associated improvement plans.
47. **Civil Drawings.** Civil drawings illustrating required improvements (i.e. water and sewer services, a storm drain system, paved parking area, and commercial curb cuts) are required to be submitted with the building plan submittal for each building.
48. **City Standards.** The applicant shall install all required improvements (including parking area, driveway approaches, and curb, gutter, sidewalk) to the Standards of the Engineering Services Department. The applicant shall obtain approval of an Encroachment Permit through the Engineering Services Department prior to constructing these improvements.
 - a. The applicant shall install all required (including curb, gutter, and sidewalk, as well as meter and backflow devices) improvements to the Standards of the Engineering Services Department.
49. **Trash Enclosure.** The applicant shall provide trash enclosure(s) for the project that lock and integrate design elements of the main building. The City will expect trash enclosure(s) to be identified on the site plan with details of structure illustrated on project plans upon the submittal of a building permit application for the project to the Building Division. Additionally, recycling service shall also be provided.
50. **Grease Traps.** The applicant shall provide a standard food service grease trap for each building that has a kitchen/food prep area, which shall be approved by the County of Stanislaus Department of Environmental Resources. The applicant/future tenant(s) shall secure the necessary permits for the food service operations.
51. **Grading Plans.** The applicant will be required to submit a grading and drainage plan to the Engineering Services Department. The paving improvements proposed for the project shall be completed to City Standards. The applicant shall demonstrate that surface water runoff from the proposed building(s), including new paving and access/driveway areas, is conveyed to on-site stormwater retention facilities and that the facilities are adequately sized for the proposed changes.
52. **SWPPP.** The City needs a copy of a Stormwater Pollution Prevention Plan (SWPPP), prior to start of construction (grading). The applicant will be required to implement construction BMP's to prevent sediment from entering the street and the City's stormdrain inlets, which includes gravel drives at entrance/exit points, wattles, and stormdrain inlet filters, as well as keeping concrete wash, chemicals, etc. out of the stormdrain system. This shall be done to the satisfaction of the Engineering Services Department. Additional information on BMP's may be found at: www.cabmphandbooks.com.

EXHIBIT A

53. **Storm Water.** The applicant shall comply with the State Water Regional Control Board Storm water Best Management Practices during construction. The project may require the developer to submit a Notice of Intent (NOI) with the State Water Resources Control Board, and obtain a Waste Discharge Identification Number (WDID). More information may be found at www.waterboards.ca.gov.
54. **Utility Easements.** Additional utility easements might be needed for sewer, water, and storm drainage.
55. **Soils Report.** A soils report may be required for the project. If such report is required, the report shall comply with the requirements of the current edition of the International Building Code with the current California amendments. The report shall provide design parameters for proposed building foundation, soil preparation, and specifications and recommendations for pavement sections including loading areas and fire lanes.
56. **Soils Certification.** The project soils engineer shall submit a soils certification prior to placement of building foundations and pavement.
57. **Pavement Design.** The applicant shall submit pavement design sections to the Engineering Services Department for review and approval prior to placement of pavement.
58. **Water Meter.** The applicant shall contact the Water Department to discuss whether or not the applicant would need to provide more than one (1) water meter for the project.
59. **Water Service.** The applicant shall contact the Water Department to discuss whether or not three (3) separate district water related service lines including fire service, domestic service, and irrigation service would be required.
60. **Sewer Capacity.** The project may be subject to providing the Engineering Services Department (209-538-5792) with water and sewer analysis to ensure the City has sufficient capacity to serve the development.
61. **Encroachment Permit.** Prior to any work being conducted within the City right-of-way and prior to issuance of building permits, the applicant shall obtain an Encroachment Permit from the City. Prior to issuance of the Encroachment Permit, the applicant shall submit to the City of Ceres for review and approval, improvement plans prepared by a registered Civil Engineer; shall pay plan check and inspection fees; shall provide a cost estimate of the improvements; and shall post sufficient surety guaranteeing the construction of the improvements.
62. **Traffic Control Plans.** The applicant shall provide traffic control plans, prepared by a registered civil engineer, for all proposed work within the City right-of-way.
63. **Water Wells.** The applicant shall indicate, in writing to the City, the disposition of any water well(s) and any other water that may exist within the site. If any wells are proposed to be abandoned, or if they are abandoned and have not been properly sealed, they must be destroyed per County of Stanislaus requirements.

EXHIBIT A

City of Ceres Fire Department

64. **Uniform Fire Code.** All construction shall conform to the requirements of the City of Ceres Building Division and City of Ceres Ordinance 84-625, as amended pertaining to the Uniform Fire Code and Ordinance 80-546, as amended, pertaining to Automatic sprinklers.
65. **Fire Extinguishers.** Fire Extinguishers shall be provided in accordance with the California Fire Code. If cooking with grease, a CLASS K Fire Extinguisher is required.
66. **Knox Box.** The applicant shall provide a Knox type locking key box at entrance to each building for Fire Department access. Said Knox type locking key box shall be placed at a location as approved by the Ceres Fire Department. A Knox override switch shall be provided if the entrances into the site include gated access points. Identify Ceres Fire Department as the fire jurisdiction.
67. **Security Lighting.** The applicant shall provide on-site security lighting to the satisfaction of the Ceres Fire Department.
68. **Fire Inspections.** A minimum of 48-hour notice is required for inspections.
69. **Vertical Obstructions.** All vertical obstructions shall be higher than 13 feet 6 inches.
70. **Address Numbers.** Address numbers on the building shall clearly be visible from the street in contrasting color. Front and rear doors to have address numbers (or Suite numbers) 6" (inches) in size.
71. **All Weather Signage.** Permanent all weather signage (minimum 3-inch letters/numbers – red background with white letters – shall be installed at:
 - a. Main Electrical Disconnect
 - b. Main Gas Shutoff
 - c. Electrical Room

City of Ceres Police Department

72. **Public Peace, Safety, Morals.** The project will be required to conform to the provisions of the Public Peace, Safety, and Morals section of the Ceres Municipal Code.

EIR ADDENDUM

FOR THE

CERES MITCHELL ROAD REDEVELOPMENT PROJECT

NOVEMBER 11, 2025

Prepared for:

City of Ceres
Planning Division
2220 Magnolia Street
Ceres, CA 95307
(209) 538-5774

Prepared by:

De Novo Planning Group
1020 Suncast Lane, Suite 106
El Dorado Hills, CA 95762
(916) 580-9818

D e N o v o P l a n n i n g G r o u p

A Land Use Planning, Design, and Environmental Firm

ATTACHMENT B

EIR ADDENDUM

FOR THE

CERES MITCHELL ROAD REDEVELOPMENT PROJECT

NOVEMBER 11, 2025

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EIR ADDENDUM

1 INTRODUCTION

This Addendum was prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. This document has been prepared to serve as an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse [SCH] # 1988053107) for the Mitchel Road Corridor Specific Plan Project (Original Project). The City of Ceres is the lead agency for the proposed amendments to the Original Project, which consists of repurposing an existing 130,373 sf building that was previously operated as a Walmart with new retail uses, and building 17,396 square feet of new retail space for a general retail store, two quick serve restaurants, and a Dutch Bros beverage shop on a 13.4 acre site within the Specific Plan area. The new buildings are intended to maximize the utilization of the site.

For the City to consider an amendment to the Original Project, the City must ensure that, if needed, environmental review consistent with the requirements of the California Environmental Quality Act (CEQA) and the State CEQA Guidelines has been completed. Because the City has previously complied with CEQA for the Original Project and the new discretionary action before the City would be a change in an already-approved project, the City can rely on information in the certified EIR previously prepared for the Original Project, to the extent they remain adequate. Consistent with the requirements of CEQA Guidelines Section 15162, the City must, therefore, determine whether any changed circumstances or “new information of substantial importance” will trigger the need for a subsequent EIR.

As described in CEQA Guidelines Section 15164, a lead agency shall prepare an addendum to a previously adopted EIR if some changes or additions to an EIR certified for a project are necessary, but none of the of conditions identified in CEQA Guidelines Section 15162 have occurred. No subsequent EIR shall be prepared for that project unless the lead agency determines, based on substantial evidence in the light of the whole record, one or more of the following conditions to be applicable:

- (1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- (2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
 - (A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;

EIR ADDENDUM

- (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
- (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
- (D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

If any of the triggers set forth above occurs, the City would be required to prepare a subsequent EIR, unless “only minor additions or changes would be necessary to make the previous EIR adequately apply to the project in the changed situation,” in which case a “supplement to an EIR” would suffice (see CEQA Guidelines, Section 15163). If there are no grounds for either a subsequent EIR or a supplement to an EIR, then the City must prepare an addendum pursuant to CEQA Guidelines Section 15164, explaining why “some changes or additions” to the EIR “are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.”

As demonstrated in the environmental analysis provided in Section 4 (Environmental Analysis), the proposed changes do not meet the criteria for preparing a subsequent EIR or negative declaration. An addendum is appropriate here because, as explained in Section 4, none of the conditions calling for preparation of a subsequent EIR or negative declaration have occurred.

Information and technical analyses from the EIR are utilized throughout this Addendum. Relevant passages from this document (consisting of the EIR) are cited and available for review at:

City of Ceres
2220 Magnolia Street
Ceres, CA 95307
Phone: 209-538-5700

2 PROJECT BACKGROUND AND PURPOSE OF THE EIR ADDENDUM

The Mitchel Road Corridor Specific Plan Project was finalized and approved by the City Council in 1989. Since certification of the EIR, much of the 436.7-acre area has developed with a mix of uses. This includes a 13.4-acre parcel located at the southeast corner of the intersection of Mitchell Road and Hatch Road. The parcel is APN 039-068-009 and is within Planning Area 2 as identified in the Mitchell Road Corridor Specific Plan. The 13.4-acre parcel was built out with a Walmart and other

retail operations; however, Walmart has since closed and new retail operations, including new buildings, are proposed. The following describes the original project.

ORIGINAL PROJECT

The Original Project consisted of adoption and implementation of the Mitchell Road Corridor Specific Plan, which included the development of 436.70 acres as described below.

LOCATION AND BOUNDARIES

The Mitchell Road Corridor study area was located on the eastern edge of the City of Ceres. Ceres is located adjacent to and south of the City of Modesto in Stanislaus County. The Mitchell Road Corridor includes a 2 1/2-mile stretch of Mitchell Road extending between Highway 99 on the south and the Tuolumne River on the north. The corridor is approximately 1/4 mile wide for most of its length but broadens to approximately half mile wide at the northern and southern ends to include large parcels in those areas.

The corridor was within the City jurisdiction except for several parcels in the north and south end which were within the unincorporated area of the County of Stanislaus. Only a small percentage of the corridor was currently developed at the time of approval, with existing commercial uses concentrated predominantly between Fowler and Roeding and at the Hatch/Mitchell and Don Pedro/Mitchell intersections.

STATEMENT OF OBJECTIVES

The Mitchell Road Corridor Specific Plan was intended to achieve the goals and objectives of the City of Ceres through the cost-effective physical and economic development of the corridor. The City intended for the Mitchell Road Corridor Specific Plan to:

1. Provide a means of controlling the direction of growth within the Mitchell Road Corridor.
2. Capitalize on and maximize the economic potential of the Mitchell Road Corridor so that the area can be viewed as an economic asset to the City.
3. Provide a land use scenario that is sensitive to existing and future residential uses in the corridor vicinity.
4. Provide guidelines which will suggest a desired aesthetic quality for the corridor area and incorporate existing and new urban design concepts.
5. Provide a circulation concept that utilizes the road-widening program and maximizes land use potential while minimizing traffic conflicts.
6. Create an image for the City that is distinctive and inviting.
7. Encourage public transit and pedestrian circulation.
8. Provide a varied and interesting building height profile as is suitable.
9. Provide for the short-term, as well as the long-term, needs of the corridor in a comprehensive planning approach.
10. Provide a plan that is sensitive to the noise attenuation needs of the corridor.
11. Provide an environment that is safe for the public at large.

EIR ADDENDUM

PROJECT CHARACTERISTICS

The Mitchell Road Specific Plan involved the intensification of the Mitchell Road Corridor from predominantly vacant and underutilized uses to a planned commercial strip. The table below details the types and quantity of the proposed land uses for the corridor based on maximum buildout of the Specific Plan.

Land Use	Acreage	Square footage
Neighborhood Commercial	52.89	691,200
Planned Commercial	138.45	1,809,300
Highway Commercial	28.86	377,100
Retail Commercial	61.62	805,300
Business Park	105.40	1,377,400
Residential	49.48	282 units
Church	4.44	1
	436.70	

The approach to the land use concept included:

- protecting existing viable residential neighborhoods;
- providing for a variety of commercial opportunities;
- enhancing and capitalizing on the key visual features within the corridor and surrounding area;
- preserving existing significant trees; and
- minimizing traffic conflicts along Mitchell Road.

The Corridor had been designated with a range of commercial land uses. In general terms, the Corridor can be divided into three distinct Planning Areas.

- Planning Area 1 is located between Roeding Road and Highway 99, is visualized as an intense commercial center to serve the City and regional needs.
- Planning Area 2, located between Hatch and Roeding Roads, is intended to serve as a local and regional target for residents to the east and west.
- Planning Area 3, between the Tuolumne River and Hatch Road, is intended to draw users and occupants from the airport and Modesto area.

APPROVALS

As part of the Original Project, the City undertook the following actions:

- Certified the Mitchell Road Corridor Specific Plan EIR;
- Adopted the Mitigation Monitoring and Reporting Program for the project; and

- Approved the Mitchell Road Corridor Specific Plan.

3. PROPOSED PROJECT

Project Site

Modifications to the Original Project are limited to an approximately 13.9-acre site located in the approved 436.7-acre Mitchell Road Corridor Specific Plan area (Modified Project site). The Modified Project site is located at the southeast corner of the intersection of Mitchell Road and Hatch Road and is APN 039-068-009. The Modified Project site is in Planning Area 2 as identified in the Mitchell Road Corridor Specific Plan.

Surrounding Uses

The surrounding area is characterized by a mix of commercial, retail, and service-oriented uses along the Mitchell Road and Hatch Road corridors. At the corner of Mitchell Road and Hatch Road, is APN 039-068-007 which contains a 3,524-square-foot (sf) McDonald's restaurant. Additionally, on APN 039-068-008 is a 3,763-sf car stereo store. Neither of these parcels are part of the Modified Project site, but are immediately adjacent and connected to the Modified Project site.

There are residential neighborhoods located to the east and west.

Existing Uses

Walmart previously occupied the existing 130,373 sf building and associated surface parking areas on-site; however, Walmart has since relocated, leaving the existing building vacant. The building has remained unoccupied since 2021.

Characteristics of the Modified Project

The Modified Project consists of repurposing the existing 130,373 sf building that was previously operated as a Walmart with the following new retail uses:

- 60,478 sf supermarket, likely to be occupied by Vallarta Supermarkets;
- 27,094 sf general retail store, likely to be occupied by Ross Dress for Less;
- 23,258 sf general retail store (tenant currently unknown);
- 9,147 sf general retail store, likely to be occupied by Five Below;
- 5,129 sf auto parts store (tenant currently unknown).

In addition, the Modified Project proposes the construction of the following new buildings on the site:

- 7,470 sf general retail store (tenant currently unknown);
- 4,137 sf (50-seat) fast-food restaurant with drive-through (tenant currently unknown);
- 3,515 sf (50-seat) fast-food restaurant with drive-through (tenant currently unknown);
- 1,288 sf quick oil change center, likely to be occupied by Take 5 Oil Change;

EIR ADDENDUM

- 986 sf (outdoor 12-seat) Dutch Bros specialty beverage shop with drive-through.

The project also proposes expanding the parking capacity to 684 stalls across the entire site. Site access would continue to be provided via the following four driveways:

- East Hatch Road driveway (full access);
- Mitchell Road northern driveway (right-turn in, right-turn out only access);
- Mitchell Road central driveway (full access);
- Mitchell Road southern driveway (full access).

No improvements to the existing driveways or street frontages along Mitchell Road and E. Hatch Road are proposed. The project site vicinity map is shown on Figure 1; the project site plan is provided in Figure 2.

4 ENVIRONMENTAL ANALYSIS

Analysis of Impacts Found to be Less Than Significant

Specific impacts within the following categories of environmental effects were found to be less than significant as set forth in more detail in the certified EIR. Following each bullet is a conclusion on whether the proposed revision changes the impact conclusion found in the original EIR.

Section 3.1: Soils and Geology

The modified project, like the original project, has the potential to directly or indirectly expose residents and property structures to mild ground shaking during seismic occurrences. The potential for the soil and geology impacts associated with the original project are discussed in section 3.1.1 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.1.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require all new structure associated with the modified project to be built in accordance with City Code, Older, existing structures which will remain should be modified to incorporate the latest building and safety standards. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.2: Air Quality

The modified project, like the original project, has the potential to directly or indirectly create air quality impacts. The potential for the impacts associated with the original project are discussed in section 3.2.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.2.3 was

INTRODUCTION

adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require air quality reduction measures for the modified project. This includes compliance with all rules and regulations by the San Joaquin Valley Air Pollution Control District. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.3: Hydrology

The modified project, like the original project, has the potential to directly or indirectly create drainage/flooding impacts, and/or water quality impacts. The potential for the impacts associated with the original project are discussed in section 3.3.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.3.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require a storm drainage plan that handles drainage/flooding impacts as well as water quality impacts associated with the modified project. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.4: Noise

The modified project, like the original project, has the potential to directly or indirectly create noise impacts from traffic, airports, and stationary sources. The potential for the impacts associated with the original project are discussed in section 3.4.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.4.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require stationary sources of noise to be reduced from compliance with City standards and best practices designed into the buildings of the modified project. The project modifications do not create significant changes to the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.5: Aesthetics & Light and Glare

The modified project, like the original project, has the potential to affect visual character, light, and glare. The potential for the impacts associated with the original project are discussed in section 3.5.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.5.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program

EIR ADDENDUM

to require design considerations, including lighting and setbacks in the commercial development of the modified project. The project modifications do not cause significant changes to the visual appearance of the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.6: Land Use

The modified project, like the original project, has the potential to affect land use. The potential for the impacts associated with the original project are discussed in section 3.6.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.6.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require setback requirements, consistency with the Airport Land Use Plan, and review for incompatible uses related to the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.7: Population & Housing

The modified project, like the original project, has the potential to affect population and housing. The potential for the impacts associated with the original project are discussed in section 3.7.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Mitigation Measure 3.7.3 was adopted, but is not relevant to the modified project. The project modifications do not cause significant changes to the land uses that would affect population or housing on the developed site or surrounding, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.8: Transportation and Circulation

The modified project, like the original project, has the potential to affect transportation and circulation. The potential for the impacts associated with the original project are discussed in section 3.8.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.8.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require traffic analysis related to the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR. However, additional VMT analysis is provided below given that such

INTRODUCTION

analysis was not a requirement at the time of the original EIR. The VMT analysis is included as an Appendix.

Vehicle Miles Traveled (VMT) Impacts caused by retail land uses discussed in TR-1 were found to be less than significant.

- The existing 130,373-square-foot building, previously occupied by a Walmart Superstore, remains entitled for similar commercial uses under current zoning. The proposed project would redevelop this building and construct additional retail space on-site, resulting in a net increase of approximately 17,396 sf of gross leasable floor area. Since the net increase is less than 50,000 sf, no further VMT analysis is required for the retail land uses, as their VMT impacts are considered less than significant. Therefore, the revisions do not increase the severity of any impact. However, the revisions do not change the environmental conclusion of less than significant that was provided in the certified EIR.

Conflicts with adopted policies, plans or programs regarding auto network improvements discussed in TR-2 were found to be less than significant.

- The revision does not propose frontage improvements along Mitchell Road or E. Hatch Road; instead, it will maintain the existing driveways as-is. The 2022 RTP/SCS identifies a planned improvement to enhance Mitchell Road to six lanes between SR 99 and the Tuolumne River. The minimum curb-to-curb width along the project site Mitchell Road frontage is 90 feet, which can accommodate a six-lane arterial without on-street parking. Similarly, E. Hatch Road is planned to be widened to a four-lane expressway between Mitchell Road and Faith Home Road, as outlined in the City of Ceres General Plan 2035 (2018). The existing E. Hatch Road frontage provides a minimum 80-foot curb-to-curb width, sufficient to accommodate a four-lane expressway without on-street parking. The revision would not conflict with these future planned auto network improvements along Mitchell Road or E. Hatch Road; therefore, the impact is considered less than significant.

The revision is expected to increase the number of trips using site driveways by 11,180 daily, including 602 during the AM peak hour and 941 during the PM peak hour, compared to prior operations when the Walmart Superstore was active on-site. This increase in peak hour trips at project driveways warrants optimizing traffic signal timings at the adjacent signalized intersections on Mitchell Road (at the Central Driveway and at Hatch Road) to reduce average vehicle delay at both intersections. With the optimization, the revisions do not change the environmental conclusion of less than significant that was provided in the certified EIR.

RECOMMENDATION TR-2: Coordinate with the City of Ceres Engineer to optimize traffic signal timings at the signalized intersections of Mitchell Road/Central Driveway and Mitchell Road/E. Hatch Road.

EIR ADDENDUM

Conflicts with adopted policies, plans or programs regarding bicycle or pedestrian facilities discussed in TR-3 were found to be less than significant.

- The Ceres Citywide Active Transportation Plan (2021) identifies planned Class 4 protected bike lane improvements on Mitchell Road between SR 99 and the Tuolumne River, as well as the installation of high-visibility crosswalks on all legs of the Mitchell Road/E. Hatch Road intersection. The existing 90-foot curb-to-curb width along the Mitchell Road frontage is sufficient to accommodate a six-lane arterial and Class IV protected bike lanes without on-street parking. The project also does not preclude the installation of high-visibility crosswalks at the adjacent Mitchell Road/E. Hatch Road intersection.

The revisions would not conflict with future bicycle or pedestrian facility planned improvements along Mitchell Road or E. Hatch Road; therefore, the impact is considered less than significant.

Inadequate transit service to meet demand discussed in TR-4 were found to be less than significant.

- The revision would not eliminate or adversely affect existing transit access. StanRTA currently operates Route 47, with bus stops located on Mitchell Road, adjacent to the project site. These existing bus stops are within a five-minute walking distance of the project site. Therefore, this impact is considered less than significant, and no mitigation is required

However, the existing StanRTA Route 47 serves a bus stop on northbound Mitchell Road, just north of the project's southern driveway. The bus stop includes signage, a bench, and a trash bin, but lacks a shelter

While not required under CEQA, installing a bus stop shelter could enhance convenience for transit users, particularly during extreme weather conditions, and may encourage greater transit use by project site workers and visitors.

RECOMMENDATION TR-4: Install bus stop shelter at existing StanRTA bus stop on northbound Mitchell Road on project site.

Increased hazards due to geometric design features discussed in TR-5 were found to be less than significant.

- The revision does not propose frontage improvements along Mitchell Road or E. Hatch Road; instead, it will maintain the existing driveways as-is. The revision is expected to increase the number of trips using site driveways by 602 AM and 941 PM peak hour trips compared to prior operations when the Walmart Superstore was active on-site. As shown in Table 2, approximately 2,070 vehicles were counted during the AM peak hour and 2,180 vehicles during the PM peak hour along the Mitchell Road frontage. Along the E. Hatch Road frontage, approximately 1,550

INTRODUCTION

vehicles were counted during the AM peak hour and 1,500 vehicles during the PM peak hour.

These high existing volumes may make it difficult for vehicles to exit the site and turn left at the unsignalized E. Hatch Road driveway and southern Mitchell Road driveway. However, if left-turn delays are significant at these unsignalized driveways, drivers have the option to exit via the signalized central Mitchell Road driveway.

General Plan Policy 3.A.7 supports the development of an expressway system, with E. Hatch Road classified as a Class C expressway. Additionally, Policy 3.B.1 requires that development and redevelopment projects consider the inclusion of medians within the existing roadway cross-section to limit turning movements along Mitchell Road and E. Hatch Road.

Although not required by CEQA, the project applicant should consider modifying the southern driveway on Mitchell Road and the E. Hatch Road driveway to prohibit left-turn movements out of the site through a combination of median and signage improvements. Restricting left-turns out of both driveways would reduce turning conflicts for vehicles and trucks, representing a safety improvement over existing condition.

RECOMMENDATION TR-5: Prohibit left-turn movements out of the southern Mitchell Road driveway and E. Hatch Road driveway to reduce turning movement conflict on these high-volume roadways, while maintaining left-turn access into both driveways to help minimize vehicle queuing within public right-of-way at nearby intersections.

Maintaining left-turn access into the site, as well as access to Rosewood Avenue (opposite the southern Mitchell Road driveway) and the self-storage facility (opposite the E. Hatch Road driveway), would help reduce queue lengths at the Mitchell Road/E. Hatch Road intersection and the central Mitchell Road driveway. Maintaining inbound access at the southern Mitchell Road driveway would also facilitate direct truck access to the loading docks located on the south side of the project site.

Any modifications to site access at project driveways must be approved by City of Ceres staff and designed in accordance with local design standards.

The queuing model developed for the proposed Dutch Bros shop estimates a 95th percentile queue length of 16 vehicles. All proposed drive-through facilities provide a minimum queueing capacity of 16 vehicles or more.

The revision does not currently propose modifications to site driveways. The 95th percentile drive-through queues anticipated at both the fast-food restaurants and the Dutch Bros location can be accommodated within the designated queuing areas

EIR ADDENDUM

without spilling back onto drive aisles. Additionally, all drive aisles meet minimum design standards (further details on parking lot design are provided below). The project would not increase hazards due to geometric design features; therefore, the impact is considered less than significant.

Although not required by CEQA, implementation of the driveway improvements specified in Recommendation TR-5 would reduce left-turn conflicts for vehicles exiting the project site, representing a safety improvement over existing conditions.

Inadequate emergency access discussed in TR-6 were found to be less than significant.

- The nearest fire station is City of Ceres Fire Station 18, located at 3101 Fowler Road, approximately 1,700 feet south of the project site. The project site includes four driveways, each providing a minimum clear width of 24 feet. The internal drive aisle network also maintains a minimum clear width of 24 feet to all on-site buildings. According to Appendix D of the 2022 California Fire Code, a minimum width of 20 feet is required for fire apparatus access roads serving buildings less than 30 feet in height. Therefore, the revision provides adequate emergency access, and the impact is considered less than significant.

The final site plan must be approved by the City of Ceres Fire Department to ensure the emergency access routes meet requirements to facilitate the safe movement of emergency vehicles. Therefore, this impact is considered less than significant, and no mitigation is required.

Cumulative impacts to transportation & circulation were found to be less than significant.

- A cumulative impact analysis evaluates the combined transportation effects of the proposed Project alongside other reasonably foreseeable developments in the surrounding area. Given the proposed project description and based on the findings of the project-level impact analysis described above, the cumulative analysis is expected to yield similar conclusions. The revisions are not anticipated to contribute substantially to cumulative transportation impacts beyond those already identified and addressed at the project level described above. As such, under cumulative conditions, the proposed Project is expected to result in less than significant transportation impacts pursuant to Appendix G of the CEQA guidelines.

Section 3.9: Public Services and Utilities

The modified project, like the original project, has the potential to affect public services and utilities. The potential for the impacts associated with the original project are discussed in section 3.9 of the Draft EIR. This section discusses: including police, fire, telephone, electricity, natural gas, water, sewer, solid waste, and hospitals. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally,

INTRODUCTION

Mitigation Measures were adopted and will remain applicable to the modified project. The mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require appropriate levels of public services and utilities to serve the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.10: Energy Conservation

The modified project, like the original project, has the potential to affect energy conservation. The potential for the impacts associated with the original project are discussed in section 3.10 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measures were adopted and will remain applicable to the modified project. The mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require appropriate energy conservation designed into the buildings of the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Analysis of Impacts Found to be Significant and Unavoidable

The DEIR indicates that implementation of the original project would create several adverse environmental impacts, which are documented in Section 3.0 of the DEIR documents. The DEIR recommends mitigation measures for most of these impacts which, if implemented, would reduce these impacts to a level of insignificance. However, several impacts were determined to be unavoidable, even within the implementation of mitigation measures described in the DEIR. This section briefly addresses these unavoidable adverse impacts.

Land Use

The original project implementation was determined to have a loss of productive agricultural land within the project site, and that loss would contribute to the cumulative loss of farmland in the Ceres area as the city grows. The modified project is on an already developed site, meaning there will be no loss of productive agricultural land. This significant and unavoidable impact is not applicable to the modified project.

Air Quality

The original project implementation was determined to have a increase activities that generate air pollutant emissions within the project site. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. There amount of construction is very limited and does not include significant site grading as was needed for the original development.

EIR ADDENDUM

Overall, the Modified Project will be required to comply with all rules and regulations of the San Joaquin Valley Air Pollution Control District. It is also notable that emissions associated with traffic under the current year is significantly lower on a per capita basis when compared to emissions in 1989 when the project was originally approved. This significant and unavoidable impact is not applicable to the modified project.

Traffic

The original project implementation was determined to have increased traffic. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. The revisions do not increase the severity of any impact. As mentioned prior, the existing 130,373-square-foot building, previously occupied by a Walmart Superstore, remains entitled for similar commercial uses under current zoning. The Modified Project would redevelop this building and construct additional retail space on-site, resulting in a net increase of approximately 17,396 sf of gross leasable floor area. Since the net increase is less than 50,000 sf, no further VMT analysis is required for the retail land uses, as their VMT impacts are considered less than significant. These revisions would be beneficial as it relates to this environmental topic. However, the revisions do not change the environmental conclusion of significant and unavoidable that was provided in the certified EIR.

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Noise

The original project implementation was determined to have an increased activities that generate noise. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. The amount of construction is very limited and does not include significant noise generating operations as was needed for the original development. The on-going retail operations would have similar noise levels as occurred during the operation of the Walmart. Overall, the Modified Project will be required to comply with all rules and regulations of the City's Noise Ordinance. The revisions do not increase the severity of any impact. Any changes in site layout or tenant mix would result in negligible differences in overall noise levels compared to existing conditions. However, the revisions do not change the environmental conclusion of significant and unavoidable that was provided in the certified EIR.

Analysis of Growth Inducement

The EIR found that growth inducing impacts associated with the Original Project were primarily related to the economic aspects of the commercial development in that they would provide jobs and could attract additional growth to the area. The Modified Project site was developed and realized any growth inducement associated with the original Walmart. However, the closing of the Walmart resulted in a loss of jobs that

have not been replaced. The Modified Project is expected to provide a similar number of jobs on the site relative what was anticipated under the Original Project.

Analysis of Impacts Found to be Irreversible Changes

The EIR found that the original project would entail the commitment of natural resources, energy resources, and human resources, and that the ongoing maintenance of the project site by the occupants will entail further commitment of energy resources in the form of natural gas and electricity generated by coal, hydro-electric power, or nuclear energy. The EIR found that the original project would irreversibly alter vacant land and convert agricultural uses, and would preclude the use of the site from other uses or open space. The EIR found that these commitments constitute long-term obligations since it is generally impossible to return the land to its original condition once it has been developed. This irreversible change is not applicable to the modified project, as the site has already been converted and it was recognized that there would be a commitment of natural resources, energy resources, and human resources to the project and on-going maintenance.

EIR ADDENDUM



Figure 1. Regional Project Location

INTRODUCTION

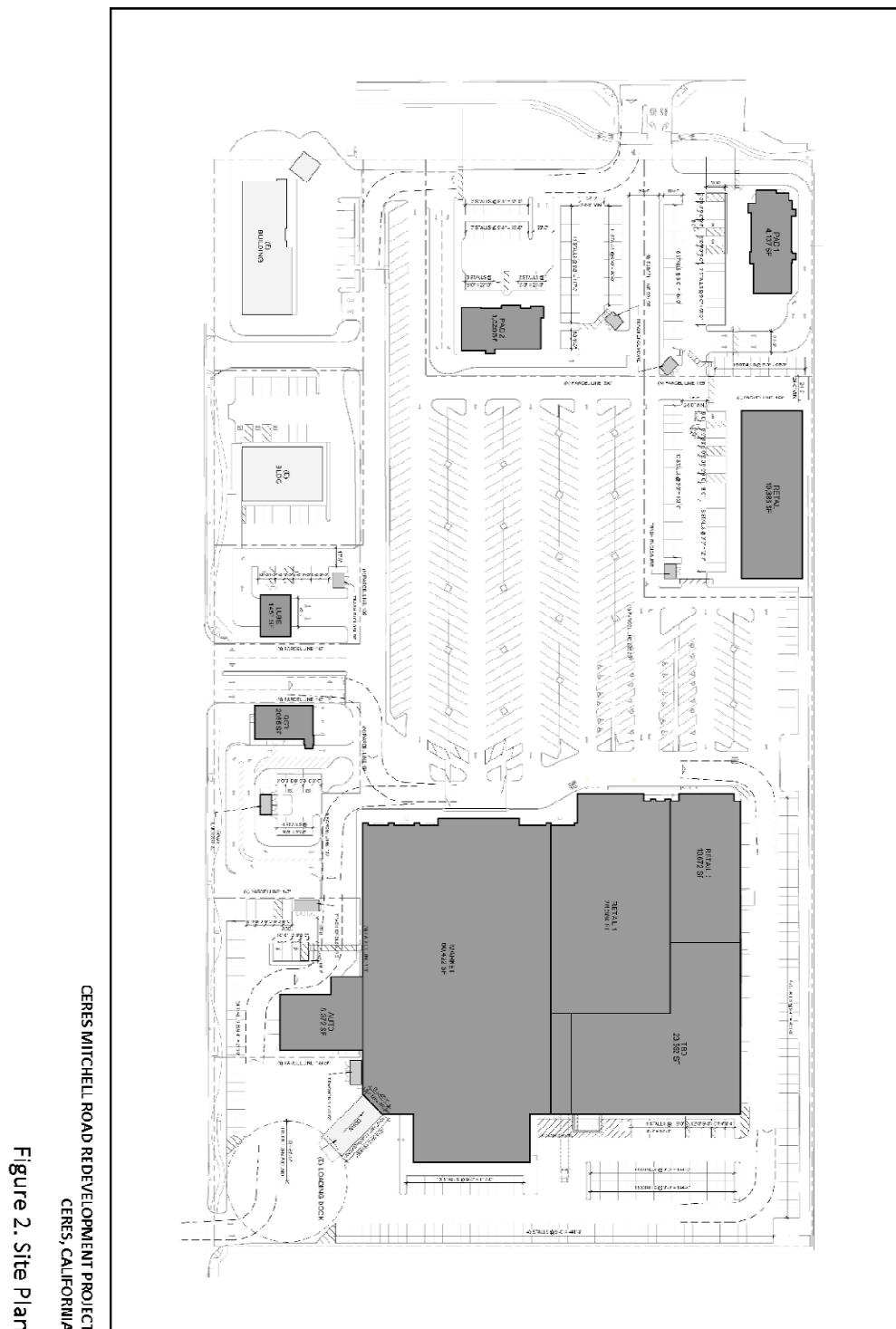


Figure 2. Site Plan

Source: DE NOVO PLANNING GROUP, 3'-0" X 2'-0"
May 2014, Site Plan, Ceres Mitchell Road Redevelopment Project

EIR ADDENDUM

Appendix A: CEQA Transportation Impact Analysis



City of Ceres Planning Commission
AGENDA REPORT

MEETING DATE: February 17, 2026

PROJECT NAME: Take 5 Oil Change

APPLICATION NUMBER: 25-31 CUP Conditional Use Permit and
25-48 SPA Site Plan Application

RECOMMENDATION:

Staff recommend the Planning Commission:

1. Determine that the project is consistent with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15162; and
2. Adopt a Resolution approving 25-31 Conditional Use Permit and 25-48 Site Plan Application for the Take 5 Oil Change located at 1670 Mitchell Road (APN: 069-011-009).

PROJECT INFORMATION	
Applicant	Phillip Spence- Durban Development
Property Owner	Vintage Properties, LP
Property Address	1670 Mitchell Road, Ceres, CA 95307
APN	APN: 069-011-009
Existing General Plan:	CC Community Commercial
Existing Zoning District:	CC Community Commercial
Existing Use:	Vacant shopping Center (former Wal-Mart)
Proposed Use:	Automotive Service – Oil Change
Parcel Size	0.38 acres
Adjacent Uses:	North: E. Hatch Road, Commercial Uses South: Commercial Uses East: TID Canal; Single-Family Residential West: Mitchell Road; Commercial Uses; Single-Family Residential
CEQA Determination	Determination of Consistency with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107)

PROJECT DESCRIPTION AND LOCATION

The proposed project is a Conditional Use Permit and a Site Plan Application for the construction of a new 1,438 square foot Take 5 Oil Change building with three queuing lanes to access the structure, 12 new parking stalls (ten exterior spaces and three bay spaces), new landscaping and paving, and trash enclosure on a 0.38-acre parcel in a larger existing 13.9-acre commercial shopping center (former Wal-Mart site) located at 1670 Mitchell Road. Figure 1 identifies the location of the site in the larger shopping center area.

Figure 1: Project Location in Shopping Center

The overall project will be oriented toward Mitchell Road with entrance bay doors on the east side and exit bay doors on the west side. Customers will enter and exit the existing commercial development using existing driveways. As Figure 2 shows, the layout of the site is designed to create the best possible flow and maximum queuing of vehicles, thereby reducing the impact and spillover onto adjacent rights-of-way and properties. The circulation pattern accommodates the turning radii of the Fire Department and waste hauler vehicles.

The three queuing lanes will all be 60 feet in length and can accommodate a total of nine vehicles, with three vehicles in each lane. Each bay inside the building will hold one vehicle for a total of four parked vehicles.

Take 5 Oil Change offers the following services: oil change, vehicle inspection, engine and cabin air cleaning and filtration, light bulb replacement, undercoating, power steering and transmission fluid change, wiper replacement installation, air filter replacement, and seasonal tire changeover. Approximately 45 vehicles are expected to visit the site daily, based on data from other Take 5 locations. The store will have six to ten employees, depending on volume and demand.



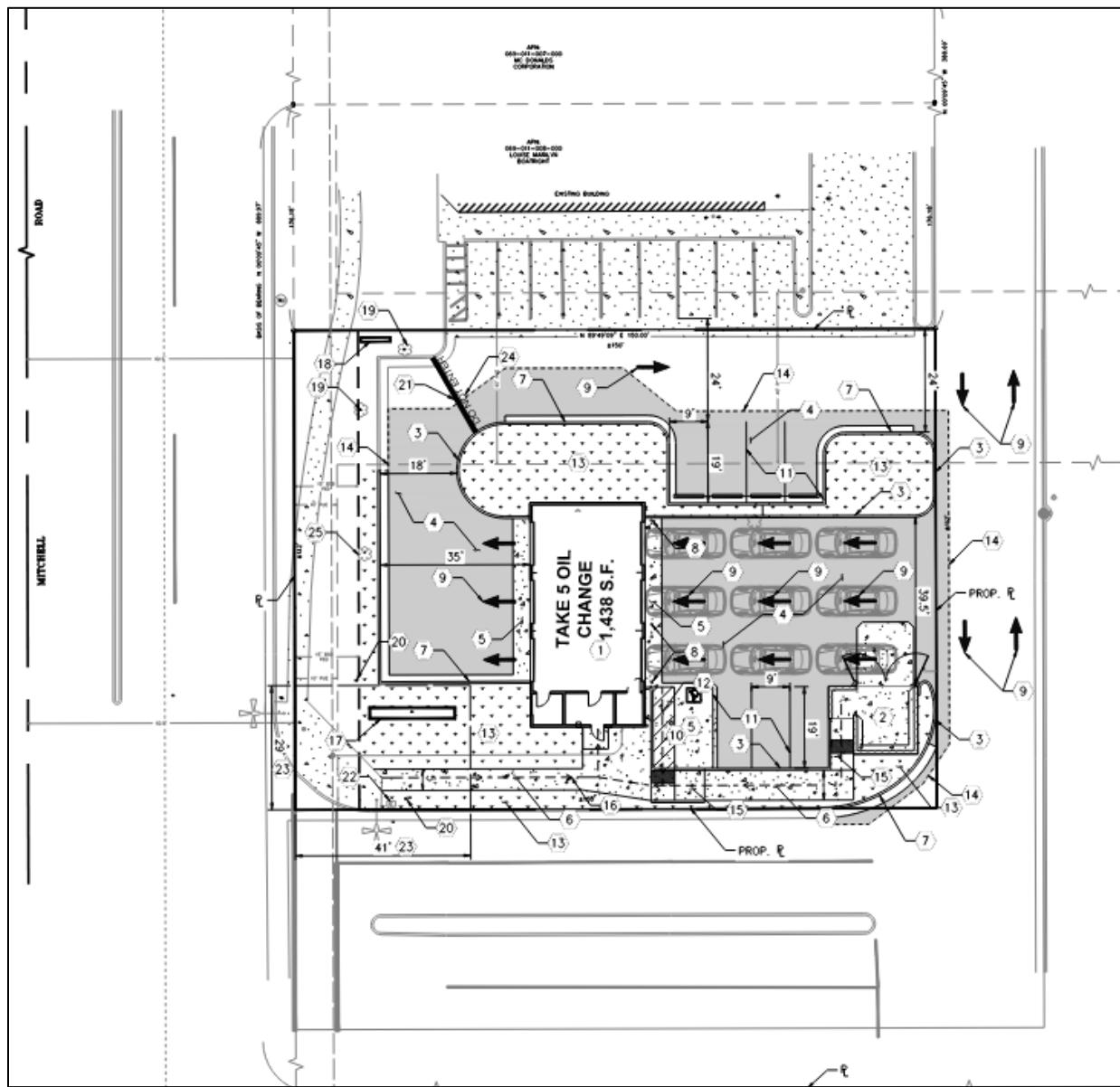


Figure 2: Aerial of Project Site

PROJECT ANALYSIS

The proposed has been analyzed with respect to conformance with:

1. Ceres General Plan
2. Mitchell Road Corridor Specific Plan
3. Ceres Municipal Code
4. California Environmental Quality Act (CEQA)

GENERAL PLAN CONFORMANCE

The project site has a land use designation of CC Community Commercial, as noted in Figure 3 below. The Community Commercial land use designation applies to the use of land for a full range of retail and service uses, including retail stores, food and drug stores, apparel stores, specialty shops, motor vehicle sales and service, home furnishings, real estate offices, restaurants, hotels, and other similar uses that serve a community-wide market. The goal of this land use is to provide convenient services and retail options to the community in one location to limit the need to go to multiple locations to meet a variety of retail needs.

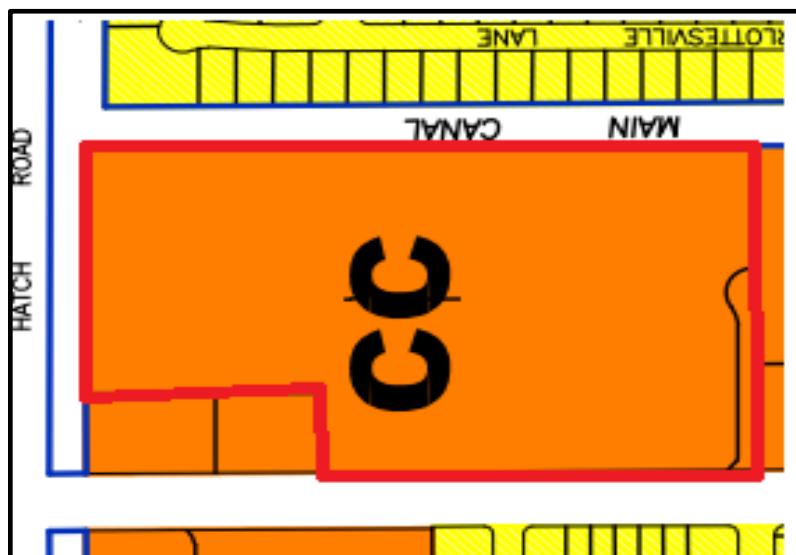


Figure 3: Land Use Diagram

The proposed project is an ideal use in the Community Commercial land use designation as it serves the existing surrounding neighborhood and provides a convenient service to the community. This use will create activity in this underutilized center without any major land use conflicts for the surrounding area. Specifically, there is no outdoor storage, there are no late-night impacts, there is minimal noise given the business model, and the business is utilizing an existing center, thus maximizing existing roadway and utility capacity. This new commercial use, both highly branded and visually active, will enhance an otherwise vacant and underutilized site.

In addition to supporting the CC Community Commercial land use designation, the proposed project is also consistent with the following goals and policies of the General Plan:

1. Infill Goals and Policies

Policy 2.D.1. Promote Infill. Promote infill development and reuse of underutilized parcels in the city to reduce pressure to develop on farmland or other “greenfield” sites on the periphery.

Analysis: The proposed project will be located on a new parcel within an existing shopping center. This project takes advantage of existing infrastructure and supports the General plan's goals around compact development. Further, the

redevelopment of this site will improve streetscape quality, safety, and the visual character of the Mitchell Road Corridor (discussed below). These improvements directly support the General Plan's objective of replacing low-activation development with active commercial hubs in the City.

2. Economic Development Goals

Goal 7.A Support a wide range of economic activity in Ceres that capitalizes on the city's location, strengthens the City's tax base, and supports and enhances quality of life.

Analysis: The proposed project will promote a healthy and diverse local economy by providing a broad range of commercial services that meet the needs of residents and even visitors to the area. The new store will provide employment opportunities for local residents, will reduce retail leakage to surrounding jurisdictions, and activate an existing commercial corridor that has fallen victim to vacancies.

Based on the above goals and policies, staff believes that the proposed project is supported by the General Plan.

MITCHELL ROAD CORRIDOR SPECIFIC PLAN

The project site is located in the Mitchell Road Corridor Specific Plan Area (MRCSP or Plan) and has a land use designation of Community Commercial. The Mitchell Road Corridor was adopted by the Ceres City Council in 1989 and amended in 1995. The MRCSP establishes a comprehensive guide for development of about 450 acres located along a 2.5 mile stretch of Mitchell Road between Highway 99 and the Tuolumne River. The Plan provides development controls as it relates to design, site planning, architecture, circulation, open space and lighting throughout the corridor area.

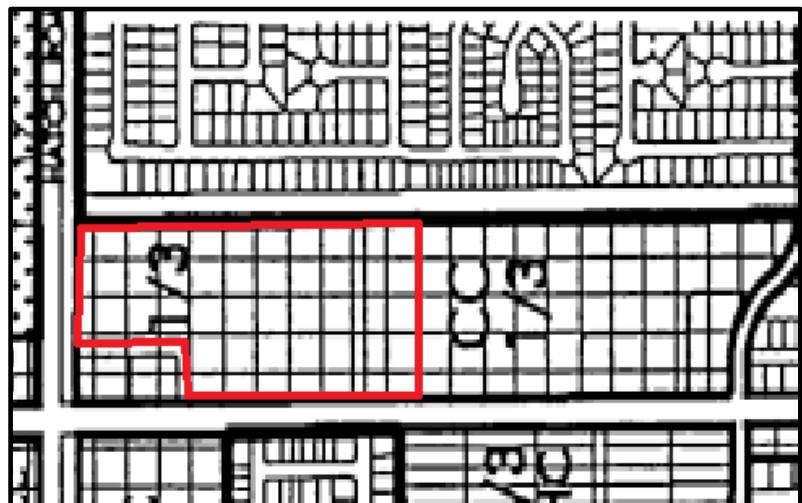


Figure 4: Specific Plan Designation

The goal of the plan is to guide development in a manner that will produce an urban corridor that integrates industrial, commercial, and residential uses into a vibrant community. The MRCSP is divided into seven land uses along the corridor that are meant to encourage specific goals or policies of the Plan. The area south of Hatch Road to Roeding Road is meant to support large-scale, region-service commercial uses. As such, the land use diagram notes this area as Community Commercial in Figure 4 below.

Community Commercial is intended to accommodate those uses that serve the needs of the community for retail goods and services used on a daily basis.

The proposed Take 5 Oil Change establishment is consistent with the goals and land use framework of the MRCSP as it helps further the vision for a vibrant, connected commercial corridor between Highway 99 and the Tuolumne River. By providing a neighborhood-serving retail service use that activates an infill commercial site, the project supports the Specific Plan's intent to encourage a diversity of compatible commercial uses, enhance corridor activity, and attract private investment that strengthens Ceres' economic vitality.

Design Standards

The MRCSP does have general design standards for Community Commercial uses in the Plan area. There is an expectation that buildings will be expressive of conventional, low-scale buildings. The architecture can have individual expression, detail, and variety depending on the user. There is no prohibition on corporate architecture like the proposed elevations in Figure 5.

The Plan suggests that buildings be restricted to two or three elements for each building to emphasize a simple contemporary design. As Figure 5 shows, the project will have earthtone colors with an attractive red metal roof, the garage doors will be paneled and not just regular roll-up doors, and the variation in color breaks the façade up for visual interest.

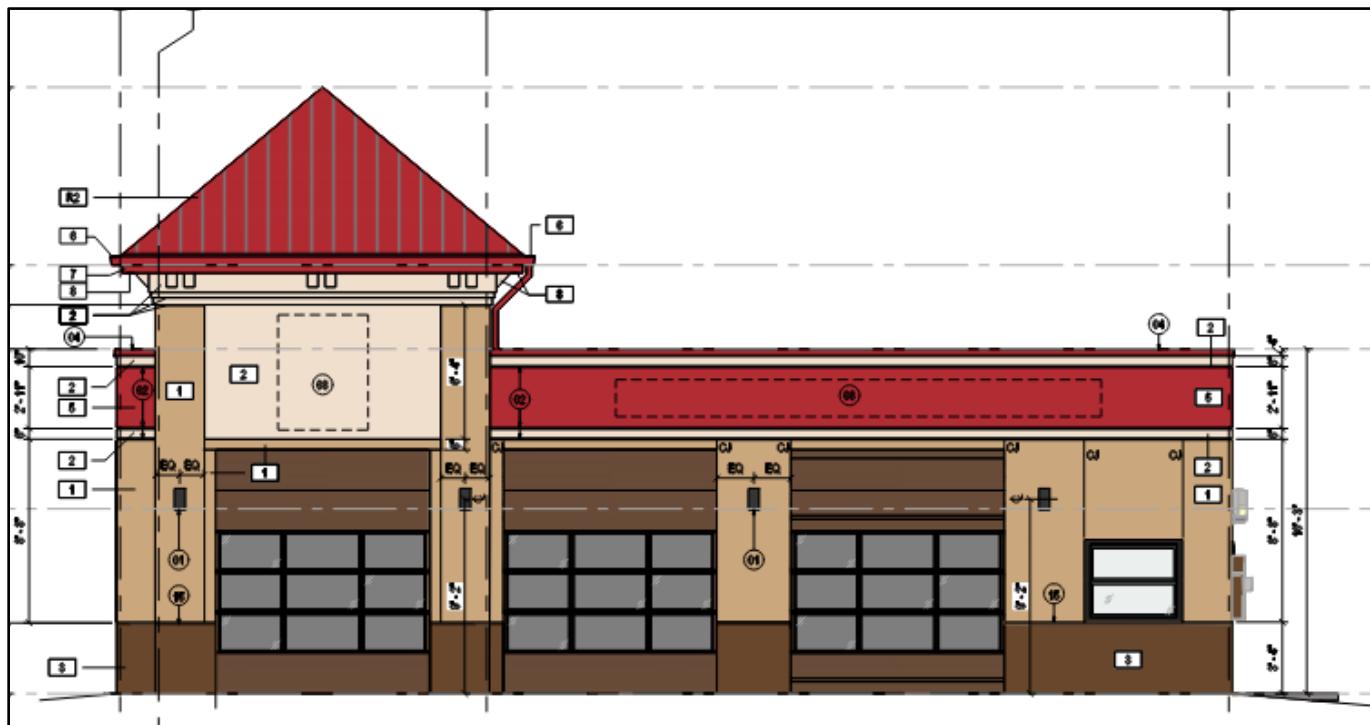


Figure 5: Dutch Bros Elevations

Based on the proposed elevations, the proposed project adheres to the design guidelines of the Specific Plan.

Development Standards

The proposed project is subject to the development standards of the Mitchell Road Corridor Specific Plan. For this zone in the Plan, a project must meet a 15-foot front setback, a 10-foot side setback, a 10-foot rear setback, and the height limit is 35 feet. The proposed project adheres to all of the setback requirements and height requirements. The tallest point of the project is 28 feet 7 inches.

The Plan also requires two parking spaces per 250 square feet of retail space. The proposed building is 1,438 square feet and requires 5.7 parking spaces. The project provides 12 spaces. Of those spaces, one space needs to be ADA accessible and the site meets this requirement.

As discussed, the project is consistent with intent, design standards and development standards of the Specific Plan and can be approved by the Commission.

ZONING CODE CONFORMANCE

Allowable Uses

The project site has a zoning district designation of CC Community Commercial, as noted in Figure 6 below. This zone is intended to provide for and promote concentrations of community-oriented commercial uses as well as appropriate public, quasi-public, and administrative professional uses within the City. The proposed development is the type of development planned and expected to be located in the Community Commercial zoning district.

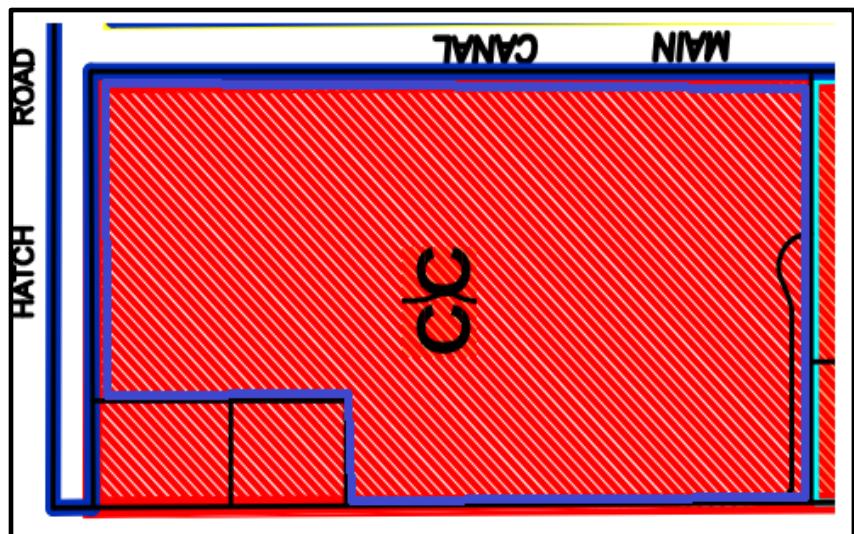


Figure 6: Zoning District Designation

Site Plan Approval

Per Section 18.16.060(I) a new building in the Community Commercial Zoning District requires site plan approval. There are no specific findings that the Commission needs to make per the code to approve a site plan, but generally, to be able to approve a site plan,

the project should be an allowable use in the zoning district; the project should be consistent with the General Plan; the project should be consistent with the development standards of the zoning district; the project should provide appropriate and safe circulation in and out of the site and around the site; the project should be in compliance with the California Environmental Quality Act; and the project should generally be a compatible use in the community. As discussed in this staff report, the project conforms to the General Plan land use designation; the project is an allowable use in the zoning district; the project is in conformance with the Mitchell Road Corridor Specific Plan; the project adheres to all development standards; the site has appropriate fire access; the site has appropriate ingress and egress; and the project is consistent with the previously adopted Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15162 (as discussed below).

Conditional Use Permit

Under the Mitchell Road Corridor Specific Plan, a Conditional Use Permit (CUP) is required when a property is located within 300 feet of a property occupied by residential units and/or is zoned for residential. The location of the project site is within 300 feet of the residential property located across Mitchell Road at Alphonse Drive. As such, this CUP is required. Per Section 18.30.090 of the Municipal Code, the Planning Commission must make the following findings in order to approve a Conditional Use Permit:

- 1. The site for the proposed use is adequate in size and shape to accommodate the use and all yards, spaces, walls and fences, parking, loading, landscaping, and other features required by this title to adjust the use with land and uses in the neighborhood;**

Analysis: The proposal was routed to various departments and agencies for review and was found to be in compliance with the City of Ceres standards for development. The proposed development is in conformity with the current Zoning District and General Plan Designation.

- 2. The site for the proposed use related to streets and highways is adequate in width and pavement type to carry the quantity and kind of traffic generated by the proposed use;**

Analysis: The proposal was routed to the Ceres Engineering Department and the Ceres Public Works Department for review. The site currently has access from a full-service driveway off of Mitchell Road and East Hatch Road. The site is located in an existing shopping center and the project layout will be integrated into the existing flow of traffic for the Center.

- 3. The proposed use will have no adverse effect on abutting property or the permitted use thereof;**

Analysis: The subject project is located in an existing shopping center that has been constructed since the 1990s. The location of new building adheres to all setback requirements of the Specific Plan and the use is not introducing any new traffic that was not previously contemplated with the prior use. No additional noise will be introduced to the site that will adversely affect abutting property.

4. The conditions stated in the decision are deemed necessary to protect the public health, safety, and general welfare.

Analysis: The proposed project was routed to various departments and agencies, and the Conditions of Approval were prepared based on the comments provided from each department and agency to ensure the protection of public health, safety, and general welfare.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

This project is consistent with the Addendum that was prepared and just adopted for the 1670 Mitchell Road Tentative Map (File No. 25-46 TPM). The Addendum was prepared to serve as an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse SCH # 1988053107) for the Mitchell Road Corridor Specific Plan Project (Original Project). This Addendum not only considered the current parcelization of the subject property, but future subdivision of the existing big box building for new uses, and potential additional smaller retailer users of the pad sites. The Take 5 Oil Change was contemplated in this Addendum as the Take 5 Oil Change was a potential project at the time the Tentative Map was under review. As demonstrated in the environmental analysis provided in Section 4 (Environmental Analysis) of the Addendum, the proposed changes did not meet the criteria for preparing a subsequent EIR or negative declaration. As such a Determination of consistency with the Addendum is appropriate because, as explained in Section 4, none of the conditions calling for preparation of a subsequent EIR or negative declaration occurred.

STAFF RECOMMENDATION

Staff recommends that the Planning Commission:

1. Determine that the project is consistent with the Addendum to the Mitchell Road Corridor Specific Plan Project Environmental Impact Report (SCH # 1988053107) pursuant to CEQA Guidelines Section 15162; and
3. Adopt a Resolution approving 25-31 Conditional Use Permit and 25-48 Site Plan Application for the Take 5 Oil Change located at 1670 Mitchell Road (APN: 069-011-009).

Attachments:

1. Attachment A – Draft PC Resolution 2026-XX
2. Attachment B – EIR Addendum – Ceres Mitchell Road Redevelopment Project

Reviewed by:

/s/Lea C. Simvoulakis
Lea C. Simvoulakis, Planning Consultant

2/9/26
Date

**DRAFT
RESOLUTION NO. PC 2026-XX**

**RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF CERES
DETERMING THAT THE PROJECT IS CONSISTENT WITH THE ADDENDUM
TO THE MITCHELL ROAD CORRIDOR SPECIFIC PLAN PROJECT
ENVIRONMENTAL IMPACT REPORT (SCH #1988053107) AND APPROVING
A CONDITIONAL USE PERMIT AND SITE PLAN APPLICATION FOR THE
TAKE 5 OIL CHANGE LOCATED AT 1670 MITCHELL ROAD, CERES CA
95307 (APN: 069-011-009)
FILE NOS. 25-31 CUP and 25-48 SPA**

WHEREAS, the Planning Commission held a duly noticed special public hearing on February 17, 2026, at 6:00 p.m. and considered a Conditional Use Permit and Site Plan Application for Take 5 Oil Change located at 1670 Mitchell Road; and

WHEREAS, the property affected by this resolution is located at 1670 Mitchell Road, Ceres, CA 95307, identified by Assessor's Parcel Number 069-011-009; and

WHEREAS, the Planning Commission based on the evidence within the staff report and project file find that:

1. The proposed project is consistent with the purpose and intent of the CC Community Commercial land use designation of the Ceres General Plan in that the CC Community Commercial designation supports the parcelization of large properties.
2. The proposed project is consistent with the purpose and intent of the CC Community Commercial land use designation as outlined in the Mitchell Road Corridor Specific Plan in that the project is a new neighborhood-service use that will provide an amenity to the community and will enhance the visual character of the corridor area.
3. The proposed project is consistent with the CC Community Commercial Zoning District in that the project conforms to all development standards and use requirements of the district.
4. The Commission is able to make all of the Conditional Use Permit findings in that the existing shopping center is of adequate size and shape for the proposed project and any traffic it generates; the project will have no adverse impact on the surrounding properties; and the project has been

conditioned to protect the public health, safety, and general welfare of the community.

5. The project is determined to be consistent with the Addendum prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. The Addendum is to the previously certified Environmental Impact Report (EIR) (State Clearinghouse SCH # 1988053107) for the Mitchell Road Corridor Specific Plan Project (Original Project). The previously adopted addendum contemplated the proposed use and determined that there would be no impacts beyond those previously studied under the EIR.
6. The conditions stated in the decision are deemed necessary to protect the public health, safety, and general welfare of the community.

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Ceres as follows:

Section 1. The Planning Commission hereby confirms that the project requires no further environmental review in that the project is consistent with the previously adopted Addendum and no new substantial changes are proposed in the project that require revision of the previous EIR or Addendum as the proposed project was contemplated in the environmental document; there are mitigation measures previously adopted to mitigate and reduce any impacts; and there have been no other substantial changes that merit a revision of the previously certified document.

Section 2. The operation of the proposed project requires compliance with the conditions of approval contained in this resolution and attached as **Exhibit "A"** on file in the Community Development Department, and the approved plan set attached as **Exhibit "B"** which shall be binding on all heirs, assignees, and successors in interest of said property.

Section 3. The Director of Community Development or designee is hereby directed to record this Resolution at the office of the County Recorder of the County of Stanislaus.

Section 4. The conditions of project approval set forth herein include certain fees, dedication requirements, reservation requirements, and other exactions. Pursuant to Government Code Section 66020 (d) (1), these conditions constitute written notice of a statement of the amount of such fees, and a description of the dedications, reservations, and other exactions. You are hereby further notified that the ninety (90) day appeal period in which you may protest these fees, dedications, reservations, and other exactions, pursuant to Government Code Section 66020 (a), has begun. If you fail to file a protest within

this ninety (90) day period complying with all of the requirements of Section 66020, you will be legally barred from later challenging such exaction.

I HEREBY CERTIFY that the foregoing Resolution was passed and adopted by the Planning Commission of the City of Ceres at a special public hearing held on the 17th day of February 2026, by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

GARY CONDIT, CHAIRPERSON
CITY OF CERES
PLANNING COMMISSION

ATTEST:

STEPHANI DAVIS
ADMINISTRATIVE SECRETARY
CITY OF CERES

ATTACHMENTS:

Exhibit A: Conditions of Approval
Exhibit B: Approved Plan Set

EXHIBIT A



City of Ceres Community Development Department Conditions Of Approval- Planning Commission

Approved by Planning Commission on: February 17, 2026

Project File Nos: 25-31 CUP Conditional Use Permit

25-48 SPA Site Plan Application

Project Name: Take 5 Oil Change

Project Address: 1670 Mitchell Road, Ceres, CA 95307

APN: 069-011-009

Property Owners: Vintage Properties, LP
3501 Coffee Road, Suite #4
Modesto, CA 95355

Project Applicant: Philip Spence
Durban Development
106 Foster Avenue
Charlotte, NC 28203

This list of conditions is not intended to be all-inclusive or a comprehensive list of City regulations. All conditions are referenced to the Take 5 Oil Change- Ceres project on file with the City of Ceres, Community Development Department, Planning Division.

City of Ceres Community Development Department: Planning Division

- Approval.** This Conditional Use Permit and Site Plan Approval is approved subject to these specific conditions. Any variation from the approved plans, proposals, supporting documents or presentations is subject to review and approval prior to implementation.
- Conformance to Plans.** This approval is dependent upon and limited to the proposals and plans contained, supporting documents submitted, and presentations made to staff as affirmed to by the applicant and presented on the plans. Any variation from these plans, proposals, supporting documents or presentations is subject to review and approval prior to implementation.

EXHIBIT A

3. **Expiration.** This approval shall expire two years from this date on **February 17, 2028**, unless altered and/or extended per State law. Prior to expiration, the owner/developer may apply for an extension, to be approved by the Community Development Director, not to exceed a total of two (2) years from the original date of expiration. **It shall be the responsibility of the applicant/property owner to be aware of the expiration of this entitlement and take the necessary steps to ensure the approval of this entitlement remains valid. It shall not be the City of Ceres' responsibility to notify the applicant/property owner of the project's expiration date. If an approved entitlement expires, the applicant/property owner shall be required to submit a new application request for this project.**
4. **Owner Obligation.** All conditions of approval shall be satisfied by the owner/developer. If at any time, any of the Conditions of Approval are found to be in non-compliance, City enforcement action may be implemented. Costs associated with compliance with the conditions shall be at the owner/developer's expense.
5. **Fees.** The developer shall pay all applicable processing fees, permit fees, City development fees, fire fees, school fees, drainage fees, habitat conservation fees and other public entity fees in effect at the time of the issuance of a building permit.
6. **Utility Companies.** The applicant is responsible for contacting all appropriate utility companies to obtain agreements for extension and/or relocation of services necessary for the proposed development.
7. **Signs.** No business identification signs or any advertising signs shall be installed or displayed without the review and approval of the Community Development Department, and all proposed advertising signs shall comply with the Sign Standards found in Chapter 26 of Title 18 of the Zoning Ordinance.
8. **Vested Rights.** This approval does not vest applicant's rights regarding future development. All ordinances, resolutions, rules, regulations and official policies governing design, improvement and construction standards and specifications applicable to the project and public improvements to be constructed by the Developer shall be those in force and effect at the time the applicable plan or permit approval is granted.
9. **Responsible Agencies.** The Developer is responsible for contacting all responsible and commenting agencies and ensuring compliance with any applicable fees and/or rules.
10. **Subsequent Development.** All activities undertaken in accordance with this approval shall comply with the City's General Plan and Municipal Code. In cases of conflict between the City's Municipal Code or map-specific conditions of approval, the governing priority shall be, to the extent legally permitted, as follows: 1) Municipal Code regulations, 2) project-specific conditions, and 3) standard conditions. The applicant shall comply with all regulations and code requirements of the Community Development Director, City Engineer, and Building Official, the Fire Chief, the Police Chief, and any other agencies requiring review of the project. If required, these agencies shall be supplied

EXHIBIT A

copies of the final maps, site plans, public improvement plans, grading plans, and building plans.

11. **Amendments.** The Planning Division may approve minor amendments to the project, provided that the amendment is still in substantial conformance with the original approval of this entitlement. A request for a minor amendment shall be submitted in writing to the Planning Division. Any request that is not deemed minor in nature must be presented to the Planning Commission through the amendment process and the applicant shall pay the necessary processing fees.
12. **Building Plans.** The project developer shall write all conditions of approval for this project on all building permit plan check sets submitted for review and approval. These conditions of approval shall be on, at all times, all grading and construction plans kept on the project site. It is the responsibility of the building developer to ensure that the project contractor is aware of, and abides by, all conditions of approval. Prior approval from the Director must be received before any changes are constituted in site design, grading, building design, building colors or materials, etc.
13. **Additional Information.** Upon Building Permit submittal, the applicant is required to show grading, access, paving, striping, storm drain system, frontage landscaping, fencing, sewer and water connections, and backflow devices to the satisfaction of the City Engineer. Civil drawings illustrating the required improvements for the project are required to be submitted with the building plan submittal. Any changes resulting from the review of these plans that cause substantial change to the site plan will require the project return to Planning Commission for a Site Plan Amendment.
14. **Easements.** As determined by the City, the applicant shall secure all necessary easements for reciprocal access, parking, landscaping and/or public utilities over the proposed property. Where required, location of all easements shall be agreed to with the City prior to the issuance of building permits. These easements shall be recorded prior to issuance of a Certificate of Occupancy.
15. **Failure to Comply.** Should the project be found, at any time, not to comply with any of the Conditions of Approval, or should the applicant construct or operate this development in any way other than as specified in the Application or Supporting documents or presentations to staff, the Planning Commission, or City Council, as modified by the Conditions of this Approval, then the terms of this Approval shall be considered to be violated.
16. **Indemnification.** The applicant shall indemnify and hold harmless the City, its council members and commissioners, officers, agents, employees, and representatives from liability for any award, damages, costs and fees, including without limitation attorneys' fees, incurred by the City and/or awarded to any plaintiff in any action related to or arising out of the City's approval of this project or subdivision Map or any environmental or other documentation related to this project. The applicant further agrees to provide a defense for the City in any such action.

EXHIBIT A

17. **Limits of Approval.** Approval of this application does not constitute approval of any other entitlement or any other necessary permit, license, or approval.
18. **Compliance with Local and State Laws.** The subject use shall be conducted in full compliance with all local and state laws. No part of this approval shall be construed to permit a violation of any part of the Ceres Municipal Code. This Site Plan Approval shall be subject to revocation if the subject use is conducted in such a manner as to cause a nuisance.
19. **Changes in Law.** This approval shall not preclude the application to development of the property of changes in City laws, regulations, plans or policies, the term of which are specifically mandated and required by changes in State or Federal laws or regulations.
20. **Other entitlements.** Approval of this application does not constitute approval of any other entitlement or any other necessary permit, license, or approval.
21. **Performance Standards.** Developer shall ensure ongoing compliance with City of Ceres Performance Standards, pursuant to Ceres Municipal Code Chapter 16, Title 18.
22. **Parking, Loading Areas, On-Site Circulation.** All on-site parking, loading areas, and on-site circulation shall conform to the approved plans dated October 30, 2025.
23. **Storage, Screening, and Fencing.** All storage, screening, and fencing shall conform to the approved plans and conform to the Ceres Municipal Code.
24. **Roof-mounted Equipment.** Applicant/developer shall ensure all roof-mounted equipment shall remain screened from public street view.
25. **Lighting.** All on-site lighting for parking areas, pedestrian areas, and vehicular or pedestrian paths of travel shall be LED lighting. A photometric plan showing the project complies with Chapter 25, Title 18 of the Ceres Municipal Code is required at building permit. Any proposed exterior lighting will need to be shielded and/or directed downward and away, so as not to create any glare onto the neighboring properties.
26. **Site Maintenance.** The site shall be permanently maintained in a neat and clean manner free of weeds, trash, and debris.
27. **CPTED.** Landscaping and lighting shall be maintained to compliance with Crime Prevention through Environmental Design (CPTED) guidelines. Shrubs shall be limited to two feet in height, and mature trees shall be limbed up to six feet above the ground. Dead or dying plants shall be replaced with materials of equal size and similar variety.
28. **Paving.** The City will require the applicant to construct the paving improvements (for parking area/access driveway) for the project as proposed on the Site Plan, which shall be completed to the satisfaction of the City Engineer and Building Official.
29. **Temporary Outdoor Storage.** During the construction phase, the applicant shall ensure temporary outdoor storage of items will be screened with a fence or wall, as required. Depending on use, fences or walls shall not exceed eight (8') feet in height including security measures, unless required for environmental mitigation or approval of the Planning Commission or City Council.

EXHIBIT A

30. **Paving.** The City will require the applicant to construct the paving improvements (for parking area/access driveway) for the project as proposed on the Site Plan, which shall be completed to the satisfaction of the City Engineer and Building Official.
31. **SJVAPCD.** The applicant shall comply with the requirements of the San Joaquin Valley Air Pollution Control District.

City of Ceres Community Development Department: Building Division

32. **Building Requirement.** The following proposal to construct this facility shall be required to be in compliance with the most current adopted California Code of Regulations Title 24 at the time of the building permit application submittal date.
33. **Circulation.** Any and all tenant space(s) with an occupancy category allowing public access shall have an exterior circulation path in accordance with Chapter 11B of Part 1 , Volume 2 of the most current adopted California Code of Regulations Title 24.
34. **Building Plans.** The building plans shall be drawn by a licensed engineer or architect.
35. **Demolition Permits.** The applicant shall secure the necessary demolition permit(s) from the City of Ceres to demolish all/any existing structures on site.
36. **Fire Code.** All construction and design shall meet or exceed the California Fire Code for fire and life safety requirements, including exiting and other life safety concerns.
37. **Addresses.** Building addresses for both buildings shall be posted and visible from the roadway in accordance with the California Fire Code and Ceres Municipal Code.
38. **Restrooms.** The applicant shall provide on-site restroom facilities during construction.
39. **Accessible Parking.** Accessible parking stalls shall have a dimension of 17' x 19' with the access area on the passenger side. ADA accessibility to the proposed building shall be required per State and Federal Law and to the approval of the Building official.
40. **During Construction.** The following conditions shall apply:
 - a. If any hazardous waste is encountered during the construction of this project, then all work shall be immediately stopped and the Stanislaus County Environmental Health Department, the Fire Department, the Police Department, and the City Inspector shall be notified immediately. Work shall not proceed until clearance has been issued by all of these agencies.
 - b. The developer shall keep adjoining public streets free and clean of project dirt, mud, materials, and debris during the construction period, as is found necessary by the City Engineer.
 - c. If grading is to take place between October 1 and May 1, both temporary and permanent erosion control plans shall be submitted for review and approval along with the grading plan. Permanent erosion control

EXHIBIT A

measures shall include hydro-seeding of all graded slopes within 60-days of completion of grading.

- d. Where soil or geologic conditions encountered in grading operations are different from that anticipated in the soil and/or geologic investigation report, or where such conditions warrant changes to the recommendations contained in the original soil investigation, a revised soil or geologic report shall be submitted for approval by the City Engineer.
- e. Sufficient surety guaranteeing the public improvements for a period of one year shall be provided.
- f. If there are substantial changes in the size, alignment, grades, etc. during construction, then original "as-built" plans on the standard size sheets will be certified by the Project Civil Engineer and returned to the City Engineering Services Department office.

City of Ceres Engineering Department

- 41. **General Compliance.** The applicant shall comply with all applicable requirements of the City of Ceres, including the Municipal Code, City Engineering Design Standards, the Subdivision Map Act, and all other applicable federal, state, and local regulations pertaining to parcel division and industrial development.
- 42. **Access and Circulation**
 - a. Legal and physical access shall be provided to each parcel. All access points will be reviewed and conditioned as part of future site development review.
 - b. Cross-access easements and internal circulation routes shall be established as needed to support multi-tenant or phased industrial development and shall be recorded prior to issuance of building permits.
 - c. Emergency vehicle access shall be reviewed and approved by the City Engineer and Fire Department during site plan review.
- 43. **Utility Services**
 - a. All water, sewer, storm drainage, and other utility connections shall be evaluated and installed at the time of site development.
 - b. Shared utility easements may be required if a single service laterals or joint utility corridors serve multiple parcels.
- 44. **Drainage and Stormwater Compliance**
 - a. All stormwater management and low-impact development (LID) features shall be incorporated during the site development process, in compliance with the City's stormwater regulations and State Water Resources Control Board requirements.
 - b. Drainage easements shall be recorded if off-site or shared drainage paths are proposed in the future development.
 - c. Physical frontage improvements, including pavement widening, curb and gutter, sidewalk, and landscaping, will be required at the time of

EXHIBIT A

development and shall not be deferred beyond the project-level entitlement.

45. **Fees and Improvement Securities.** All applicable development impact fees, utility connection fees, and securities for public improvements shall be collected and processed during the site development phase or at building permit issuance.
46. **Phasing and Project Coordination.** If development is phased, all conditions applicable to each phase, including frontage and utility improvements, shall be clearly addressed through site plan review and associated improvement plans.
47. **Civil Drawings.** Civil drawings illustrating required improvements (i.e. water and sewer services, a storm drain system, paved parking area, and commercial curb cuts) are required to be submitted with the building plan submittal for each building.
48. **City Standards.** The applicant shall install all required improvements (including parking area, driveway approaches, and curb, gutter, sidewalk) to the Standards of the Engineering Services Department. The applicant shall obtain approval of an Encroachment Permit through the Engineering Services Department prior to constructing these improvements.
 - a. The applicant shall install all required (including curb, gutter, and sidewalk, as well as meter and backflow devices) improvements to the Standards of the Engineering Services Department.
49. **Trash Enclosure.** The applicant shall provide trash enclosure(s) for the project that lock and integrate design elements of the main building. The City will expect trash enclosure(s) to be identified on the site plan with details of structure illustrated on project plans upon the submittal of a building permit application for the project to the Building Division. Additionally, recycling service shall also be provided.
50. **Grease Traps.** The applicant shall provide a standard food service grease trap for each building that has a kitchen/food prep area, which shall be approved by the County of Stanislaus Department of Environmental Resources. The applicant/future tenant(s) shall secure the necessary permits for the food service operations.
51. **Grading Plans.** The applicant will be required to submit a grading and drainage plan to the Engineering Services Department. The paving improvements proposed for the project shall be completed to City Standards. The applicant shall demonstrate that surface water runoff from the proposed building(s), including new paving and access/driveway areas, is conveyed to on-site stormwater retention facilities and that the facilities are adequately sized for the proposed changes.
52. **SWPPP.** The City needs a copy of a Stormwater Pollution Prevention Plan (SWPPP), prior to start of construction (grading). The applicant will be required to implement construction BMP's to prevent sediment from entering the street and the City's stormdrain inlets, which includes gravel drives at entrance/exit points, wattles, and stormdrain inlet filters, as well as keeping concrete wash, chemicals, etc. out of the stormdrain system. This shall be done to the

EXHIBIT A

satisfaction of the Engineering Services Department. Additional information on BMP's may be found at: www.cabmphandbooks.com.

53. **Storm Water.** The applicant shall comply with the State Water Regional Control Board Storm water Best Management Practices during construction. The project may require the developer to submit a Notice of Intent (NOI) with the State Water Resources Control Board, and obtain a Waste Discharge Identification Number (WDID). More information may be found at www.waterboards.ca.gov.
54. **Utility Easements.** Additional utility easements might be needed for sewer, water, and storm drainage.
55. **Pavement Design.** The applicant shall submit pavement design sections to the Engineering Services Department for review and approval prior to placement of pavement.
56. **Water Meter.** The applicant shall contact the Water Department to discuss whether or not the applicant would need to provide more than one (1) water meter for the project.
57. **Water Service.** The applicant shall contact the Water Department to discuss whether or not three (3) separate district water related service lines including fire service, domestic service, and irrigation service would be required.
58. **Sewer Capacity.** The project may be subject to providing the Engineering Services Department (209-538-5792) with water and sewer analysis to ensure the City has sufficient capacity to serve the development.
59. **Encroachment Permit.** Prior to any work being conducted within the City right-of-way and prior to issuance of building permits, the applicant shall obtain an Encroachment Permit from the City. Prior to issuance of the Encroachment Permit, the applicant shall submit to the City of Ceres for review and approval, improvement plans prepared by a registered Civil Engineer; shall pay plan check and inspection fees; shall provide a cost estimate of the improvements; and shall post sufficient surety guaranteeing the construction of the improvements.
60. **Traffic Control Plans.** The applicant shall provide traffic control plans, prepared by a registered civil engineer, for all proposed work within the City right-of-way.

City of Ceres Fire Department

61. **Uniform Fire Code.** All construction shall conform to the requirements of the City of Ceres Building Division and City of Ceres Ordinance 84-625, as amended pertaining to the Uniform Fire Code and Ordinance 80-546, as amended, pertaining to Automatic sprinklers.
62. **Fire Extinguishers.** Fire Extinguishers shall be provided in accordance with the California Fire Code. If cooking with grease, a CLASS K Fire Extinguisher is required.
63. **Knox Box.** The applicant shall provide a Knox type locking key box at entrance to each building for Fire Department access. Said Knox type locking key box shall be placed at a location as approved by the Ceres Fire

EXHIBIT A

Department. A Knox override switch shall be provided if the entrances into the site include gated access points. Identify Ceres Fire Department as the fire jurisdiction.

64. **Security Lighting.** The applicant shall provide on-site security lighting to the satisfaction of the Ceres Fire Department.
65. **Fire Inspections.** A minimum of 48-hour notice is required for inspections.
66. **Vertical Obstructions.** All vertical obstructions shall be higher than 13 feet 6 inches.
67. **Address Numbers.** Address numbers on the building shall clearly be visible from the street in contrasting color. Front and rear doors to have address numbers (or Suite numbers) 6" in size.
68. **All Weather Signage.** Permanent all weather signage (minimum 3-inch letters/numbers – red background with white letters – shall be installed at:
 - a. Main Electrical Disconnect
 - b. Main Gas Shutoff
 - c. Electrical Room

City of Ceres Police Department

69. **Public Peace, Safety, Morals.** The project will be required to conform to the provisions of the Public Peace, Safety, and Morals section of the Ceres Municipal Code.

TAKE 5 OIL CHANGE - CERES, CA

GENERAL NOTES

1. COORDINATE ALL DEVICE LOCATIONS AND MOUNTING HEIGHTS WITH THE ARCHITECT & EQUIPMENT VENDORS PRIOR TO INSTALLATION.
2. DEDICATED OUTLETS SHALL BE 120V, 15A, 120V.
3. CONVENIENCE RECEPTACLES SHALL BE 120V, 15A AND 120V.
4. LAMP AND JUNCTION BOXES WITH THE CORRESPONDING CIRCUIT BREAKERS, LABEL TO THE APPROPRIATE BACK LETTTERS ON WHITE CONDUIT AND SHALL BE PROVIDED WITH A GROUND.
5. INSULATED SCAFFOLDING STANCHIONS SHALL BE PROVIDED AND SHALL BE PROVIDED WITH A GROUND.
6. ALL RECEPTACLES BEARING 6'-0" OR ANGLED RECEPTACLES WITH THE CORRESPONDING CIRCUIT BREAKERS, LABEL TO THE APPROPRIATE BACK LETTTERS ON WHITE CONDUIT AND SHALL BE PROVIDED WITH A GROUND.
7. ALL LIGHT FIXTURES CONTAINING BATTERY PACKS OR BATTERY BACKUPS SHALL BE LOCATED IN THE ROOM/AREA PROVIDED IN THE INSIDE OF THE INSULATED BACK CONDUIT. THESE LIGHTING FIXTURES SHALL BE PROVIDED WITH A GROUND.
8. 6' (1.8M) DROPS FROM LIGHT FIXTURES TO GRAVEL OR 24' (7.3M) FOR SECURITY.
9. ALL FIXTURES FOR LIGHTS WHICH ARE SPOTTED TO BE MOUNTED IN THE SAME COUPLED PLATE, WHICH POSSIBLY COULD DAMAGE THE PLATE.
10. WALL MOUNTED INDUCTION SENSORS W/ SINGLE AMPERE SPAN TO BE LUMINARIES, DEDICATED OUTLET, OR APPROVED EQUIPMENT. WALL MOUNTED INDUCTION SENSORS SHALL BE PROVIDED WITH A GROUND. APPROVED EQUIPMENT, WHICH POSSIBLY COULD DAMAGE THE PLATE.
11. REINFORCED SPACES AREAS, AL. PLATES ABOVE 1" THICK, AND UP TO 18" HIGH SHALL BE PROVIDED AS CLASS 1 DIVISION 1 AREAS.
12. ALL FIXTURES DEFENDED AS SERVICE AREA, SMALL COMPANY W/ REC. ARTICLE 511. LUMINARIES, DEDICATED OUTLET, OR APPROVED EQUIPMENT. WHICH POSSIBLY COULD DAMAGE THE PLATE.
13. 120V, 15A OUTLET SHALL BE PLACED IN THE SERVICE BAY, 15' (4.57M) FROM THE REAR OF THE EQUIPMENT, WHICH POSSIBLY COULD DAMAGE THE PLATE.
14. DEDICATED OUTLETS FOR ALL EQUIPMENT, WHICH POSSIBLY COULD DAMAGE THE PLATE.
15. DEDICATED OUTLETS IN THIS AREA SHALL BE PROVIDED WITH A GROUND.
16. ALL RECEPTACLES SHALL BE PROVIDED WITH A GROUND.

1670 E MITCHELL ROAD
LIGHTING PLANS
Title: TAKE 5 OIL CHANGE
DURBAN DEVELOPMENT

No. Date Old App. C/P SUBMITTAL
P-02/25

FOR: TAKE 5 OIL CHANGE

Lighting

Welded

Structural

Architectural

Electrical

Plumbing

MEP

Landscaping

Soils

Geotechnical

Geological

Geohazard

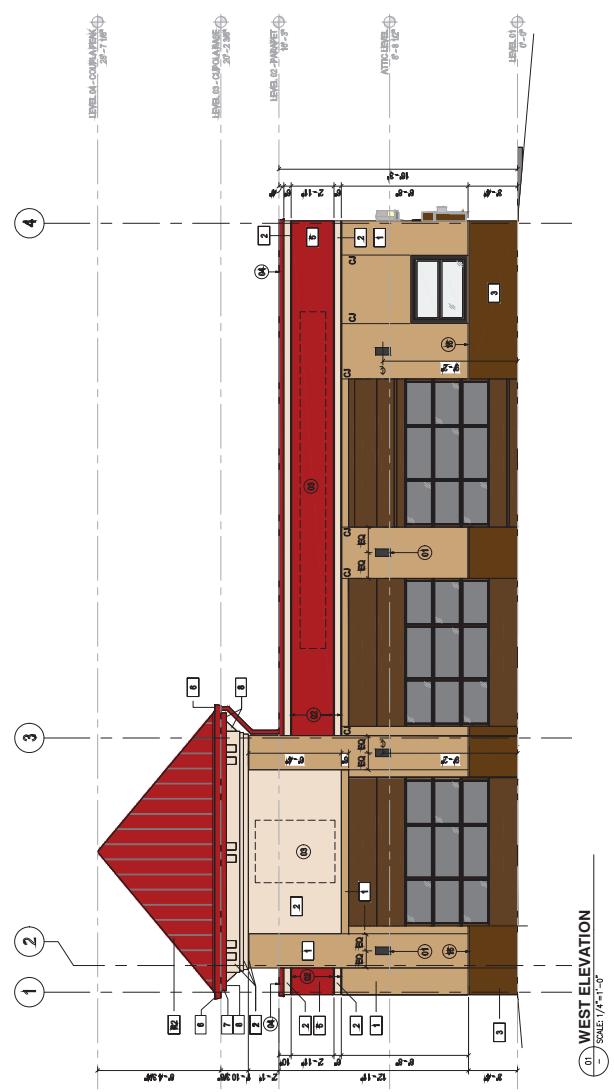
Geospatial



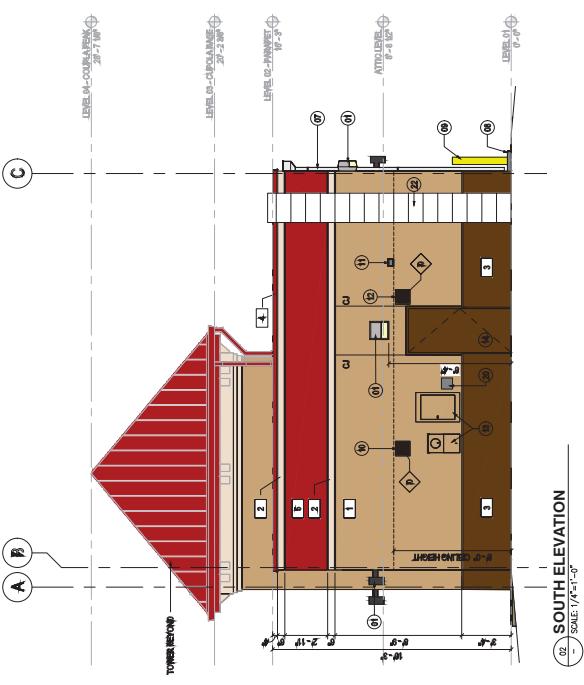
24155
Job Number
A21
Sheet
Title:

ITEM #6

TAKE 5 OIL CHANGE - CERES, CA



WEST ELEVATION



GENERAL NOTES	
NOTE: SURFACE TO BE PRIMED SEPARATELY	01. LIGHT/WHITE (RAL 9010) REFER TO ELECTRICAL DIVS.
EXTERIOR FINISH NOTES:	02. 1/4" DIA NAILS
1. EFS TO ST THRM CLASSIC, DRYV	03. ALL LOCATIONS PROPOSED ON CHANGES ON 3/25/22
2. INSULATION US MO OR APPROVED EQUAL	04. ALL SURFACE TO BE PRIMED SEPARATELY
3. EFS TO HAVE ULTRA-HIGH IMPACT ST 4x7	05. PRIMER/SEALANT OVERLAYS SUPER COLOR
4. TO 9' AFF	06. DRAINS (DRAINS) 12" DIA, DOWNSPOUT LEAD/HEAD COLOR.
5. METAL DOOR TO BE AFR DUTCH SEMI-OPEN	07. DRAINS BROWN
6. BEDROOMS TO BE DEEP SEE PAVILION AND 2'	08. 1/2 BRASH COLOR CONNECT TO STORMDAN
7. KYNAR 500, 20 MILS THICK FLUOROCARBON	09. 1/2" DIA PIPE POLARIC THERMAL COLOR TO MATCH
8. KYNAR 500, 20 MILS THICK FLUOROCARBON	10. EFS IN THRM ST KABIN/LOVER
9. KYNAR 500, 20 MILS THICK FLUOROCARBON	11. TOILET ROOM WALLS, CAP
10. KYNAR 500, 20 MILS THICK FLUOROCARBON	12. 1/2" IN. DRAINS COLOR TO MATCH FINISH
11. KYNAR 500, 20 MILS THICK FLUOROCARBON	13. ELECTRICAL METER PANEL, PAINT DARK BRONZE
12. EFS BONDDED AS NORTH, SOUTH, EAST, WEST BASED ON CONSTRUCTION PLAN ON SHEET A1.	14. PAN DOOR & FRAME TO SW 1007 - STURDY
	15. BROWN, BROWN, BROWN
	16. MOUNTED ABOVE FINISH TO LOC R LEVEL
	17. PERMANENT LADDER ACCES PROVIDED FOR MAINTENANCE
	AS PER SECTION 30.3.12 GAC 2022.

SHEET NOTES	
NOTE SURFACE TO BE PRIMED SEPARATELY	01. LIGHT/WHITE (RAL 9010) REFER TO ELECTRICAL DIVS.
EXTERIOR FINISH NOTES:	02. 1/4" DIA NAILS
1. EFS TO ST THRM CLASSIC, DRYV	03. ALL LOCATIONS PROPOSED ON CHANGES ON 3/25/22
2. INSULATION US MO OR APPROVED EQUAL	04. ALL SURFACE TO BE PRIMED SEPARATELY
3. EFS TO HAVE ULTRA-HIGH IMPACT ST 4x7	05. PRIMER/SEALANT OVERLAYS SUPER COLOR
4. TO 9' AFF	06. DRAINS (DRAINS) 12" DIA, DOWNSPOUT LEAD/HEAD COLOR.
5. METAL DOOR TO BE AFR DUTCH SEMI-OPEN	07. DRAINS BROWN
6. BEDROOMS TO BE DEEP SEE PAVILION AND 2'	08. 1/2 BRASH COLOR CONNECT TO STORMDAN
7. KYNAR 500, 20 MILS THICK FLUOROCARBON	09. 1/2" DIA PIPE POLARIC THERMAL COLOR TO MATCH
8. KYNAR 500, 20 MILS THICK FLUOROCARBON	10. EFS IN THRM ST KABIN/LOVER
9. KYNAR 500, 20 MILS THICK FLUOROCARBON	11. TOILET ROOM WALLS, CAP
10. KYNAR 500, 20 MILS THICK FLUOROCARBON	12. 1/2" IN. DRAINS COLOR TO MATCH FINISH
11. KYNAR 500, 20 MILS THICK FLUOROCARBON	13. ELECTRICAL METER PANEL, PAINT DARK BRONZE
12. EFS BONDDED AS NORTH, SOUTH, EAST, WEST BASED ON CONSTRUCTION PLAN ON SHEET A1.	14. PAN DOOR & FRAME TO SW 1007 - STURDY
	15. BROWN, BROWN, BROWN
	16. MOUNTED ABOVE FINISH TO LOC R LEVEL
	17. PERMANENT LADDER ACCES PROVIDED FOR MAINTENANCE
	AS PER SECTION 30.3.12 GAC 2022.

TAKE 5 OIL CHANGE	
FOR:	DURBAN DEVELOPMENT
DESIGNED BY:	Berghausen Engineers, Inc.
DRIVEN BY:	West 72nd Avenue South
APPROVED BY:	45251622 Berghausen.com
REVISION:	06/06/22
DATE:	7/25/22
NO.:	24155
Sheet:	2421
Title:	
1670 E MITCHELL ROAD	
CERES, CA	
1/4" SUBMITAL	
P-02/25	
No. Date Rev. Appr.	

TAKE 5 OIL CHANGE - CERES, CA

This architectural cross-section diagram illustrates a building's internal structure and exterior. The building is divided into several levels and rooms, with various materials and features indicated. Key elements include:

- Exterior:** The building has a red brick base and a brown upper section. A yellow horizontal band runs across the middle of the brown section.
- Rooms:** The diagram shows multiple rooms, some with windows and doors. A central room on the right has a double door labeled "2-11".
- Staircases:** A red staircase is located on the left side, and a brown staircase is on the right side.
- Levels:** The building features several levels, with labels such as "LEVEL 01 - GROUND FLOOR", "LEVEL 02 - MEZZANINE", and "LEVEL 03 - ROOF".
- Dimensions:** Horizontal dimensions are marked as "20'-0"" and "20'-2 1/2"". Vertical dimensions include "10'-0"" and "10'-6"".
- Structural Components:** The diagram shows various structural elements like beams, columns, and joists.
- Annotations:** Numbered circles (1 through 4) point to specific features: 1 points to a red brick wall; 2 points to a red staircase; 3 points to a brown wall; 4 points to a brown staircase.

03 EAST ELEVATION
- SCALE: 1/4" = 1'-0"

NORTH ELEVATION 04 - SCALE: 1/4" = 1'-0"

ITEM #6

TAKE 5 OIL CHANGE - CERES, CA

EXTERIOR ELEVATIONS
1670 E METCHELL ROAD
CERES, CA

Designers and Architects Bergshamen Engineers, Inc.
1025 72nd Avenue South
Seattle, WA 98102
206.725.18822
bergshamen.com

Contractor BBLI
1025 72nd Avenue South
Seattle, WA 98102
206.725.18822
bergshamen.com

ITEM #6

TAKE 5 OIL CHANGE
DURBAN DEVELOPMENT

FOR: Date: 02/25/23
No. Date: 02/25/23
Cpl. Date: 02/25/23
Revised:

Sheet: Drawing No. 06/16/23
Title: TAKE 5 OIL CHANGE

EXTERIOR FINISH SCHEDULE

NO.	AREA	COATS	FINISH
1	EFS MAIN COLOR	-	MATCH SW COLOR #7983, STONEBRICK FINISH TO MATCH TO MEDIUM SAND OR DRYVIT "SANDBEVEL FINE"
2	EFS ACCENT BAND	-	MATCH SW COLOR #7978, COTTAGE CREAM FINISH TO MATCH TO FINE SAND OR DRYVIT "SANDBLAST"
3	EFS WANSOT	-	MATCH SW COLOR #8987, STARDUST FINISH TO MATCH TO MEDIUM SAND OR DRYVIT "SANDBEVEL"
4	COPING CORNICE	-	MATCH SW COLOR #8981, POSITIVE RED FINISH TO BE KYW 900 REFINISHED
5	EFS SECONDARY COLOR	-	MATCH SW COLOR #8971, POSITIVE RED FINISH TO MATCH SW COLOR #7978, COTTAGE CREAM
6	TOWER, ROOF AND GUTTER	-	MATCH SW COLOR #8971, POSITIVE RED
7	PRE-INSULATED STANDING SEAM	-	MATCH SW COLOR #8971, POSITIVE RED
8	TOWER - VASIA PRE-PRIMED LP SMARTSIDE TIN	2	MATCH SW COLOR #8971, POSITIVE RED
	TOWER - DURBAN PRE-PRIMED LP SMARTSIDE VENTED SOFTIT	2	MATCH SW COLOR #7978, COTTAGE CREAM

GENERAL NOTES

NOTE: SOURCE TO BE PERMITTED SEPARATELY
EXTERIOR INSUL. NOTES

1. EFS TO ECO-TERMIC CLASSIC, DRYVIT OUTULUX OR PLUSMO OR APPROVED EQUAL, ALL BOROUGH TO BE PERMITTED SEPARATELY
2. EFS TO AVAIL TRA-HOR IMPACT TEST TO 47" AFF EFS TO AVAIL TRA-HOR IMPACT TEST TO 47" AFF
3. EFS TO AVAIL TRA-HOR IMPACT TEST TO 47" AFF
4. EFS TO AVAIL STANDING SEAM OR APPROVED EQUAL
5. METAL ROOF TO ECO-DURSEAM OR APPROVED EQUAL, WITH DECO PANELS AND 2" METAL ROOF PANELS TO BE 24 GA. GALV WITH KYW 900, 20 MIL THICK FLUORCARBON
6. METAL ROOF TO ECO-DURSEAM OR APPROVED EQUAL, WITH DECO PANELS AND 2" METAL ROOF PANELS TO BE 24 GA. GALV WITH KYW 900, 20 MIL THICK FLUORCARBON
7. METAL COPING TO BE PRE-FINISHED, 24 GA. GALV WITH KYW 900, 20 MIL THICK FLUORCARBON
8. ENSEIGNS DEVOIDED IN NORTH, SOUTH, EAST, WEST BASED ON CONSTRUCTION PLAN ON SHEET A.1.1.
9. PERMANENT UNDER ACCESS PROVIDED FOR MAINTENANCE AS PER SECTION 304.3.1.2. GAC 2022

SHEET NOTES

- 01 LIGHTING ETC. (TYPICAL) REFER TO ELECTRICAL DIS.
- 02 1" CANT NAILS
- 03 1" V-NAILS
- 04 LOCATION OF PROPOSED CHANGE SIGN (32' E.F.)
- 05 ALL BOROUGH TO BE PERMITTED SEPARATELY
- 06 PRE-FINISHED GALV COPING TO MATCH FINISH
- 07 PRE-FINISHED METAL OVERFLOW SCOPER COLOR, COLOR BRONZE
- 08 PRE-FINISHED METAL DOWNSPOUT & LEAFHEAD COLOR, COLOR BRONZE
- 09 PRE-FINISHED METAL DOWNSPOUT & LEAFHEAD COLOR, COLOR BRONZE
- 10 PRE-FINISHED METAL DOWNSPOUT & LEAFHEAD COLOR, COLOR BRONZE
- 11 TOILET ROOM WALL CAPS
- 12 TOILET ROOM WALL CAPS
- 13 ELECTRICAL METER PAN, PANT HARBORNE
- 14 PAINT DOORS & FRAME TO ECO SW #607 - STURDY BROWN
- 15 T-COVEL REVEAL IN S. E. DESIGN, MODEL: 8001
- 16 MOULDING TO HAVE 1/4" IN. PROTRUSION
- 17 MOULDING TO HAVE 1/4" IN. PROTRUSION
- 18 MOULDING TO HAVE 1/4" IN. PROTRUSION
- 19 MOULDING TO HAVE 1/4" IN. PROTRUSION
- 20 MOULDING TO HAVE 1/4" IN. PROTRUSION
- 21 MOULDING TO HAVE 1/4" IN. PROTRUSION
- 22 PERMANENT UNDER ACCESS PROVIDED FOR MAINTENANCE AS PER SECTION 304.3.1.2. GAC 2022

EAST ELEVATION
SCALE: 1/4" = 1'-0"

NORTH ELEVATION
SCALE: 1/4" = 1'-0"

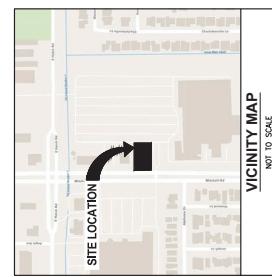
TAKE 5 OIL CHANGE – CERES, CA PRELIMINARY SITE PLAN

Know what's below.
Call before you dig.
Dial 811



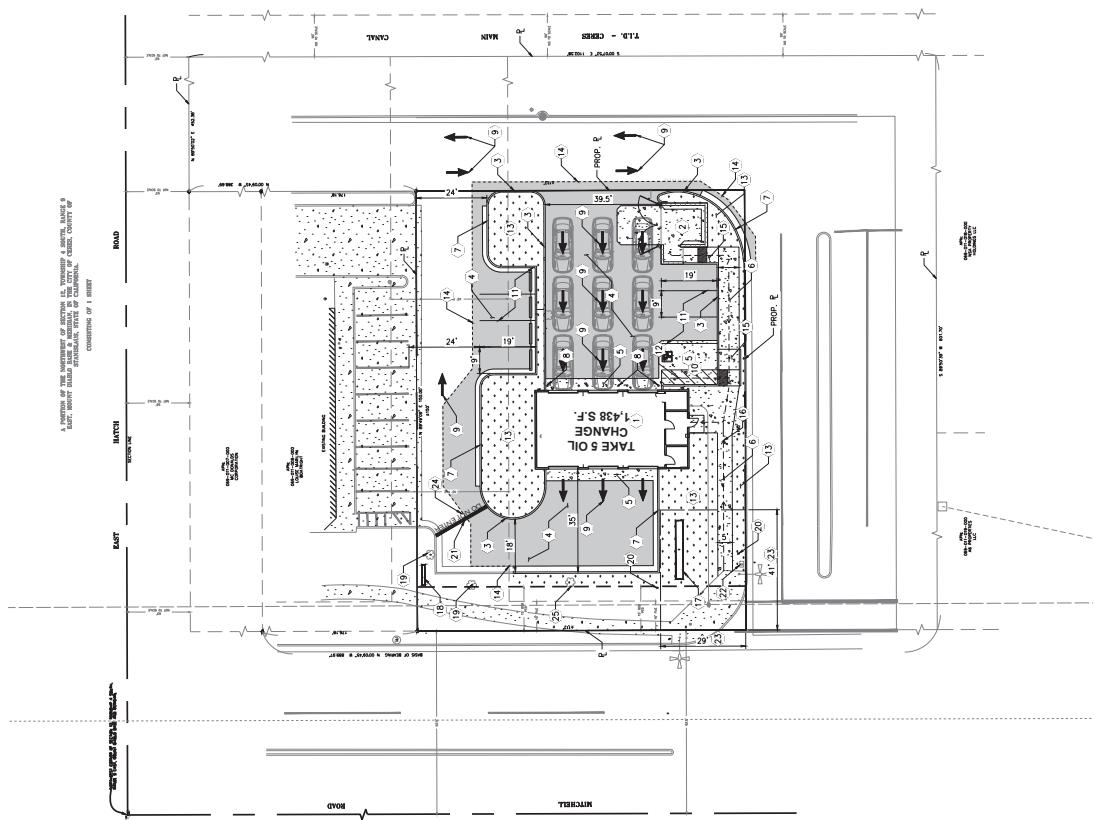
1670 MITCHELL ROAD
CERES, CA

PRELIMINARY SITE PLAN
TAKE 5 OIL CHANGE



CONSTRUCTION NOTES:

1. PROPOSED TAKE-5 OIL CHANGE BUILDING. SEE ARCHITECTURAL PLANS FOR DETAILS.
2. PROPOSED LOCATION OF TRASH ENCLOSURE WITH CONCRETE PAD BUILT TO CITY OF TULARE STANDARDS. REFER TO ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
3. PROPOSED CONCRETE BARRIER CURB.
4. PROPOSED ASPHALT PAVING.
5. PROPOSED CONCRETE PAVING.
6. PROPOSED CONCRETE SIGNS.
7. PROPOSED CONCRETE CURB AND GUTTER.
8. PROPOSED GUARD POST, TYPICAL.
9. PROPOSED DIRECTIONAL PAVEMENT MARKINGS, TYPICAL.
10. PROPOSED VAN ACCESSIBLE PARKING STALL, TYPICAL AND SIGNAGE.
11. PROPOSED EADING STALL, STREETS, TYPICAL, ALL STRIPPING TO CONFORM WITH AUTHORITY HAVING JURISDICTION REQUIREMENTS.
12. PROPOSED 45 DEGREE STRIPPING AT MAX ON CENTER CONFORMING TO CITY REQUIREMENTS.
13. PROPOSED LANDSCAPING AREA. REFER TO LANDSCAPE PLANS FOR ADDITIONAL INFORMATION.
14. PROPOSED NEAR SAW CUT LINE.
15. PROPOSED VAN ACCESSIBLE CURB RAMP WITH DETECTABLE WARNING STRIP.
16. PROPOSED ACCESSIBLE PATH.
17. EXISTING MONUMENT SIGN TO REMAIN AND BE PROTECTED.
18. PROPOSED MONUMENT SIGN.
19. EXISTING TREE TO REMAIN AND BE PROTECTED.
20. EXISTING SIGN TO REMAIN AND BE PROTECTED.
21. PROPOSED STOP BAR TO CITY STANDARDS.
22. EXISTING DRY UTILITIES AND AMPLITUDES TO REMAIN AND BE PROTECTED.
23. PROPOSED 20' x 4' SOURCE ELEMENT.
24. PROPOSED 20' x 4' SOURCE ELEMENT.
25. PROPOSED 20' x 4' SOURCE ELEMENT.



TAKE 5 OIL CHANGE

PRELIMINARY SITE PLAN

TAKE 5 OIL CHANGE

1670 MITCHELL ROAD
CERES, CA

TAKE 5 OIL CHANGE

</div

**TAKE 5 OIL CHANGE - CERES, CA
PRELIMINARY GRADING AND DRAINAGE PLAN**

Know what's **below**.
Call before you dig.
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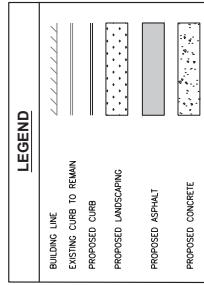
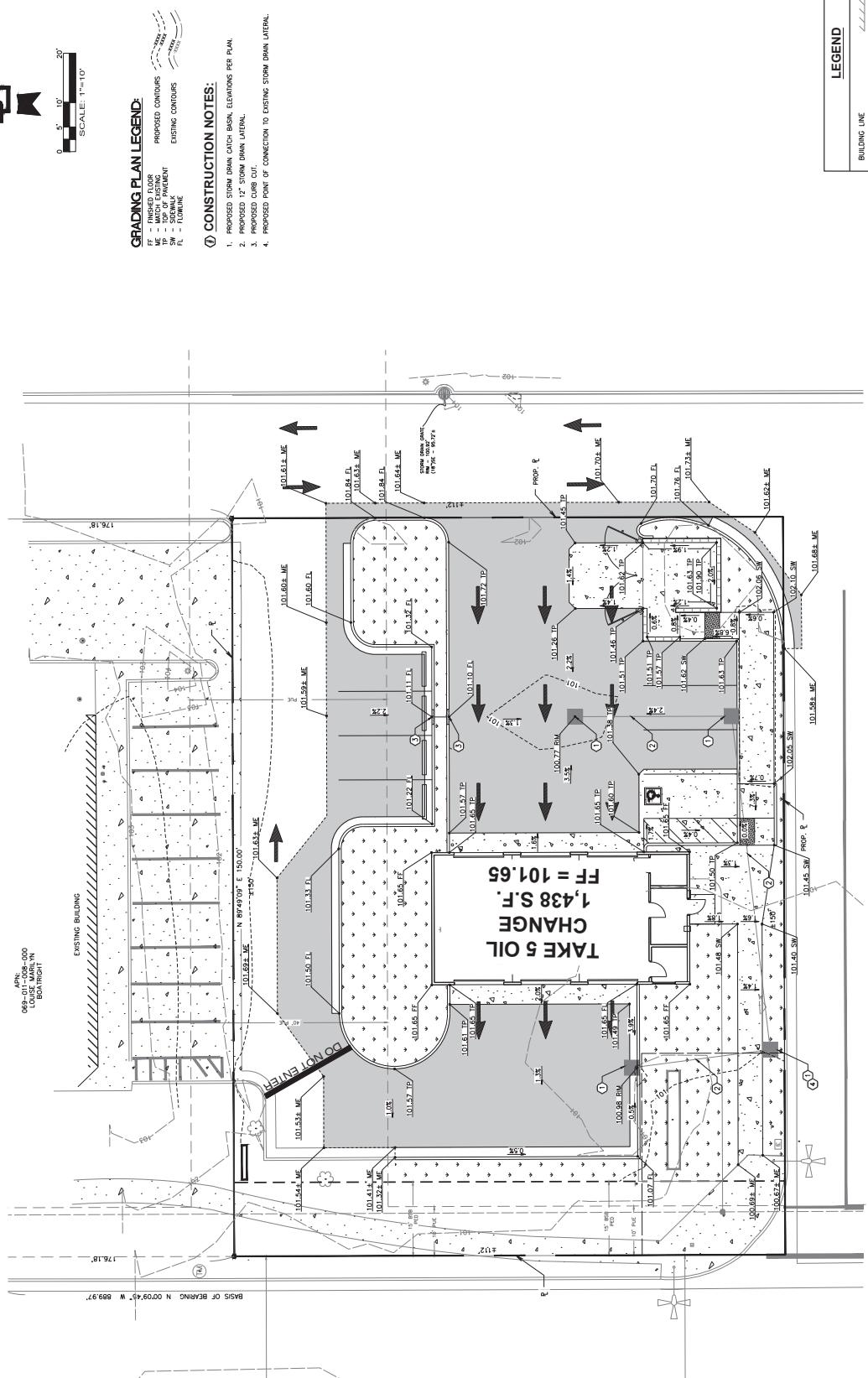
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5
60

DURBAN DRAFT LOCAL PLAN 2030

1 OF 2
Sheet
24155

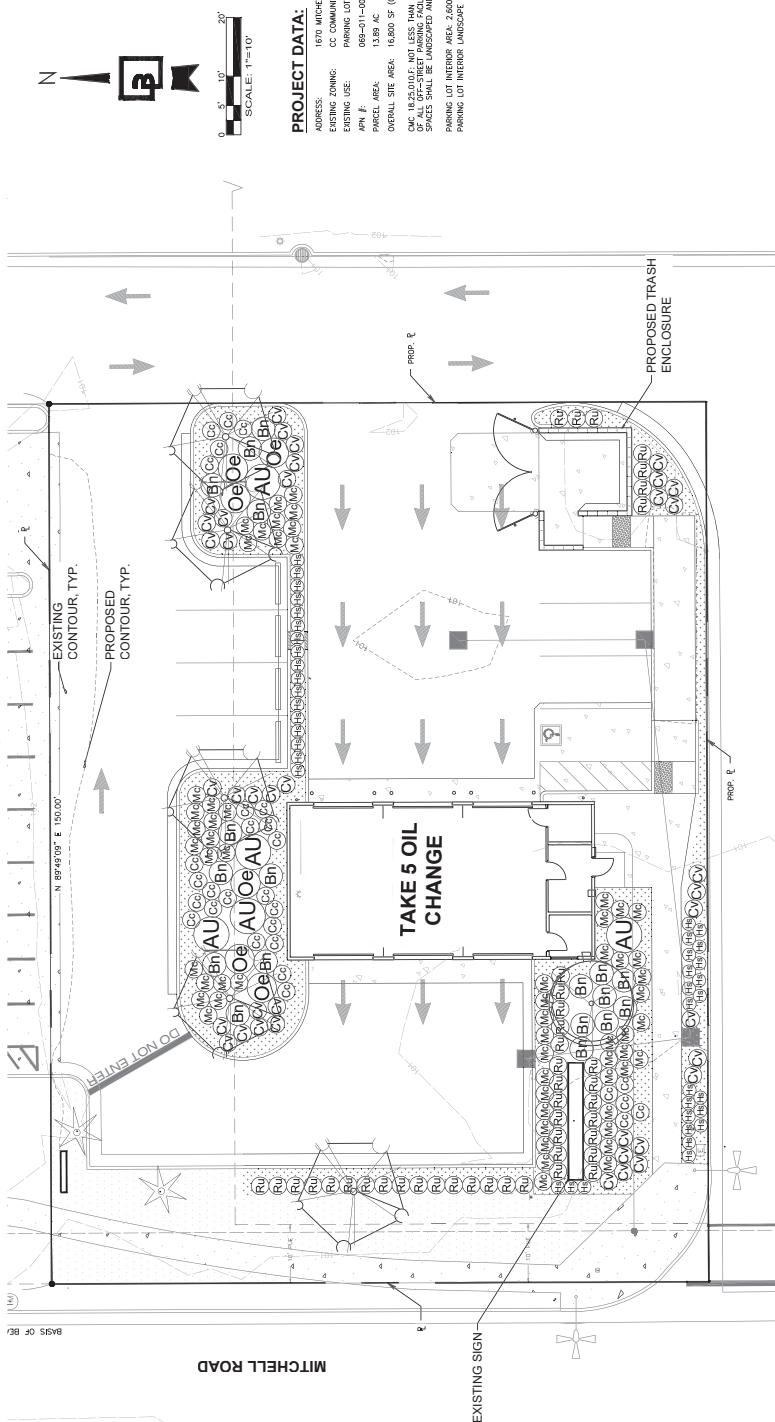
ITEM #6



3



TAKE 5 OIL CHANGE - CERES, CA PRELIMINARY LANDSCAPE PLAN



Call before you dig.
Dial 811



1670 MITCHELL ROAD
PRELIMINARY LANDSCAPE PLAN
CERES, CA

TAKE 5 OIL CHANGE
DURBAN DEVELOPMENT

For:



City of Ceres
Planning Department
1670 Mitchell Road
Ceres, CA 95307
Phone: (209) 724-8000
Fax: (209) 724-8022
Email: Planning@CeresCA.gov

Project Name: TAKE 5 OIL CHANGE
Project Address: 1670 MITCHELL ROAD, CERES, CA 95307
Project Type: COMMERCIAL
Project Description: COMMERCIAL
Project Status: Pending
Project Contact: (209) 724-8000
Project Email: Planning@CeresCA.gov

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24155

Sheet

OP 2



24155

Sheet

OP 2

ITEM #6

EIR ADDENDUM

FOR THE

CERES MITCHELL ROAD REDEVELOPMENT PROJECT

NOVEMBER 11, 2025

Prepared for:

City of Ceres
Planning Division
2220 Magnolia Street
Ceres, CA 95307
(209) 538-5774

Prepared by:

De Novo Planning Group
1020 Suncast Lane, Suite 106
El Dorado Hills, CA 95762
(916) 580-9818

D e N o v o P l a n n i n g G r o u p

A Land Use Planning, Design, and Environmental Firm

ATTACHMENT B

EIR ADDENDUM

FOR THE

CERES MITCHELL ROAD REDEVELOPMENT PROJECT

NOVEMBER 11, 2025

Prepared for:

City of Ceres
Planning Division
2220 Magnolia Street
Ceres, CA 95307
(209) 538-5774

Prepared by:

De Novo Planning Group
1020 Suncast Lane, Suite 106
El Dorado Hills, CA 95762
(916) 580-9818

EIR ADDENDUM

1 INTRODUCTION

This Addendum was prepared in accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines. This document has been prepared to serve as an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse [SCH] # 1988053107) for the Mitchel Road Corridor Specific Plan Project (Original Project). The City of Ceres is the lead agency for the proposed amendments to the Original Project, which consists of repurposing an existing 130,373 sf building that was previously operated as a Walmart with new retail uses, and building 17,396 square feet of new retail space for a general retail store, two quick serve restaurants, and a Dutch Bros beverage shop on a 13.4 acre site within the Specific Plan area. The new buildings are intended to maximize the utilization of the site.

For the City to consider an amendment to the Original Project, the City must ensure that, if needed, environmental review consistent with the requirements of the California Environmental Quality Act (CEQA) and the State CEQA Guidelines has been completed. Because the City has previously complied with CEQA for the Original Project and the new discretionary action before the City would be a change in an already-approved project, the City can rely on information in the certified EIR previously prepared for the Original Project, to the extent they remain adequate. Consistent with the requirements of CEQA Guidelines Section 15162, the City must, therefore, determine whether any changed circumstances or “new information of substantial importance” will trigger the need for a subsequent EIR.

As described in CEQA Guidelines Section 15164, a lead agency shall prepare an addendum to a previously adopted EIR if some changes or additions to an EIR certified for a project are necessary, but none of the of conditions identified in CEQA Guidelines Section 15162 have occurred. No subsequent EIR shall be prepared for that project unless the lead agency determines, based on substantial evidence in the light of the whole record, one or more of the following conditions to be applicable:

- (1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- (2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
 - (A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;

EIR ADDENDUM

- (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
- (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
- (D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

If any of the triggers set forth above occurs, the City would be required to prepare a subsequent EIR, unless “only minor additions or changes would be necessary to make the previous EIR adequately apply to the project in the changed situation,” in which case a “supplement to an EIR” would suffice (see CEQA Guidelines, Section 15163). If there are no grounds for either a subsequent EIR or a supplement to an EIR, then the City must prepare an addendum pursuant to CEQA Guidelines Section 15164, explaining why “some changes or additions” to the EIR “are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.”

As demonstrated in the environmental analysis provided in Section 4 (Environmental Analysis), the proposed changes do not meet the criteria for preparing a subsequent EIR or negative declaration. An addendum is appropriate here because, as explained in Section 4, none of the conditions calling for preparation of a subsequent EIR or negative declaration have occurred.

Information and technical analyses from the EIR are utilized throughout this Addendum. Relevant passages from this document (consisting of the EIR) are cited and available for review at:

City of Ceres
2220 Magnolia Street
Ceres, CA 95307
Phone: 209-538-5700

2 PROJECT BACKGROUND AND PURPOSE OF THE EIR ADDENDUM

The Mitchel Road Corridor Specific Plan Project was finalized and approved by the City Council in 1989. Since certification of the EIR, much of the 436.7-acre area has developed with a mix of uses. This includes a 13.4-acre parcel located at the southeast corner of the intersection of Mitchell Road and Hatch Road. The parcel is APN 039-068-009 and is within Planning Area 2 as identified in the Mitchell Road Corridor Specific Plan. The 13.4-acre parcel was built out with a Walmart and other

retail operations; however, Walmart has since closed and new retail operations, including new buildings, are proposed. The following describes the original project.

ORIGINAL PROJECT

The Original Project consisted of adoption and implementation of the Mitchell Road Corridor Specific Plan, which included the development of 436.70 acres as described below.

LOCATION AND BOUNDARIES

The Mitchell Road Corridor study area was located on the eastern edge of the City of Ceres. Ceres is located adjacent to and south of the City of Modesto in Stanislaus County. The Mitchell Road Corridor includes a 2 1/2-mile stretch of Mitchell Road extending between Highway 99 on the south and the Tuolumne River on the north. The corridor is approximately 1/4 mile wide for most of its length but broadens to approximately half mile wide at the northern and southern ends to include large parcels in those areas.

The corridor was within the City jurisdiction except for several parcels in the north and south end which were within the unincorporated area of the County of Stanislaus. Only a small percentage of the corridor was currently developed at the time of approval, with existing commercial uses concentrated predominantly between Fowler and Roeding and at the Hatch/Mitchell and Don Pedro/Mitchell intersections.

STATEMENT OF OBJECTIVES

The Mitchell Road Corridor Specific Plan was intended to achieve the goals and objectives of the City of Ceres through the cost-effective physical and economic development of the corridor. The City intended for the Mitchell Road Corridor Specific Plan to:

1. Provide a means of controlling the direction of growth within the Mitchell Road Corridor.
2. Capitalize on and maximize the economic potential of the Mitchell Road Corridor so that the area can be viewed as an economic asset to the City.
3. Provide a land use scenario that is sensitive to existing and future residential uses in the corridor vicinity.
4. Provide guidelines which will suggest a desired aesthetic quality for the corridor area and incorporate existing and new urban design concepts.
5. Provide a circulation concept that utilizes the road-widening program and maximizes land use potential while minimizing traffic conflicts.
6. Create an image for the City that is distinctive and inviting.
7. Encourage public transit and pedestrian circulation.
8. Provide a varied and interesting building height profile as is suitable.
9. Provide for the short-term, as well as the long-term, needs of the corridor in a comprehensive planning approach.
10. Provide a plan that is sensitive to the noise attenuation needs of the corridor.
11. Provide an environment that is safe for the public at large.

EIR ADDENDUM

PROJECT CHARACTERISTICS

The Mitchell Road Specific Plan involved the intensification of the Mitchell Road Corridor from predominantly vacant and underutilized uses to a planned commercial strip. The table below details the types and quantity of the proposed land uses for the corridor based on maximum buildout of the Specific Plan.

Land Use	Acreage	Square footage
Neighborhood Commercial	52.89	691,200
Planned Commercial	138.45	1,809,300
Highway Commercial	28.86	377,100
Retail Commercial	61.62	805,300
Business Park	105.40	1,377,400
Residential	49.48	282 units
Church	4.44	1
	436.70	

The approach to the land use concept included:

- protecting existing viable residential neighborhoods;
- providing for a variety of commercial opportunities;
- enhancing and capitalizing on the key visual features within the corridor and surrounding area;
- preserving existing significant trees; and
- minimizing traffic conflicts along Mitchell Road.

The Corridor had been designated with a range of commercial land uses. In general terms, the Corridor can be divided into three distinct Planning Areas.

- Planning Area 1 is located between Roeding Road and Highway 99, is visualized as an intense commercial center to serve the City and regional needs.
- Planning Area 2, located between Hatch and Roeding Roads, is intended to serve as a local and regional target for residents to the east and west.
- Planning Area 3, between the Tuolumne River and Hatch Road, is intended to draw users and occupants from the airport and Modesto area.

APPROVALS

As part of the Original Project, the City undertook the following actions:

- Certified the Mitchell Road Corridor Specific Plan EIR;
- Adopted the Mitigation Monitoring and Reporting Program for the project; and

- Approved the Mitchell Road Corridor Specific Plan.

3. PROPOSED PROJECT

Project Site

Modifications to the Original Project are limited to an approximately 13.9-acre site located in the approved 436.7-acre Mitchell Road Corridor Specific Plan area (Modified Project site). The Modified Project site is located at the southeast corner of the intersection of Mitchell Road and Hatch Road and is APN 039-068-009. The Modified Project site is in Planning Area 2 as identified in the Mitchell Road Corridor Specific Plan.

Surrounding Uses

The surrounding area is characterized by a mix of commercial, retail, and service-oriented uses along the Mitchell Road and Hatch Road corridors. At the corner of Mitchell Road and Hatch Road, is APN 039-068-007 which contains a 3,524-square-foot (sf) McDonald's restaurant. Additionally, on APN 039-068-008 is a 3,763-sf car stereo store. Neither of these parcels are part of the Modified Project site, but are immediately adjacent and connected to the Modified Project site.

There are residential neighborhoods located to the east and west.

Existing Uses

Walmart previously occupied the existing 130,373 sf building and associated surface parking areas on-site; however, Walmart has since relocated, leaving the existing building vacant. The building has remained unoccupied since 2021.

Characteristics of the Modified Project

The Modified Project consists of repurposing the existing 130,373 sf building that was previously operated as a Walmart with the following new retail uses:

- 60,478 sf supermarket, likely to be occupied by Vallarta Supermarkets;
- 27,094 sf general retail store, likely to be occupied by Ross Dress for Less;
- 23,258 sf general retail store (tenant currently unknown);
- 9,147 sf general retail store, likely to be occupied by Five Below;
- 5,129 sf auto parts store (tenant currently unknown).

In addition, the Modified Project proposes the construction of the following new buildings on the site:

- 7,470 sf general retail store (tenant currently unknown);
- 4,137 sf (50-seat) fast-food restaurant with drive-through (tenant currently unknown);
- 3,515 sf (50-seat) fast-food restaurant with drive-through (tenant currently unknown);
- 1,288 sf quick oil change center, likely to be occupied by Take 5 Oil Change;

EIR ADDENDUM

- 986 sf (outdoor 12-seat) Dutch Bros specialty beverage shop with drive-through.

The project also proposes expanding the parking capacity to 684 stalls across the entire site. Site access would continue to be provided via the following four driveways:

- East Hatch Road driveway (full access);
- Mitchell Road northern driveway (right-turn in, right-turn out only access);
- Mitchell Road central driveway (full access);
- Mitchell Road southern driveway (full access).

No improvements to the existing driveways or street frontages along Mitchell Road and E. Hatch Road are proposed. The project site vicinity map is shown on Figure 1; the project site plan is provided in Figure 2.

4 ENVIRONMENTAL ANALYSIS

Analysis of Impacts Found to be Less Than Significant

Specific impacts within the following categories of environmental effects were found to be less than significant as set forth in more detail in the certified EIR. Following each bullet is a conclusion on whether the proposed revision changes the impact conclusion found in the original EIR.

Section 3.1: Soils and Geology

The modified project, like the original project, has the potential to directly or indirectly expose residents and property structures to mild ground shaking during seismic occurrences. The potential for the soil and geology impacts associated with the original project are discussed in section 3.1.1 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.1.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require all new structure associated with the modified project to be built in accordance with City Code, Older, existing structures which will remain should be modified to incorporate the latest building and safety standards. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.2: Air Quality

The modified project, like the original project, has the potential to directly or indirectly create air quality impacts. The potential for the impacts associated with the original project are discussed in section 3.2.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.2.3 was

INTRODUCTION

adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require air quality reduction measures for the modified project. This includes compliance with all rules and regulations by the San Joaquin Valley Air Pollution Control District. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.3: Hydrology

The modified project, like the original project, has the potential to directly or indirectly create drainage/flooding impacts, and/or water quality impacts. The potential for the impacts associated with the original project are discussed in section 3.3.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.3.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require a storm drainage plan that handles drainage/flooding impacts as well as water quality impacts associated with the modified project. The project modifications do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.4: Noise

The modified project, like the original project, has the potential to directly or indirectly create noise impacts from traffic, airports, and stationary sources. The potential for the impacts associated with the original project are discussed in section 3.4.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.4.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require stationary sources of noise to be reduced from compliance with City standards and best practices designed into the buildings of the modified project. The project modifications do not create significant changes to the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.5: Aesthetics & Light and Glare

The modified project, like the original project, has the potential to affect visual character, light, and glare. The potential for the impacts associated with the original project are discussed in section 3.5.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.5.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program

EIR ADDENDUM

to require design considerations, including lighting and setbacks in the commercial development of the modified project. The project modifications do not cause significant changes to the visual appearance of the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.6: Land Use

The modified project, like the original project, has the potential to affect land use. The potential for the impacts associated with the original project are discussed in section 3.6.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.6.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require setback requirements, consistency with the Airport Land Use Plan, and review for incompatible uses related to the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.7: Population & Housing

The modified project, like the original project, has the potential to affect population and housing. The potential for the impacts associated with the original project are discussed in section 3.7.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Mitigation Measure 3.7.3 was adopted, but is not relevant to the modified project. The project modifications do not cause significant changes to the land uses that would affect population or housing on the developed site or surrounding, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.8: Transportation and Circulation

The modified project, like the original project, has the potential to affect transportation and circulation. The potential for the impacts associated with the original project are discussed in section 3.8.2 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measure 3.8.3 was adopted and will remain applicable to the modified project. This mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require traffic analysis related to the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR. However, additional VMT analysis is provided below given that such

INTRODUCTION

analysis was not a requirement at the time of the original EIR. The VMT analysis is included as an Appendix.

Vehicle Miles Traveled (VMT) Impacts caused by retail land uses discussed in TR-1 were found to be less than significant.

- The existing 130,373-square-foot building, previously occupied by a Walmart Superstore, remains entitled for similar commercial uses under current zoning. The proposed project would redevelop this building and construct additional retail space on-site, resulting in a net increase of approximately 17,396 sf of gross leasable floor area. Since the net increase is less than 50,000 sf, no further VMT analysis is required for the retail land uses, as their VMT impacts are considered less than significant. Therefore, the revisions do not increase the severity of any impact. However, the revisions do not change the environmental conclusion of less than significant that was provided in the certified EIR.

Conflicts with adopted policies, plans or programs regarding auto network improvements discussed in TR-2 were found to be less than significant.

- The revision does not propose frontage improvements along Mitchell Road or E. Hatch Road; instead, it will maintain the existing driveways as-is. The 2022 RTP/SCS identifies a planned improvement to enhance Mitchell Road to six lanes between SR 99 and the Tuolumne River. The minimum curb-to-curb width along the project site Mitchell Road frontage is 90 feet, which can accommodate a six-lane arterial without on-street parking. Similarly, E. Hatch Road is planned to be widened to a four-lane expressway between Mitchell Road and Faith Home Road, as outlined in the City of Ceres General Plan 2035 (2018). The existing E. Hatch Road frontage provides a minimum 80-foot curb-to-curb width, sufficient to accommodate a four-lane expressway without on-street parking. The revision would not conflict with these future planned auto network improvements along Mitchell Road or E. Hatch Road; therefore, the impact is considered less than significant.

The revision is expected to increase the number of trips using site driveways by 11,180 daily, including 602 during the AM peak hour and 941 during the PM peak hour, compared to prior operations when the Walmart Superstore was active on-site. This increase in peak hour trips at project driveways warrants optimizing traffic signal timings at the adjacent signalized intersections on Mitchell Road (at the Central Driveway and at Hatch Road) to reduce average vehicle delay at both intersections. With the optimization, the revisions do not change the environmental conclusion of less than significant that was provided in the certified EIR.

RECOMMENDATION TR-2: Coordinate with the City of Ceres Engineer to optimize traffic signal timings at the signalized intersections of Mitchell Road/Central Driveway and Mitchell Road/E. Hatch Road.

EIR ADDENDUM

Conflicts with adopted policies, plans or programs regarding bicycle or pedestrian facilities discussed in TR-3 were found to be less than significant.

- The Ceres Citywide Active Transportation Plan (2021) identifies planned Class 4 protected bike lane improvements on Mitchell Road between SR 99 and the Tuolumne River, as well as the installation of high-visibility crosswalks on all legs of the Mitchell Road/E. Hatch Road intersection. The existing 90-foot curb-to-curb width along the Mitchell Road frontage is sufficient to accommodate a six-lane arterial and Class IV protected bike lanes without on-street parking. The project also does not preclude the installation of high-visibility crosswalks at the adjacent Mitchell Road/E. Hatch Road intersection.

The revisions would not conflict with future bicycle or pedestrian facility planned improvements along Mitchell Road or E. Hatch Road; therefore, the impact is considered less than significant.

Inadequate transit service to meet demand discussed in TR-4 were found to be less than significant.

- The revision would not eliminate or adversely affect existing transit access. StanRTA currently operates Route 47, with bus stops located on Mitchell Road, adjacent to the project site. These existing bus stops are within a five-minute walking distance of the project site. Therefore, this impact is considered less than significant, and no mitigation is required

However, the existing StanRTA Route 47 serves a bus stop on northbound Mitchell Road, just north of the project's southern driveway. The bus stop includes signage, a bench, and a trash bin, but lacks a shelter

While not required under CEQA, installing a bus stop shelter could enhance convenience for transit users, particularly during extreme weather conditions, and may encourage greater transit use by project site workers and visitors.

RECOMMENDATION TR-4: Install bus stop shelter at existing StanRTA bus stop on northbound Mitchell Road on project site.

Increased hazards due to geometric design features discussed in TR-5 were found to be less than significant.

- The revision does not propose frontage improvements along Mitchell Road or E. Hatch Road; instead, it will maintain the existing driveways as-is. The revision is expected to increase the number of trips using site driveways by 602 AM and 941 PM peak hour trips compared to prior operations when the Walmart Superstore was active on-site. As shown in Table 2, approximately 2,070 vehicles were counted during the AM peak hour and 2,180 vehicles during the PM peak hour along the Mitchell Road frontage. Along the E. Hatch Road frontage, approximately 1,550

INTRODUCTION

vehicles were counted during the AM peak hour and 1,500 vehicles during the PM peak hour.

These high existing volumes may make it difficult for vehicles to exit the site and turn left at the unsignalized E. Hatch Road driveway and southern Mitchell Road driveway. However, if left-turn delays are significant at these unsignalized driveways, drivers have the option to exit via the signalized central Mitchell Road driveway.

General Plan Policy 3.A.7 supports the development of an expressway system, with E. Hatch Road classified as a Class C expressway. Additionally, Policy 3.B.1 requires that development and redevelopment projects consider the inclusion of medians within the existing roadway cross-section to limit turning movements along Mitchell Road and E. Hatch Road.

Although not required by CEQA, the project applicant should consider modifying the southern driveway on Mitchell Road and the E. Hatch Road driveway to prohibit left-turn movements out of the site through a combination of median and signage improvements. Restricting left-turns out of both driveways would reduce turning conflicts for vehicles and trucks, representing a safety improvement over existing condition.

RECOMMENDATION TR-5: Prohibit left-turn movements out of the southern Mitchell Road driveway and E. Hatch Road driveway to reduce turning movement conflict on these high-volume roadways, while maintaining left-turn access into both driveways to help minimize vehicle queuing within public right-of-way at nearby intersections.

Maintaining left-turn access into the site, as well as access to Rosewood Avenue (opposite the southern Mitchell Road driveway) and the self-storage facility (opposite the E. Hatch Road driveway), would help reduce queue lengths at the Mitchell Road/E. Hatch Road intersection and the central Mitchell Road driveway. Maintaining inbound access at the southern Mitchell Road driveway would also facilitate direct truck access to the loading docks located on the south side of the project site.

Any modifications to site access at project driveways must be approved by City of Ceres staff and designed in accordance with local design standards.

The queuing model developed for the proposed Dutch Bros shop estimates a 95th percentile queue length of 16 vehicles. All proposed drive-through facilities provide a minimum queueing capacity of 16 vehicles or more.

The revision does not currently propose modifications to site driveways. The 95th percentile drive-through queues anticipated at both the fast-food restaurants and the Dutch Bros location can be accommodated within the designated queuing areas

EIR ADDENDUM

without spilling back onto drive aisles. Additionally, all drive aisles meet minimum design standards (further details on parking lot design are provided below). The project would not increase hazards due to geometric design features; therefore, the impact is considered less than significant.

Although not required by CEQA, implementation of the driveway improvements specified in Recommendation TR-5 would reduce left-turn conflicts for vehicles exiting the project site, representing a safety improvement over existing conditions.

Inadequate emergency access discussed in TR-6 were found to be less than significant.

- The nearest fire station is City of Ceres Fire Station 18, located at 3101 Fowler Road, approximately 1,700 feet south of the project site. The project site includes four driveways, each providing a minimum clear width of 24 feet. The internal drive aisle network also maintains a minimum clear width of 24 feet to all on-site buildings. According to Appendix D of the 2022 California Fire Code, a minimum width of 20 feet is required for fire apparatus access roads serving buildings less than 30 feet in height. Therefore, the revision provides adequate emergency access, and the impact is considered less than significant.

The final site plan must be approved by the City of Ceres Fire Department to ensure the emergency access routes meet requirements to facilitate the safe movement of emergency vehicles. Therefore, this impact is considered less than significant, and no mitigation is required.

Cumulative impacts to transportation & circulation were found to be less than significant.

- A cumulative impact analysis evaluates the combined transportation effects of the proposed Project alongside other reasonably foreseeable developments in the surrounding area. Given the proposed project description and based on the findings of the project-level impact analysis described above, the cumulative analysis is expected to yield similar conclusions. The revisions are not anticipated to contribute substantially to cumulative transportation impacts beyond those already identified and addressed at the project level described above. As such, under cumulative conditions, the proposed Project is expected to result in less than significant transportation impacts pursuant to Appendix G of the CEQA guidelines.

Section 3.9: Public Services and Utilities

The modified project, like the original project, has the potential to affect public services and utilities. The potential for the impacts associated with the original project are discussed in section 3.9 of the Draft EIR. This section discusses: including police, fire, telephone, electricity, natural gas, water, sewer, solid waste, and hospitals. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally,

INTRODUCTION

Mitigation Measures were adopted and will remain applicable to the modified project. The mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require appropriate levels of public services and utilities to serve the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Section 3.10: Energy Conservation

The modified project, like the original project, has the potential to affect energy conservation. The potential for the impacts associated with the original project are discussed in section 3.10 of the Draft EIR. There are several relevant General Plan goals and policies that will remain applicable to the modified project. The project is consistent with those goals and policies. Additionally, Mitigation Measures were adopted and will remain applicable to the modified project. The mitigation will remain unchanged and will be implemented as provided by the Mitigation Monitoring Program to require appropriate energy conservation designed into the buildings of the modified project. The project modifications do not cause significant changes to the land uses on the developed site, and they do not change the environmental conclusion of less than significant with mitigation that was provided in the certified EIR.

Analysis of Impacts Found to be Significant and Unavoidable

The DEIR indicates that implementation of the original project would create several adverse environmental impacts, which are documented in Section 3.0 of the DEIR documents. The DEIR recommends mitigation measures for most of these impacts which, if implemented, would reduce these impacts to a level of insignificance. However, several impacts were determined to be unavoidable, even within the implementation of mitigation measures described in the DEIR. This section briefly addresses these unavoidable adverse impacts.

Land Use

The original project implementation was determined to have a loss of productive agricultural land within the project site, and that loss would contribute to the cumulative loss of farmland in the Ceres area as the city grows. The modified project is on an already developed site, meaning there will be no loss of productive agricultural land. This significant and unavoidable impact is not applicable to the modified project.

Air Quality

The original project implementation was determined to have a increase activities that generate air pollutant emissions within the project site. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. There amount of construction is very limited and does not include significant site grading as was needed for the original development.

EIR ADDENDUM

Overall, the Modified Project will be required to comply with all rules and regulations of the San Joaquin Valley Air Pollution Control District. It is also notable that emissions associated with traffic under the current year is significantly lower on a per capita basis when compared to emissions in 1989 when the project was originally approved. This significant and unavoidable impact is not applicable to the modified project.

Traffic

The original project implementation was determined to have increased traffic. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. The revisions do not increase the severity of any impact. As mentioned prior, the existing 130,373-square-foot building, previously occupied by a Walmart Superstore, remains entitled for similar commercial uses under current zoning. The Modified Project would redevelop this building and construct additional retail space on-site, resulting in a net increase of approximately 17,396 sf of gross leasable floor area. Since the net increase is less than 50,000 sf, no further VMT analysis is required for the retail land uses, as their VMT impacts are considered less than significant. These revisions would be beneficial as it relates to this environmental topic. However, the revisions do not change the environmental conclusion of significant and unavoidable that was provided in the certified EIR.

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Noise

The original project implementation was determined to have an increased activities that generate noise. The modified project is on an already developed site, meaning there will be a reuse of the site facilities in a similar way that was originally anticipated. The amount of construction is very limited and does not include significant noise generating operations as was needed for the original development. The on-going retail operations would have similar noise levels as occurred during the operation of the Walmart. Overall, the Modified Project will be required to comply with all rules and regulations of the City's Noise Ordinance. The revisions do not increase the severity of any impact. Any changes in site layout or tenant mix would result in negligible differences in overall noise levels compared to existing conditions. However, the revisions do not change the environmental conclusion of significant and unavoidable that was provided in the certified EIR.

Analysis of Growth Inducement

The EIR found that growth inducing impacts associated with the Original Project were primarily related to the economic aspects of the commercial development in that they would provide jobs and could attract additional growth to the area. The Modified Project site was developed and realized any growth inducement associated with the original Walmart. However, the closing of the Walmart resulted in a loss of jobs that

have not been replaced. The Modified Project is expected to provide a similar number of jobs on the site relative what was anticipated under the Original Project.

Analysis of Impacts Found to be Irreversible Changes

The EIR found that the original project would entail the commitment of natural resources, energy resources, and human resources, and that the ongoing maintenance of the project site by the occupants will entail further commitment of energy resources in the form of natural gas and electricity generated by coal, hydro-electric power, or nuclear energy. The EIR found that the original project would irreversibly alter vacant land and convert agricultural uses, and would preclude the use of the site from other uses or open space. The EIR found that these commitments constitute long-term obligations since it is generally impossible to return the land to its original condition once it has been developed. This irreversible change is not applicable to the modified project, as the site has already been converted and it was recognized that there would be a commitment of natural resources, energy resources, and human resources to the project and on-going maintenance.

EIR ADDENDUM



Figure 1. Regional Project Location

INTRODUCTION

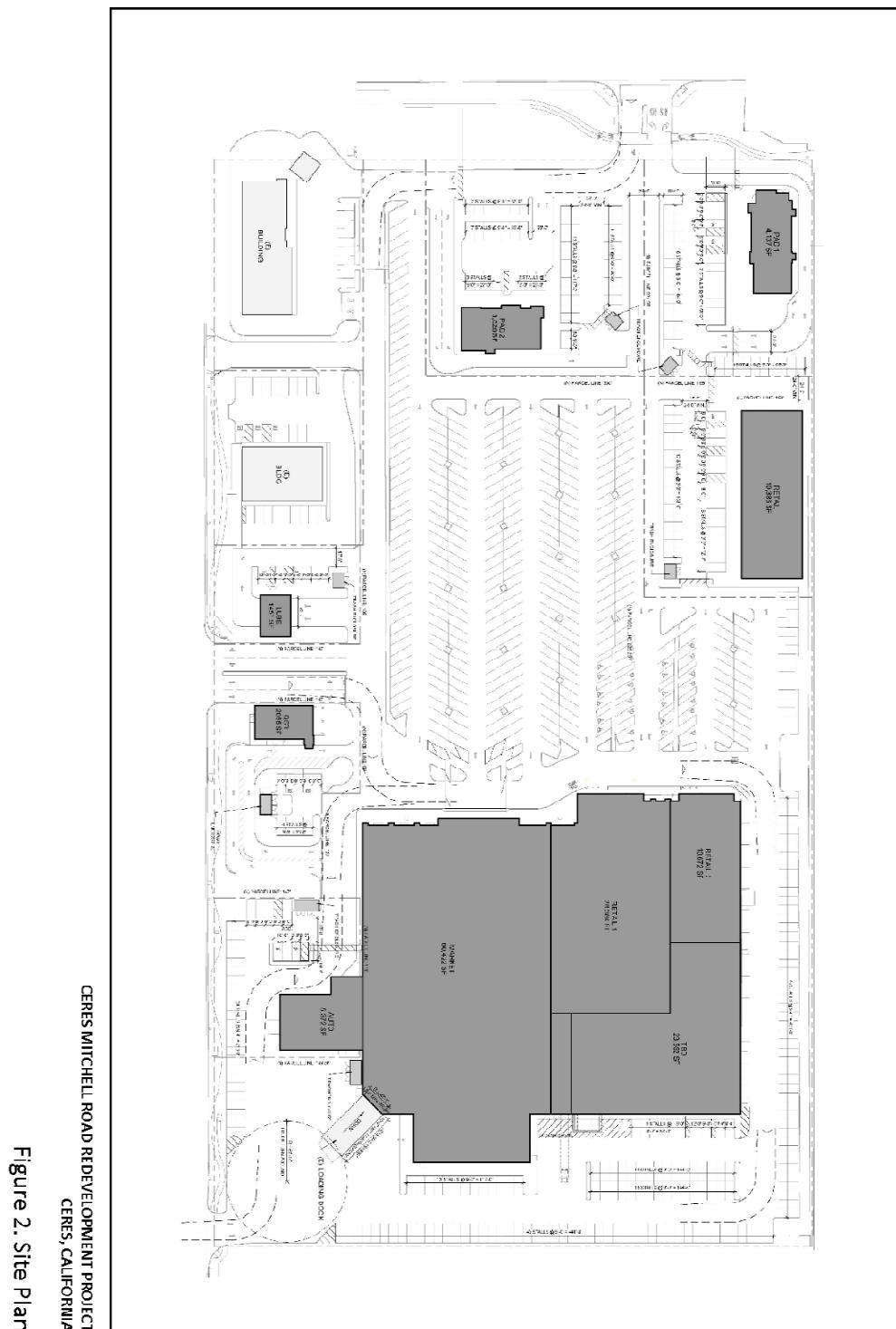


Figure 2. Site Plan

Source: DE NOVO Architects, § 14500.25.
May 2014, Supplement 17, 2025.

EIR ADDENDUM

Appendix A: CEQA Transportation Impact Analysis

